

THE HEART OF HORACE:

A COMMUNITY-DRIVEN DOWNTOWN NEIGHBORHOOD PLAN



In Collaboration with:




METROCOG
FM REGIONAL TRANSPORTATION PLANNING ORGANIZATION

FINAL DRAFT
11/05/2025

Prepared by:





The preparation of this document was funded in part by the United States Department of Transportation with funding administered through the North Dakota Department of Transportation, the Federal Highway Administration, and the Federal Transit Administration. Additional funding was provided through local contributions from the City of Horace, in North Dakota. The United States Government and the State of North Dakota and Minnesota assume no liability for the contents or use thereof.

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Preparation of this document was funded, in part, through Complete Streets set-aside funding as described in Section 11206 of the Infrastructure Investment and Jobs Act (IIJA). Eligible planning activities herein are intended to ensure safe and adequate travel of all users of the transportation system, including pedestrians, bicyclists, public transportation users, children, older individuals, individuals with disabilities, motorists, and freight vehicles.

ACKNOWLEDGEMENTS

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EXECUTIVE SUMMARY

This chapter of the Horace Downtown Neighborhood Plan (Plan) provides an overview and background about what downtown is, how downtown Horace is defined, why downtown Horace is important, and how the Plan will be used moving forward.

Horace Downtown Neighborhood Plan



PLAN VISION & GOALS

The Vision and Goals represent the comments and preferences of Horace residents who participated in public engagement throughout this project and the approximately **1,300 individual responses received** through the Plan's development!

Vision

Horace, North Dakota is a small, welcoming town nestled along the Sheyenne River. We've quickly grown into a community that cherishes its unique character and are dedicated to preserving the qualities that make Horace such a great place to live. Whether you're enjoying the beautiful, rural surroundings, taking a stroll to our charming Main Street, or biking to the famous "Uncle John's" giant cottonwood tree, you'll find a community for all ages and stages of life; a good hometown.

Our Downtown Neighborhood Plan will reflect our community's values to:

Preserve our quality of life that has persevered for over 150 years

Thoughtfully accommodate growth

Establish a publicly supported vision for downtown

Sustain what makes Horace such a desirable place to call home

Goals

1. Preserve community character and strengthen the city's identity.
2. Right-size infrastructure to surrounding development.
3. Invest in older, core parts of town.
4. Prioritize connection and safety for people walking and biking.
5. Identify funding opportunities for Plan implementation.
6. Cultivate and sustain unique community experiences and opportunities.

HOW DO WE USE THE PLAN?



Establish Policy

The Plan outlines the City's official stance on the future of downtown and multimodal transportation development, offering clear guidance for leaders, residents, businesses, and visitors.



Identify & Prioritize Strategies

It provides actionable steps and prioritizes projects to turn the City's vision into reality.



Provide Guardrails

The Plan preserves Horace's identity and reflects public input, setting expectations for future development that meets the community's vision for the future.

Additional Uses

- Review public engagement results and public sentiment
- Identify community challenges and opportunities
- Understand and shape community identity
- Envision future growth
- Prioritize implementation strategies
- Explore funding options

QUICK FACTS

- **Horace Population in 2023:** 6,655
- **Area:** 11.6 square miles.
- **Density:** 573.7 people per square mile.
- **Median household income:** \$146,184.
- **Number of households:** 1,951.
- **Average household size:** 2.8 people



DOES HORACE HAVE A DOWNTOWN?

Yes! However, locals generally refer to this area as 'Main Street'. Like other small towns across North Dakota, Main Street has served as the heart of Horace's downtown for over 150 years. Historically, the boundary of downtown has not been defined. Through development of the Plan, a clearer definition of Downtown Horace has been established by identifying a study focus area.



CITY HISTORY

Horace was established in 1871 (Dakota Territory) by a diverse group of settlers including professionals such as doctors, teachers, and merchants. Since settlement, the primary way to make a living was through farming and agriculture. As the community grew and technology advanced, grain elevators were constructed in 1900 on Main Street, in the very heart of the community, and have served as distinctive landmarks of Horace since then.

Main Street has long served Horace's central business district and primary location for commerce and local services. As the photos show below, Main Street has had numerous businesses and storefronts, likely since shortly after settlement in 1871, as documented in historical photographs.



Photo circa the 1940s or 1950s. There was a grocery store located in the southwest corner of the intersection of Main Street and Center Avenue (Fairway Foods). Grain elevators can be seen in the background.

WHY IS DOWNTOWN HORACE IMPORTANT?



1. Preserving Community Character

Downtown Horace helps maintain the city's small-town charm, which is threatened by rapid growth. The Downtown Neighborhood Plan ensures future development aligns with residents' vision, with right-sized infrastructure and design.



2. Managing Growth Pressure

Horace has grown over 500 percent since 1990 and continues to expand. Downtown offers a way to focus growth inward, preserving community values.



3. Financial Efficiency & Value

Downtown development uses existing infrastructure and generates higher value per acre than suburban-style development. It reduces reliance on special tax assessments and increases property and sales tax revenue through compact, mixed-use designs that foster new business and economic development.



4. Multimodal Transportation Hub

Downtown is walkable and bike-friendly due to its traditional layout. While new paths have been added across the community, downtown remains central to connecting neighborhoods and promoting pedestrian and cyclist access citywide.

PUBLIC ENGAGEMENT

Round 1 Engagement

In the Fall of 2024, the first round of public engagement included two significant efforts to collect feedback from Horace residents about the future of downtown:

- Online Community Survey #1
- Bean Days – In-Person Pop-Up Booths

Stakeholder Meetings

The project team conducted specific outreach to key stakeholders of the Downtown Neighborhood Plan. Stakeholders were identified as strategic partners in plan development and for implementation of future Plan recommendations. Additionally, property and business owners along Main Street were invited to participate in stakeholder meetings to discuss opportunities and challenges of the plan.

Round 2 Engagement

At the beginning of 2025, the second round of public engagement provided two additional ways to collect feedback from Horace residents about the future of downtown:

- Online Community Survey #2
- Public Workshops

The second round of public engagement for the Plan presented options and concepts for the future of downtown, and future connections and multimodal transportation strategies.



WHAT GUIDES OUR DOWNTOWN PLAN?

Residents, both old and new, were in agreement about the future of Horace:

1. Rural Small Town Identity
2. A Community Accessible by Walking, Biking, and Driving
3. Utility Improvements and Maintenance
4. High Quality of Life
5. Activities for Everyone

There were 1,300 individual responses to engagement activities and surveys as part of the Plan's development

Resident for Less than 5 years
47% of respondents

Resident for 5 years or more
48% of respondents



FINAL DOWNTOWN CONCEPT

The final vision for Downtown Horace positions the core downtown—identified by the community—as the vibrant center of the city's economic and social life. At its heart, the plan celebrates Horace's small-town charm and agricultural heritage by featuring one of the historic grain elevators as a central landmark. This community space, adjacent to the Senior Center, will serve as a gathering place for events such as a farmer's market, community celebrations, and potentially a Veteran's Memorial. Key elements of the concept include:

**Preserving
Character & Scale**

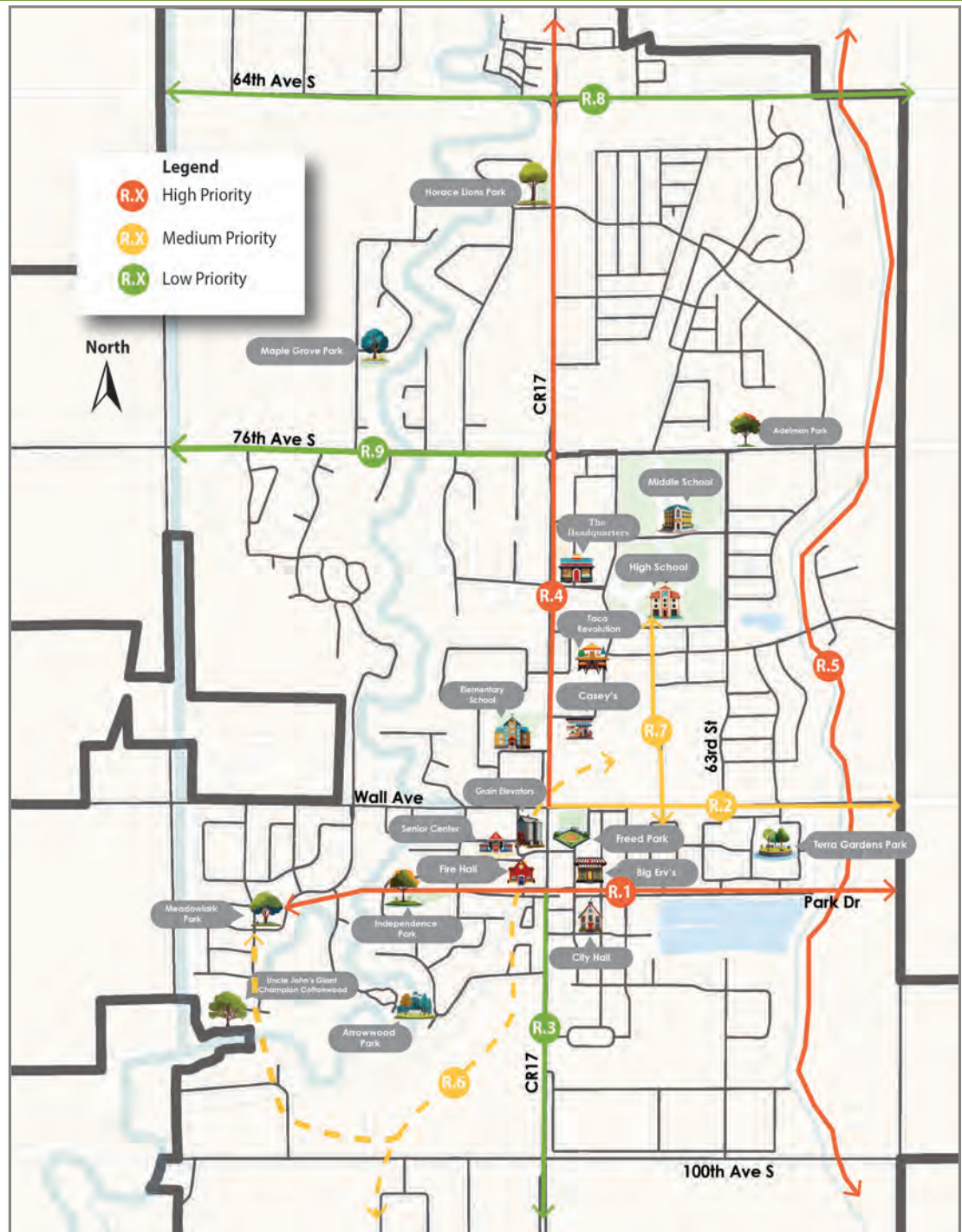
**Commercial
Development**

**Connectivity &
Accessibility**

**Traffic & Parking
Improvements**

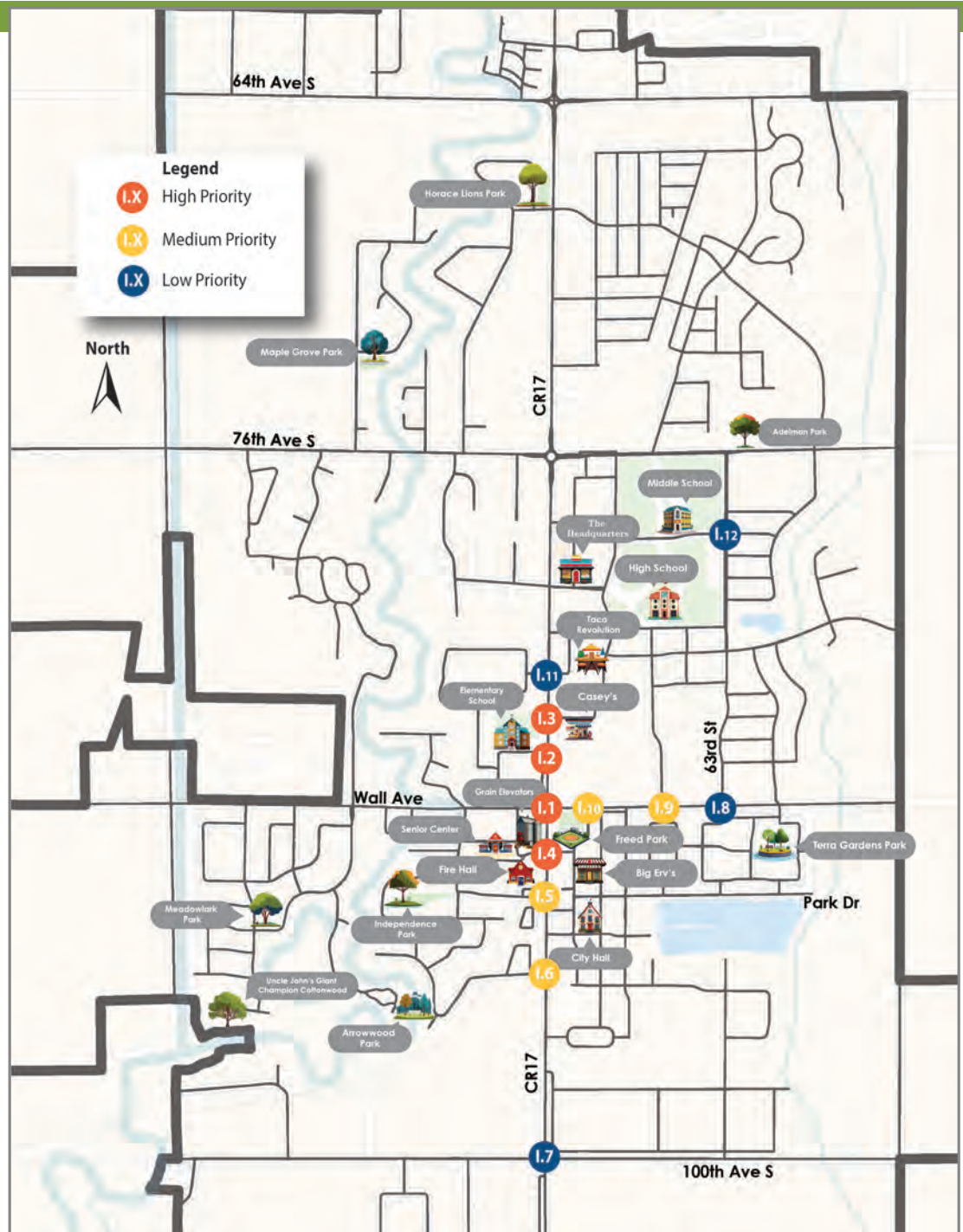
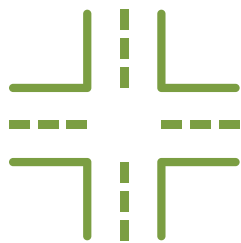


MULTIMODAL ROUTE PRIORITY



ID	Location	From	To	Description	Priority
R.4	Main St./CR 17	Wall Ave.	52nd Ave. S.	Shared Use Path, both sides.	High
R.5	Drain 27	100th Ave. S.	64th Ave. S.	Off-Street Trail, at least one side.	High
R.1	Park Dr.	Boxelder Cir.	Drain 27	Off-Street Trail and Yield Street.	High
R.2	Wall Ave.	Main St.	57th St. S.	Sidewalk, north side; and Shared Use Path, south side.	Medium
R.7	Front Street	Wall Ave.	82nd Ave. S.	Shared Use Path, west side; and Sidewalk, east side.	Medium
R.6	Southwest Alignment	S. of 100th Ave. S.	Wall Ave.	Off-Street Trail and Shared Use Path.	Medium
R.3	Main St./CR 17	Park Drive	S. of 100th Ave. S.	Shared Use Path, both sides.	Low
R.9	76th Ave. S.	Sheyenne Diversion	57th St. S.	Shared Use Path, south side; and Sidewalk, north side.	Low
R.8	64th Ave. S.	Sheyenne Diversion	57th St. S.	Shared Use Path, at least one side.	Low

MULTIMODAL INTERSECTION PRIORITY



ID	Intersection Location	Description	Priority
I.1	Main St. & Wall Ave.	Maintain existing intersection control with multimodal improvements.	High
I.2	Main St. & Ironwood Dr.	Maintain existing control with strategic upgrades to multimodal improvements.	High
I.3	CR 17 & Lakeview Dr.	Maintain Existing Configuration.	High
I.4	Main St. & Center Ave.	Convert to 4-Way Stop control with multimodal improvements.	High
I.5	Main St. & Park Dr.	Convert to 4-Way Stop control with multimodal improvements.	Medium
I.7	CR 17 & 100th Ave. S.	Convert to Roundabout with multimodal improvements.	Medium
I.9	Wall Ave. & Front St.	Maintain existing intersection control with multimodal improvements.	Medium
I.10	Wall Ave. & Nelson Dr./Future Lakeview Dr.	Convert to 4-Way Stop control with multimodal improvements.	Medium
I.6	CR 17 & Liberty Ln.	Maintain existing intersection control with multimodal improvements.	Low
I.8	Wall Ave. & 63rd St. S.	Maintain existing intersection control with multimodal improvements.	Low
I.11	CR 17 & Chestnut Dr. or 81st Ave. S.	Maintain existing intersection control with multimodal improvements.	Low
I.12	63rd St. S. & 79th Ave. S.	Maintain existing intersection control with multimodal improvements.	Low

INTRODUCTION

An aerial photograph of downtown Horace, Nebraska, with a blue color overlay. A wide street runs diagonally from the bottom left towards the top right. On the left side of the street, there are several commercial buildings, including a large white warehouse-like structure and a parking lot filled with cars and trucks. On the right side, a prominent building features a large sign that reads "Harvest States" with a stylized logo above it. Further up the street on the right, a tall white water tower is visible, topped with a globe that has the word "Horace" written on it. The background shows more residential and commercial buildings, trees, and a flat landscape under a clear sky.

This chapter of the Horace Downtown Neighborhood Plan (Plan) provides an overview and background about what downtown is, how downtown Horace is defined, why downtown Horace is important, and how the Plan will be used moving forward.

WHAT IS DOWNTOWN?

Downtown Definition for Small Towns

The central business district or the primary commercial area, serving as the heart of the community. Often a smaller-scale version of an urban downtown, characterized by a concentration of commercial shops, services, offices, and public institutions.

Downtown is defined by¹:

Concentration of Commerce

Characterized by a concentration of businesses, including but not limited to retail stores, restaurants, and service providers, often serving the needs of residents and visitors.

In Horace, there is a *concentration of commerce*, with several existing businesses and/or services along Main Street, which historically has served as the city's central business district.

Economic Importance

Revitalization efforts in rural downtowns are often focused on boosting economic activity and attracting new businesses and services.

Horace's downtown has *economic importance* and historically has been home to critical local economic activity. For example, the grain elevators (now inoperable) were a long-standing landmark of agricultural commodities and economic activity.

Social Hub

Downtowns often serve as a social hub for the community, providing places for people to gather, socialize, and connect.

Downtown Horace has been the community's gathering place for community events, including Bean Days, the city's annual festival. Since 1998, Bean Days has been celebrated on Main Street with a parade, numerous events, and live entertainment. The Senior Center and Freed Park provide other notable places for gathering in downtown Horace (see map in **Figure 4**).

Cultural and Historical Significance

Many rural downtowns retain historic buildings and structures that reflect the community's heritage, adding to unique character and appeal.

Horace's downtown includes several buildings and homes that reflect the city's agricultural heritage, *cultural, and historical significance* including the water tower, grain elevators, and several other buildings which have stood as landmarks since the early 1900's.

Figure 1. 2024 Bean Days Parade



¹ USDA. National Agricultural Library. www.nal.usda.gov/rural-development-communities/downtown-revitalization. Accessed April 23, 2025.

Multiple Functions

Rural downtowns often include multiple land uses such as retail shops, services, offices, public institutions, and housing, serving multiple functions for communities.

Downtown Horace serves *multiple functions* and has a mix of commercial, institutional, and single-family residential land uses.

Pedestrian-friendliness

Because of the inherent smaller scale, often just a few blocks, rural downtowns are typically more pedestrian-friendly, making it easier for people to explore and experience the area by walking.

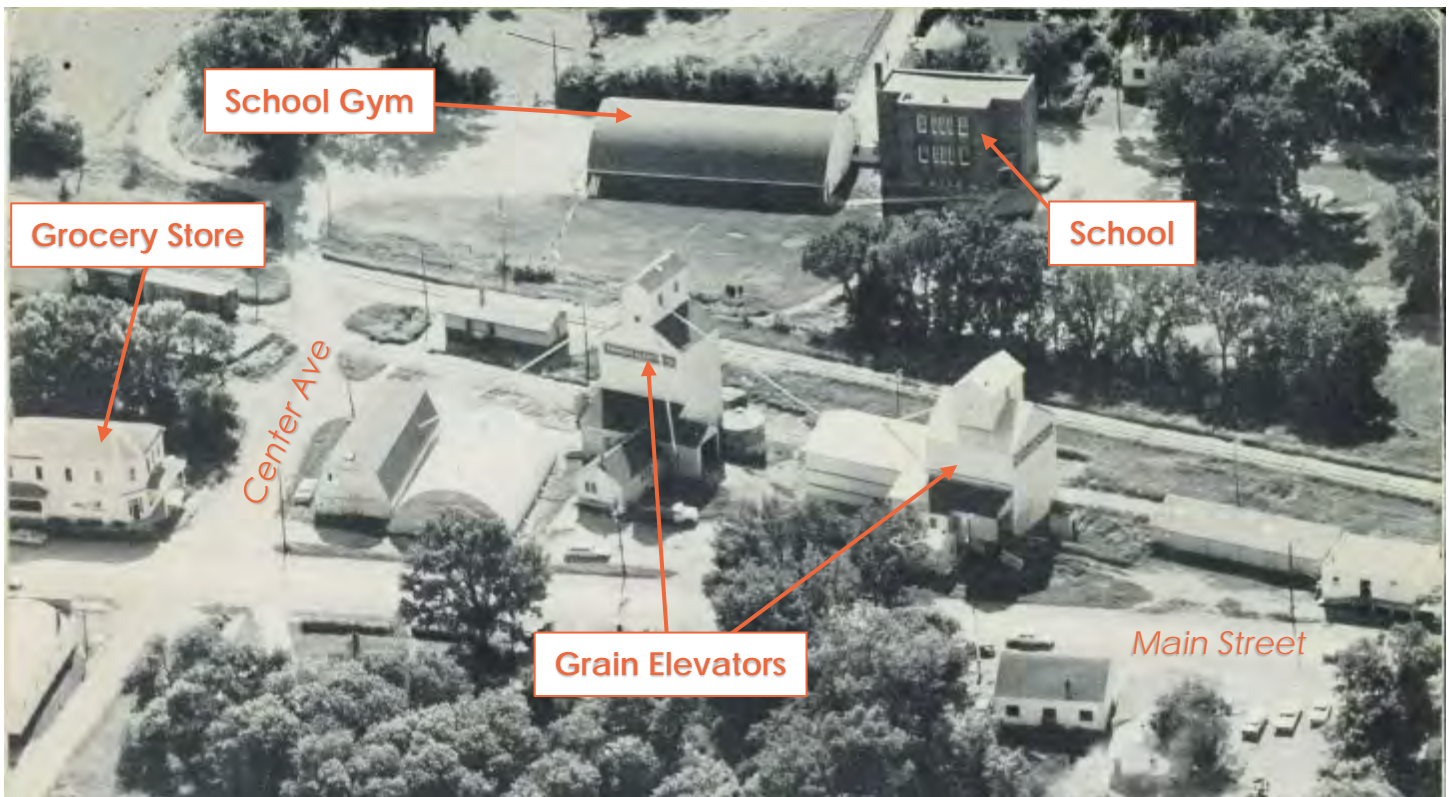
Until recently, with the addition of new developments, Main Street between Wall/88th Avenue South (downtown) was the only street with sidewalks in Horace. *Pedestrian friendliness* has been a priority in the area for a long time, as indicated by long-term investment in pedestrian infrastructure.

Strong Sense of Place

The close-knit local community and sense of local roots can contribute to a strong sense of place with rural downtowns, fostering a sense of belonging and identity.

Downtown Horace has a *strong sense of place*, due to the community events which take place there, the existing businesses and institutions, and the unique character of the built and natural environment (Grain Elevators, large trees along the Sheyenne River, etc.).

Figure 2. Main Street, Circa 1965



DOES HORACE HAVE A DOWNTOWN?

Yes! However, locals generally refer to this area as 'Main Street'. Like other small towns across North Dakota, Main Street has served as the heart of Horace's downtown for over 150 years. Historically, the boundary of downtown has not been defined. Through development of the Plan, a clearer definition of Downtown Horace has been established by identifying a study focus area (see map in **Figure 4**).

CITY HISTORY

Horace was established in 1871 (Dakota Territory) by a diverse group of settlers including professionals such as doctors, teachers, and merchants. Since settlement, the primary way to make a living was through farming and agriculture. As the community grew and technology advanced, grain elevators were constructed in 1900 on Main Street, in the very heart of the community, and have served as distinctive landmarks of Horace since then.

Main Street has long served Horace's central business district and primary location for commerce and local services. As the photos show in **Figure 3**, Main Street has had numerous businesses and storefronts, likely since shortly after settlement in 1871, as documented in historical photographs.

Figure 3. Horace's Main Street Through Time

Main Street Looking South – Late 1800s/Early 1900s



Photo circa the late 1800s or early 1900s, prior to private automobile use in North Dakota. There are several businesses along Main Street, as indicated by storefronts and hitching posts for horses.

Main Street Looking Northwest – 1940s-1950s



Photo circa the 1940s or 1950s. There was a grocery store located in the southwest corner of the intersection of Main Street and Center Avenue (Fairway Foods). Grain elevators can be seen in the background.

Main Street Looking North – 1973

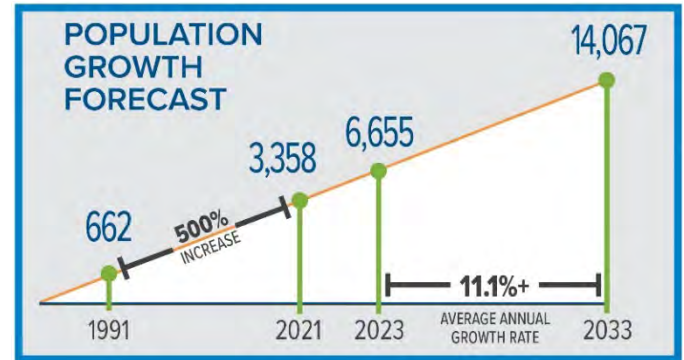


Photo circa 1973. There was an automobile service business (as indicated by the Standard Oil sign) located on the east side of Main Street between Center Avenue and Wall Avenue/88th Avenue South.

Figure 4. Downtown Neighborhood Plan Focus Area



Figure 5. Horace Population Growth & Forecast (City of Horace)



Community Character

As growth has continued and is projected to continue, there is documented concern from new and long-term residents about the future of Horace. Residents and visitors alike believe growth poses a risk to the cherished small-town character of the community and there is genuine concern that rapid change through new development may negatively transform Horace.

Downtown is key to preserving the small-town charm Horace is known for. The Plan will guide the City to establish guardrails to future downtown development, provide recommendations, and include strategies supportive of residents' vision for the future of Horace. The community's vision includes right-sized infrastructure and development downtown.

Downtown Value

Horace's downtown is strategically important to the financial health of the community. There is documented concern from residents about the investment in Horace's growth areas, with particular concern about how growth is financed through special tax assessments.

Downtown Horace provides the opportunity to capitalize on existing infrastructure and grow the commercial tax base of the city with more efficient

WHY IS DOWNTOWN HORACE IMPORTANT?

Downtown is critically important for Horace, presenting an essential location for the preservation of community character and helping the city focus inward to preserve the values that have become threatened by significant growth and development pressure. Through the Downtown Neighborhood Plan's framework, Horace can generate exceptional value to the city through smart, efficient investments downtown. It is also important for downtown to continue to serve as a hub for multimodal transportation connections across the community including walking and biking.

Growth Pressure

Horace is growing rapidly, creating enormous development pressure on the city and threatening dramatic change to the community as development and potential redevelopment occurs.

Between 2022 and 2023, Horace's population increased by 32 percent. In 10 years (2033), the City anticipates a population of more than 14,000 people.

Since 1990, the population of Horace has increased by over 500 percent. Over the past few years, Horace has been the fastest-growing city in North Dakota, by percent change. As shown in **Figure 5**, an average annual growth rate of approximately 11.1 percent is anticipated through the year 2033. However, the rate is only an estimate and will likely fluctuate. The City's latest 10-year population outlook estimates a population over 14,000 residents in 2033². See **Appendix A** for more information regarding growth in Horace.

² City of Horace. (2025). *10-Year Population Outlook*.

development and less reliance upon special tax assessments used for capital investments such as for streets, utilities, and other basic infrastructure. Horace’s downtown area is defined by more traditional, storefront-style development. Compact, walkable, and mixed-use design strategies are not only central to creating inviting, human-centered spaces – it turns out, this is also much more financially efficient for the community.

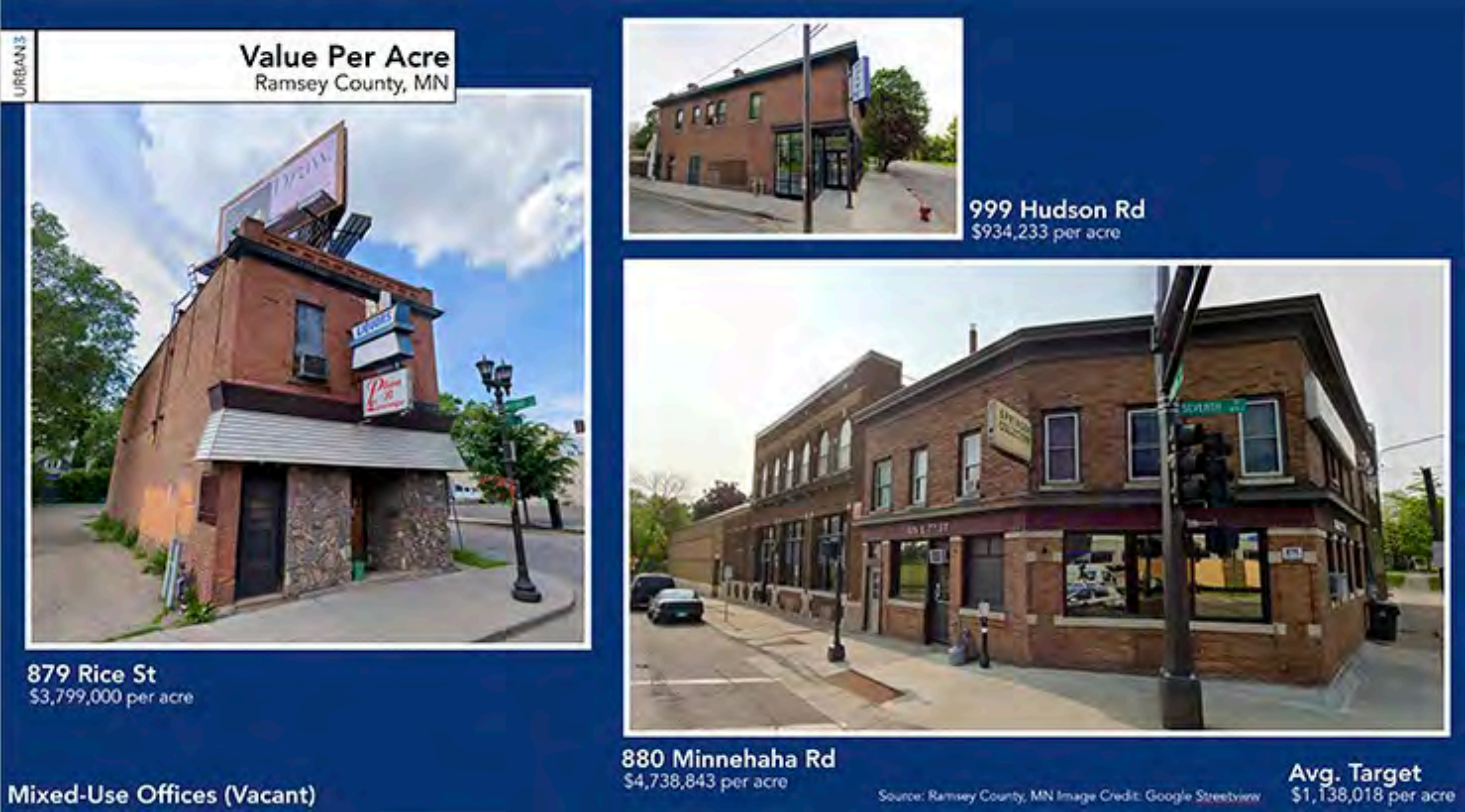
In most cities, downtowns and older neighborhoods are very economically productive. Newer urban developments, especially places surrounded by expensive streets and parking lots, are weak economic performers.

Land is a critical commodity in Horace, and infrastructure like roads and utility infrastructure to serve the land are calculated based upon length. As such, when comparing the value of land through a per-acre analysis, the picture is clear; traditional development is not only more valuable to communities than suburban, auto-oriented development, but it is cheaper for the City to serve.

Table 1. Ramsey County Value Per Acre Example

Address	Value Per Acre	Difference
Avg. Ramsey County Target Store	\$1,138,018	N/A
879 Rice St.	\$3,799,000	+ \$2,660,982
999 Hudson Rd.	\$934,233	- (\$203,785)
880 Minnehaha Rd.	\$4,738,843	+ \$3,600,825

Figure 6. Ramsey County Value Per Acre Example



Recent research from Ramsey County, Minnesota highlights that traditional commercial and mixed-use developments (i.e. commercial use main floor, offices or condo/apartment above) often have a higher value than big-box suburban style development (e.g. Target)³. **Figure 6** and **Table 1** show three (3) separate traditional building locations in Ramsey County and each location's value per acre (VPA). Financial efficiency in downtown Horace is expected through both infrastructure costs, property tax revenue, and local sales tax revenue.

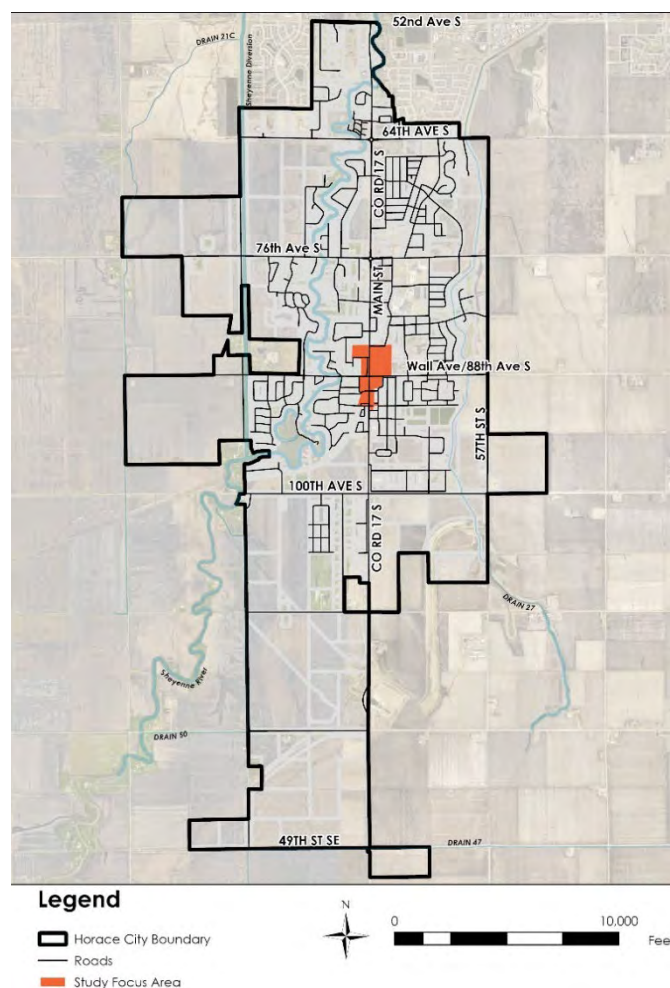
Hub for Pedestrians & Bicyclists

Traditional small-town downtowns like Horace's Main Street are hubs of pedestrian activity. Because development occurred before widespread adoption of the automobile in the late 1800s and early 1900s, development patterns allowed numerous storefronts on a single block. In a traditional downtown setting, including Horace's downtown area, it is most convenient to walk from storefront to storefront or to other nearby destinations rather than driving, because destinations are much closer together and more conducive to walking.

As Horace has grown, sidewalks and shared use paths have been added with new developments across the city. However, full connections across town and to downtown do not exist. The City has already constructed some paths to connect people and fill the gaps, but there is still a lot of work to be done to connect older and newer neighborhoods across the city. Downtown Horace is very centrally located and provides an ideal focal point to connect multimodal sidewalks and paths, not only to

get to and from downtown, but to connect across the entire community (see **Figure 7**).

Figure 7. Downtown Horace Context Map



³ Lindeke, B. (2021, February 16). *Per-acre analysis: a unique way of looking at urban economics*. MinnPost. <https://www.minnpost.com/cityscape/2021/02/per-acre-analysis-a-unique-way-of-looking-an-urban-economics/>

WHY COMPLETE STREETS?

A portion of the funding for the Downtown Neighborhood Plan came from a federal Complete Streets grant. This planning effort is very relevant to forwarding Complete Streets, as the transportation system in Horace continues to expand and accommodate more traffic and alternative ways to get around, such as walking and biking. Downtown plans, such as this Plan, are very relevant to the tenets of Complete Streets, by considering a vibrant, active downtown with a mix of various traffic modes and ways to get around for everyone.

Complete Streets are a transportation policy and design approach that prioritize the safety and accessibility of all users of a street, including:

- Pedestrians
- Bicyclists
- Drivers/Motorists
- Freight Vehicles
- Emergency Services

Regardless of age or ability, *complete streets* aim to create streets that are safe, convenient, and comfortable for everyone.

The Downtown Neighborhood Plan incorporates *complete streets* as a critical, guiding component. The benefits⁴ of complete streets complement are shown below.



Safety

70% of complete streets projects resulted in crash reduction by decreasing accidents and injuries for all road users.



Quality of Life

People in walkable neighborhoods are nearly twice as likely to engage in adequate levels of physical activity, and less likely to have obesity. Active transportation is linked to greater



Economic Development

Complete streets can provide a 10 to one (10:1) return on investment by attracting residents, visitors, and boosting local businesses.



Community Character

Complete streets contribute to vibrant public spaces where people can gather, socialize, and enjoy their community, fostering a stronger sense of place.

⁴ <https://www.smartgrowthamerica.org/>

An aerial photograph of a suburban neighborhood, showing a central road lined with trees and houses. The image is overlaid with a semi-transparent green filter. The title 'BACKGROUND & CONTEXT' is written in large, bold, white capital letters across the upper portion of the image.

BACKGROUND & CONTEXT

This chapter of the Plan provides an overview of existing conditions to set the background and context of Horace. See Appendix A for more detail regarding the community's background and context.

DEMOGRAPHICS

Much of the demographic information reported in this chapter was collected from the U.S. Census Bureau’s American Community Survey (ACS) five-year rolling estimates for 2018-2022, the most recent available sample. In addition to population estimates, the ACS provides revealing information about people, households, and travel habits. Five-year ACS estimates are typically the most reliable socioeconomic data source, including the largest sample size and smallest margin of error. However, these data do not reflect the 2023 estimated population as gathered by the City’s *10-Year Population Outlook* and more recent growth. Long-term, demographics in Horace will likely change, as is typical for communities experiencing rapid population growth.

Age Composition

The age structure of Horace reflects a diverse population distribution with a notable concentration of younger individuals. About one-third (34%) of the population is under 19 years old. The age category of 0 to 9 years is particularly prominent, with 20%. The higher proportion of youth population is consistent with the large share of families and larger household sizes.

The 20 to 29 age group comprises 5% of the population, while the broader category of 20 to 49 years comprises 40%. This suggests that Horace has a healthy proportion of middle-aged adults, contributing to the community’s potential for growth.

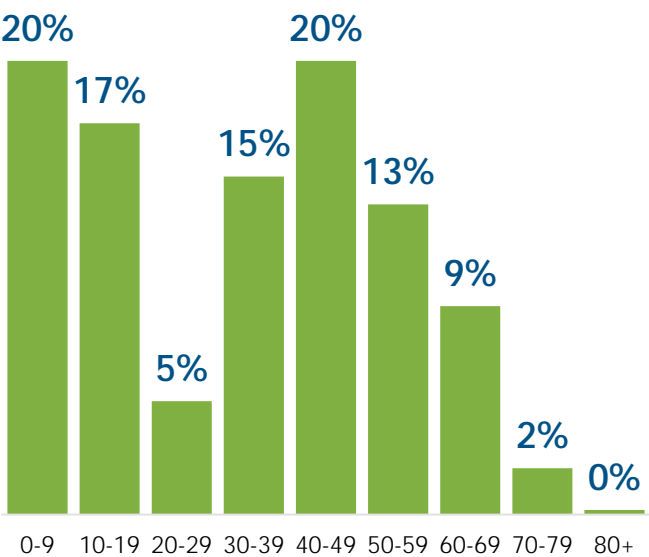
The city’s population aged 60+ years, makes up a relatively small proportion of the overall population, but is reflective of some of the longest-term residents.

36.3

Median age

Almost four years older than the median age in the Fargo, ND-MN Metro Area: 32.7; and slightly older than the median age in North Dakota: 35.4

Population by age range



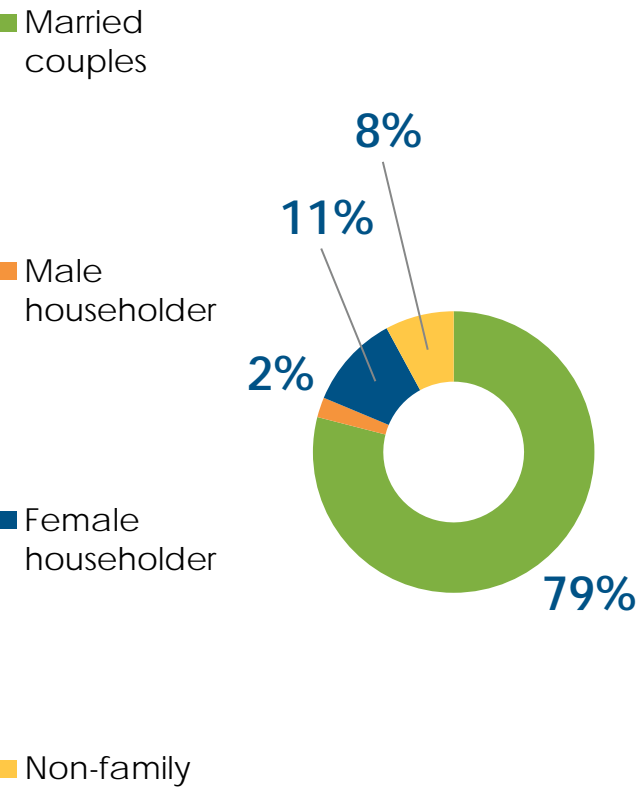
Households

Households include all people who occupy a housing unit, such as a single-family home or apartment, as their place of residence.

Married-couple households make up the largest portion, accounting for approximately 79% of the total. A vast majority (69%) of married-couple households have children under 18.

Horace’s household demographics also includes a notable number of single-person households and a range of family structures. The presence of both younger and older populations suggests that Horace is a community with varying needs and dynamics.

Population by household type



1,240

Number of households

2.8

Persons per household

About 20 percent larger than the figure in the Fargo, ND-MN Metro Area: 2.3; and about 20 percent larger than the figure in North Dakota: 2.3.

SCHOOL ENROLLMENT

The higher proportion of youth population is consistent with the large share of families and larger household sizes. With growth, this has also put immense pressure on the public school system in Horace, as part of the West Fargo Public School District (WFPSD). As a result, between 2019 and 2022, WFPSD constructed a new Middle School and High school in Horace. The existing Elementary school was also expanded in 2022. It should also be noted that the enrollment area for schools in Horace also includes neighborhoods in south West Fargo.

As shown in the following figures⁵, the population living within each school's enrollment area aligns closely with projections of enrolled students. School improvements have been further catalysts for growth, making the community an even more attractive place to live.

Horace Elementary School

The enrollment projections for Horace Elementary School reflect a steady growth trend over the coming years. Starting with the 2023/2024 academic year, the enrollment was 450 students. This number is anticipated to increase to 546 students in the 2024/2025 school year, demonstrating a significant rise in elementary-age population.

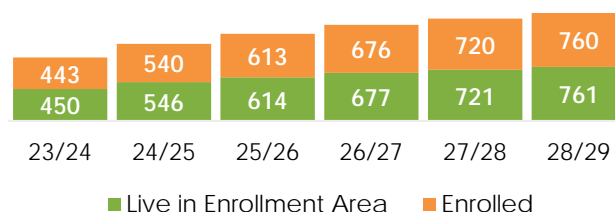
Enrollment is projected to continue growing, reaching 614 students in 2025/26, 677 in 2026/27, and eventually 721 students by 2027/28. The upward trend continues into the 2028/29 school year, with projections indicating an enrollment of 761 students as shown in **Figure 9**.

Figure 8. Aerial Image of Horace Elementary School



Figure 9. Enrollment Projections - Horace Elementary

Horace Elementary School Enrollment Projections



Current projections show that Horace Elementary School will be over operational and maximum capacity starting in the 2026/27 school year.

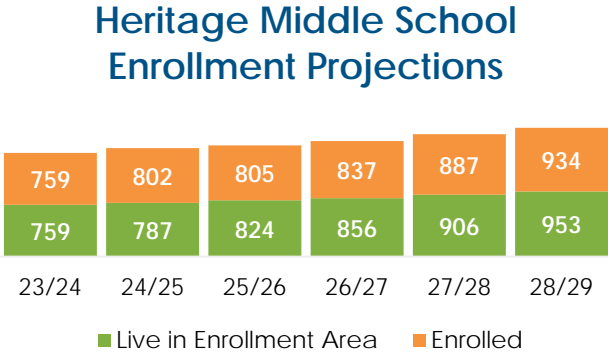
Heritage Middle School

Heritage Middle School is experiencing a steady increase in enrollment projections over the next several years as well. As of the 2023/24 school year, enrollment was 759 students. This number is expected to rise consistently, with projections of 787 students for the 2024/25 school year and further increases to 824 in 2025/26, 856 in 2026/27, 906 in 2027/28, and reaching 953 students by the 2028/29 school year as shown in **Figure 10**.

⁵ West Fargo Public School Schools. (2024). *Planning for the Future: West Fargo Public Schools, Enrollment Analysis*.

Current projections show that Heritage Middle School will be over operational and maximum capacity starting in the 2027/28 school year.

Figure 10. Enrollment Projections - Heritage Middle School



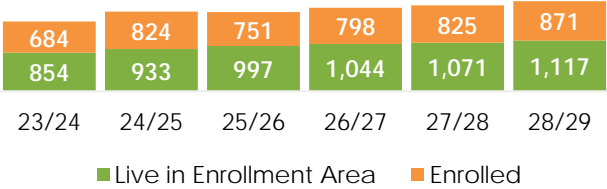
Horace High School

Horace High School enrollment projections show similar results to the other two schools. Starting with an existing enrollment of 854 students in the 2023/24 school year, the numbers are projected to rise significantly. For the 2024/25 school year, enrollment is expected to reach 933 students, followed by 997 in 2025/26.

Future projections indicate attendance will continue to increase, reaching 1,044 students in 2026/27 and climbing to 1,071 in 2027/28. By the 2028/29 school year, the projected enrollment is anticipated to be around 1,117 students as shown in **Figure 11**.

Figure 11. Enrollment Projections - Horace High School

Horace High School Enrollment Projections



2025 Bond Referendum

It should be noted that in February 2025, WFPSD voters approved a \$99.7 million bond referendum to finance projects in the next one (1) to five (5) years⁶:

- Completing/Expanding Heritage Middle School;
- Completing/Expanding Horace High School and adding a multipurpose room; and
- Other school district improvements.

HOUSING UNITS

Horace's housing market is competitive, with a strong demand reflecting a low vacancy rate, rapid growth, and higher property values.

1,294
Number of housing units

Housing unit production has kept pace with growth as shown in **Figure 14**. Based on City building permit data from 2013 to 2024, there was a consistent uptick in housing production starting in 2015. Between 2020 and 2021, housing production exploded by nearly 400%. There were 491 housing units permitted for

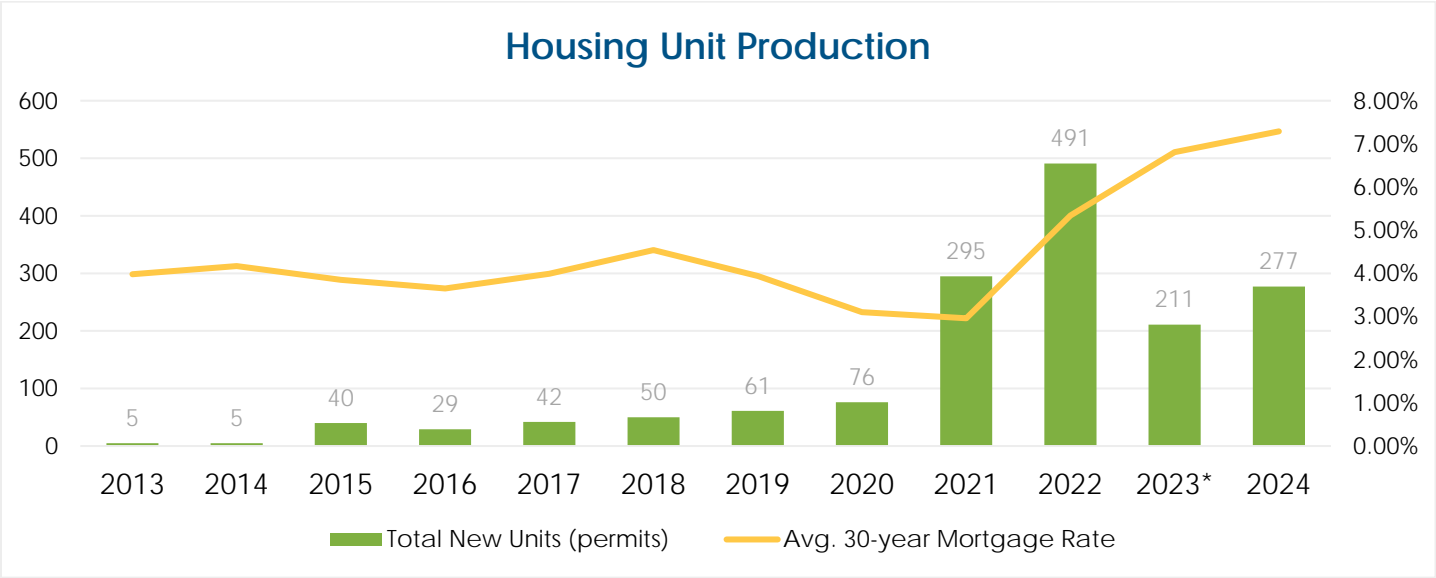
⁶ West Fargo Public Schools. (2025). *Question 1 Projects*. <https://www.west-fargo.k12.nd.us/about-us/2025-bond-referendum/question-1-projects>

construction in 2022, the highest on record for the city however, it is important to note the figure also includes 180- multi-family units.

Based on Census data, there are a total of 1,294 housing units in Horace. Out of the total units, 1,240

are occupied, indicating a high occupancy rate (95.8%) and low vacancy rate (4.2 %). A high occupancy rate suggests a high demand for housing in the city and a booming housing market in Horace; people want to live in the community.

Figure 12. City of Horace building Permit Data vs. Average 30-year Mortgage Rate



TRANSPORTATION

The multimodal transportation system plays a pivotal role connecting the community and providing access to employment, services, and amenities. Quality education, healthcare, housing, employment, and economic development are heavily dependent on a well-planned and maintained transportation system.

Horace is connected to multiple major locations in the FM area to the northeast. These connections are crucial as most Horace residents rely on the job market, medical facilities, entertainment, or other essential goods and services located nearby in the FM area.

Existing Street System

Horace’s existing street system is comprised of regional streets that connect the community to the broader FM area and local streets that provide access and connectivity within and between Horace’s neighborhoods. Due to the historically small population of Horace, and like most small towns across North Dakota, streets in older Horace neighborhoods have rural cross sections with stormwater drainage ditches (no curb & gutter), limited or no on-street parking, and no sidewalks.

As rapid growth has occurred, newer neighborhood subdivisions and roads have been built with urbanized cross sections including underground stormwater (curb & gutter), sidewalks, and/or shared use paths.

The main road in and out of Horace is CR 17 (Main Street), passing north-south through the city center, eventually providing connection to Interstate 94 and US Highway 10-Business Route in the city of West Fargo to the north. As the only existing major north-south route, CR 17 (Main Street) is a critical connection to the east-west regional network including 52nd Avenue S., 76th Avenue S. (CR 6), 100th Avenue S. (CR 14), and 124th Avenue S. (CR 16). 52nd Avenue S., 100th Avenue S. (CR 14), and 124th Avenue S. (CR 16) connect to Interstate 29 approximately two miles east of the city boundary. 76th Avenue S. (CR 6) provides a critical regional connection to 45th Street S. in Fargo and further connectivity to 64th Avenue S. (Fargo) and 52nd Avenue S. (Fargo).

Figure 14 displays generalized categories of the street network in Horace:

Regional: Typically, section line streets. Some urbanized with curb and gutter however most are rural cross section roadways. Regional routes are typically spaced 1-mile apart; however, the regional category identified on Figure 14 should not be used to determine current or future street function and design as not all can nor are planned to provide regional mobility/connection.

Urbanized: Typically local roads with curb and gutter, and sidewalks.

Rural: Typically local roads with drainage ditch, no curb nor gutter, and no sidewalks.

Platted: Typically local road right-of-way (ROW) which has been platted for new development. Horace's current standard is to urbanize street with sidewalks on at least one side.

Figure 14. Typical Local Road Cross Section - Older Neighborhood



Typical Local Road Cross Section – Newer Neighborhood



Figure 13. Street Categories

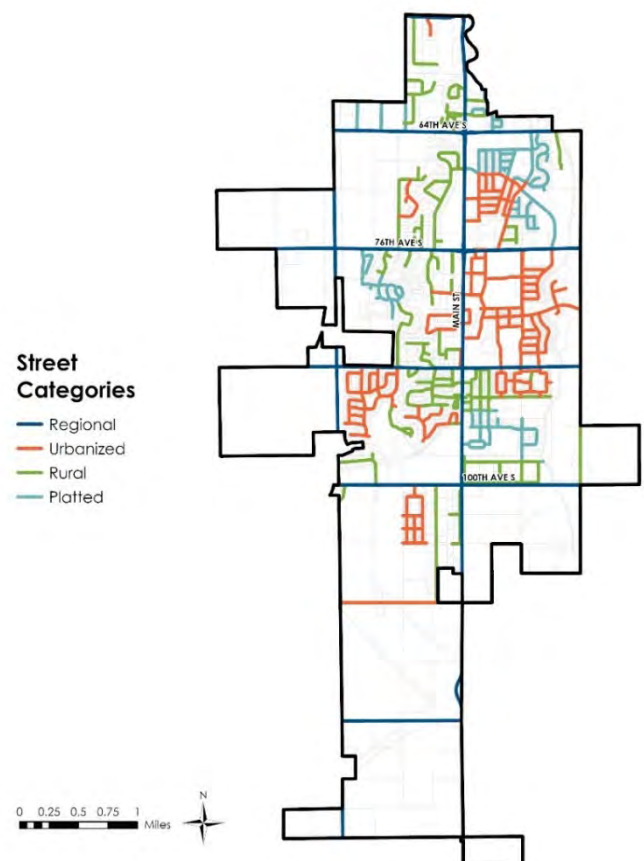


Figure 15. Mobility and Accessibility Characteristics of Classified Roads



As shown in **Figure 15**, higher-functioning roadways, such as arterials, support longer trips, provide greater mobility, and have limited access, serving as connectors between communities and regions. In contrast, lower-classified roads, like collectors and local roads, accommodate shorter trips, offer lower mobility, have more access points, and link to higher-classification roadways. Maintaining this balance is crucial for an effective transportation network. The mobility vs. access balance is critical in Horace, as north-south mobility is currently almost solely provided by Main Street (CR 17).

Functional Classification

All roadways have functional classification types such as interstate, arterial (major or minor), collector, and local roads. Functional classifications are determined by traffic volumes and travel purpose, and influence roadway characteristics such as posted speed, capacity, and design. In addition, roads are classified by ownership, facility type, and connection to the overall transportation system. Most of the roads in Horace are classified as local. Arterials and collectors are listed below and mapped in **Figure 16**. It is important to note that **Figure 16** functional classification is defined by Metro COG. The City of Horace has its own classification however, for the purposes of the Plan, Metro COG classification's are used, as they are tied directly to eligibility of federal funds.

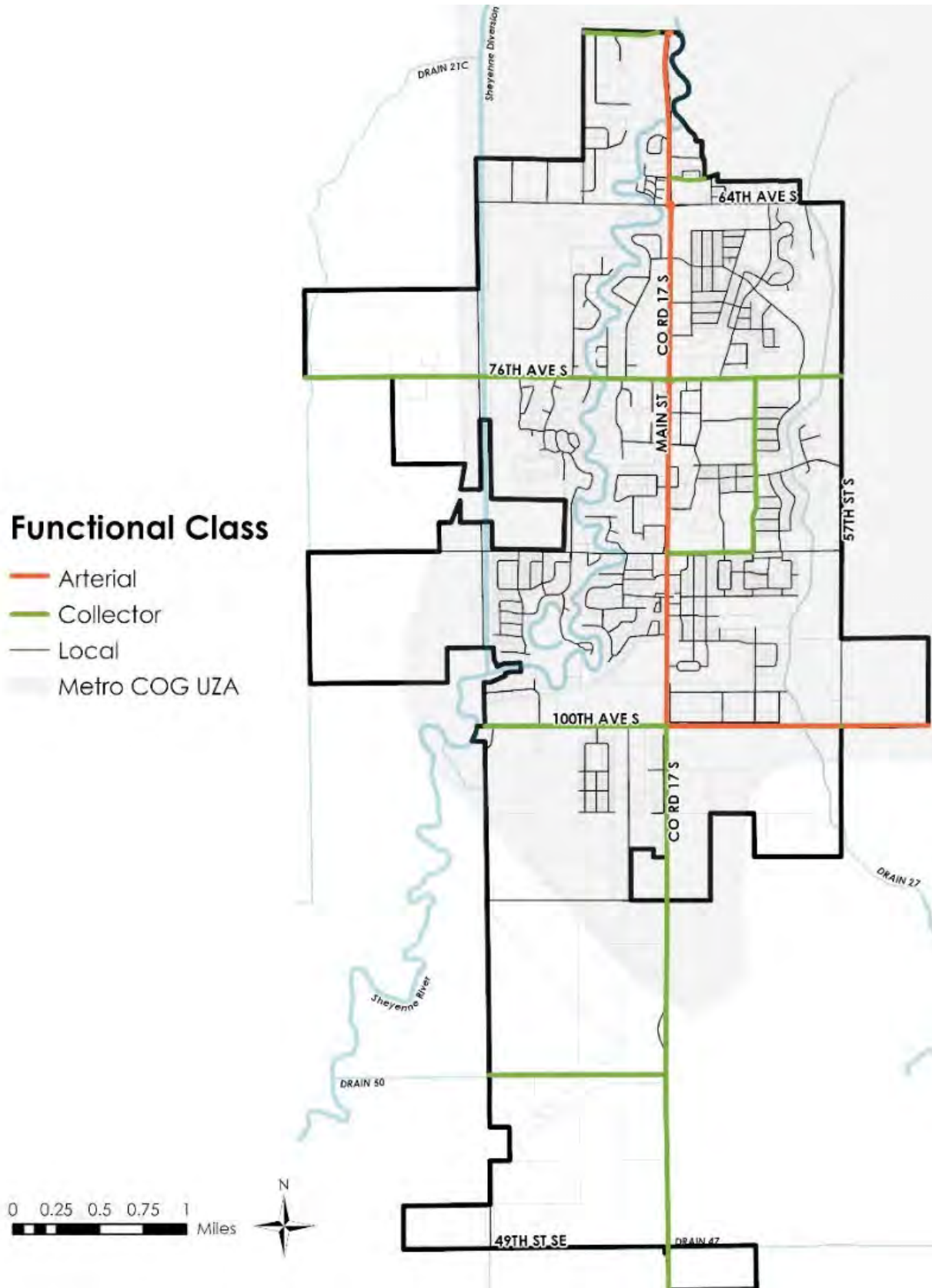
Minor Arterials:

- CR 17 north of 100th Avenue S.
- 100th Avenue S. (CR 14) east of CR 17

Collectors:

- CR 17 south of 100th Avenue S. (CR 14)
- 63rd Street S. between 76th Avenue S. and 88th Avenue S./Wall Avenue S.
- 52nd Avenue S. west of CR 17
- Deer Creek Parkway S. east of CR 17
- 76th Avenue S. (CR 6)
- 88th Avenue/Wall Avenue S. between CR 17 and 63rd Street S.
- 100th Avenue S. (CR 14) west of CR 17
- 124th Avenue S. (CR16) west of CR 17

Figure 16. Horace Roadway Functional Classification



Bicycle & Pedestrian Facilities

Existing Bike & Pedestrian Facilities

Recently, the City of Horace has prioritized improving multimodal infrastructure to accommodate people walking, biking, and rolling. Horace has made progress in building new sidewalks, shared use paths, and other multimodal improvements that help residents and visitors get around town outside of a motor vehicle.

As described above, most new neighborhood subdivisions in Horace include pedestrian or bicycle facilities. There are shared use paths and sidewalks scattered throughout new neighborhoods both east and west of Main Street/CR 17 as illustrated by **Figure 17**. Recently, the City has constructed shared use path facilities on both sides of Main Street/CR 17 north of Wall Avenue S./88th Avenue S. and along the south side of Wall Avenue S. west of Main Street/CR 17. Horace has successfully taken a phased approach to constructing bicycle and pedestrian facilities, expanding and extending critical connections over time.

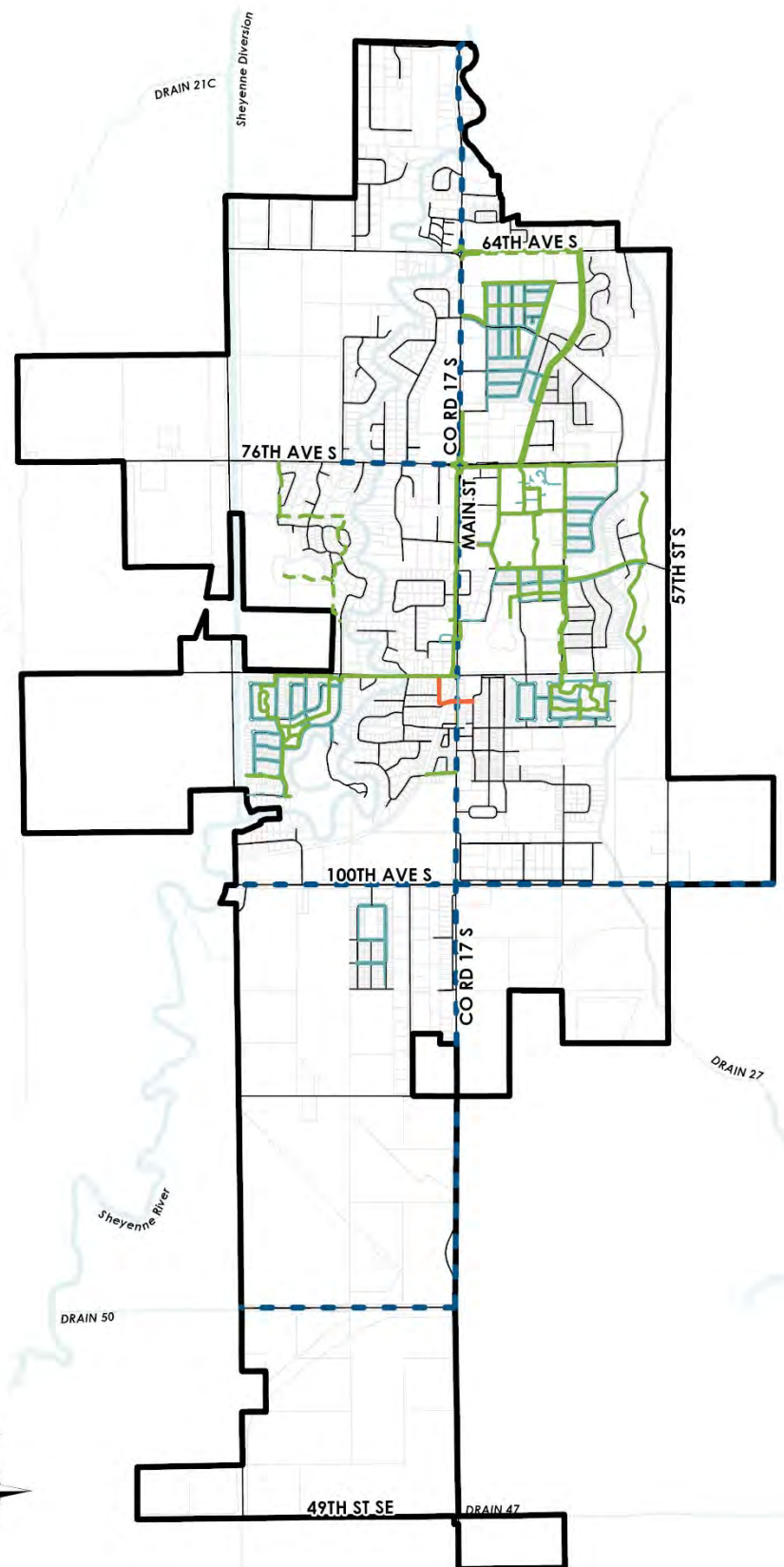
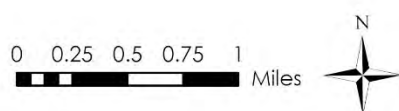
In 2023, the City took a unique approach to bicycle and pedestrian improvements in older neighborhoods without sidewalks or roads with urbanized cross sections: The Center Avenue/Thue Court designated walking lane project, also known as a yield street. The yield street provides demarcation of bicycle and pedestrian space on the roadway with dashed striping on both sides. When no pedestrians or bicyclists are present, vehicles can cross over the dashed lines and split the road. When pedestrians and bicyclists are present, cars must yield to them before crossing over the dashed lines (functioning as a single vehicular lane while preserving the space and safety of pedestrians and bicyclists on the roadway). The yield street connects across two older neighborhoods east and west of Main Street.

Metro COG identifies existing bicycle facilities or paved road shoulders wider than four feet along Main Street/CR 17, 100th Avenue S. (CR 14), 124th Avenue S. (CR 16), and west of CR 17 for one-half mile on 76th Avenue S. Typically, bicyclists and/or pedestrians who use these facilities are confident and comfortable riding or walking alongside vehicular traffic. Additionally, many residents in Horace utilize recreational modes of alternative transportation, including golf carts and ATVs, although there are few dedicated facilities for informal vehicles.

Figure 17. Existing Bike & Pedestrian Facilities

Existing Bike & Pedestrian Facilities

- Shared Use Path
- Sidewalk
- Designated Walking Lane
- - - Shared Use Path (under construction)
- - - Shoulder Width over 4-feet



Future Bike & Pedestrian Facilities

Figure 18 shows short- and long-term bicycle and pedestrian facility improvements, as developed through various regional and local planning efforts. The City is prioritizing trail and sidewalk connections to improve mobility and safety for bicyclists, pedestrians, and other users, understanding the infrastructure will expand options for travel and recreation. Currently, the City's top priorities include bike & pedestrian infrastructure along major roads (minor arterials & collectors), strategic neighborhood routes, and long-term off-street trails such as the Drain 27 trail on the east side of the community.

As listed by the City, current priority pedestrian segments include paved shoulders on 100th Avenue South, Riverdale Drive, and 48th Street Southeast.

Metro 2050, Metro COG's Metropolitan Transportation Plan, indicates broader bicycle and pedestrian projects, which include building shared use paths along Drain 27, facilities on either side of CR 17, facilities along Wall Avenue South, and a potential off-street facility connecting the FM Diversion to Horace. The potential, future off-street facility connecting the FM Diversion to Horace has been shown on the former Red River Valley and Western (RRVW) railroad spur. The alignment was identified and shown in the following plans:

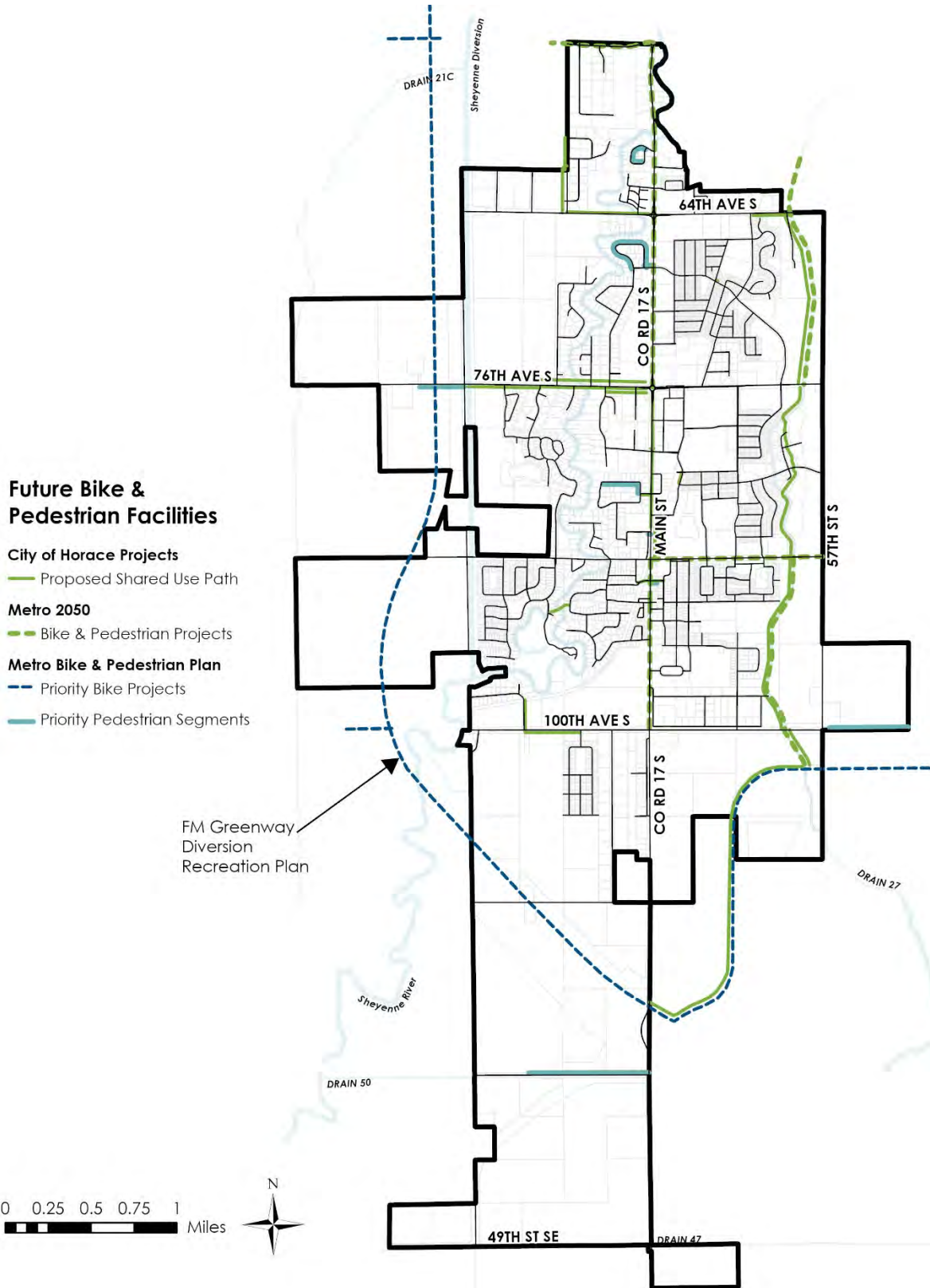
- Horace Comprehensive Plan (2019) – Figure 12-3
- Metro COG Bike and Pedestrian Plan (2022) – page 35
- Metro COG Metropolitan Transportation Plan (2025) – page 36 (see project #85)

However, through Plan development and discussions with the Metro Flood Diversion Authority (MFDA), the City's opportunity to purchase the right-of-way (ROW) necessary for the off-street trail alignment on the RRVW railroad has slipped away, as adjacent private property owners have been approached with the option to purchase the former

railroad ROW. The off-street path alignment will be reflected differently moving forward in the Plan so as not to interfere with private real estate transactions currently underway; therefore, the path alignment is not reflected in **Figure 18**.

The City of Horace's Capital Improvement Plan (CIP) bike & pedestrian facilities or projects mostly include improving pedestrian safety and constructing 10-foot-wide shared use paths to increase connectivity across the community.

Figure 18. Future Bike and Pedestrian Facilities



LAND USE

Existing Land Use

Within Horace city limits, all properties are categorized based on existing land use, reflecting how each property is currently utilized. The land use pattern is always evolving and is shaped by city zoning.

Residential Uses

Residential land use constitutes approximately 15% of the city and is primarily depicted in shades of yellow and orange on the Existing Land Use Map (Figure 25). The residential land use of Horace largely consists of “Suburban Residential” areas, along with other developments of varying densities, including mobile home parks.

Commercial and Industrial Uses

Commercial and industrial land uses together comprise a smaller portion of Horace. The commercial land, marked in red, is primarily concentrated along CR 17 in the “Old Town” area and between 76th Avenue S. and Wall Avenue/88th Avenue S. The “Old Town” area extends from the intersection of Main Street/CR 17 and Wall Avenue/88th Avenue S., covering about a half mile north and south, and about one eighth mile east and west of Main Street/CR 17. Established industrial uses, marked in purple, are located along the north side of 100th Avenue and east of CR 17.

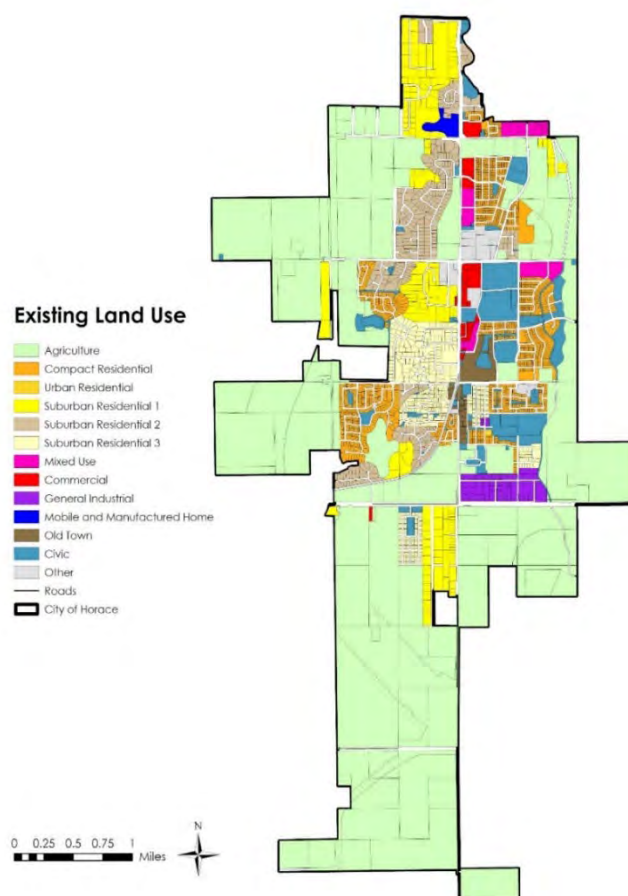
Agricultural Area

The most significant existing land use category in the city is agricultural land, which accounts for roughly 65% of the area. This agricultural area encircles the developed regions. This area is primed for development with the completion of the Red River Diversion.

Additional Uses

Additional uses are mostly located east of Main Street/CR 17, diversifying the land use as Horace continues to see development.

Figure 19. Existing Land Use



Future Land Use

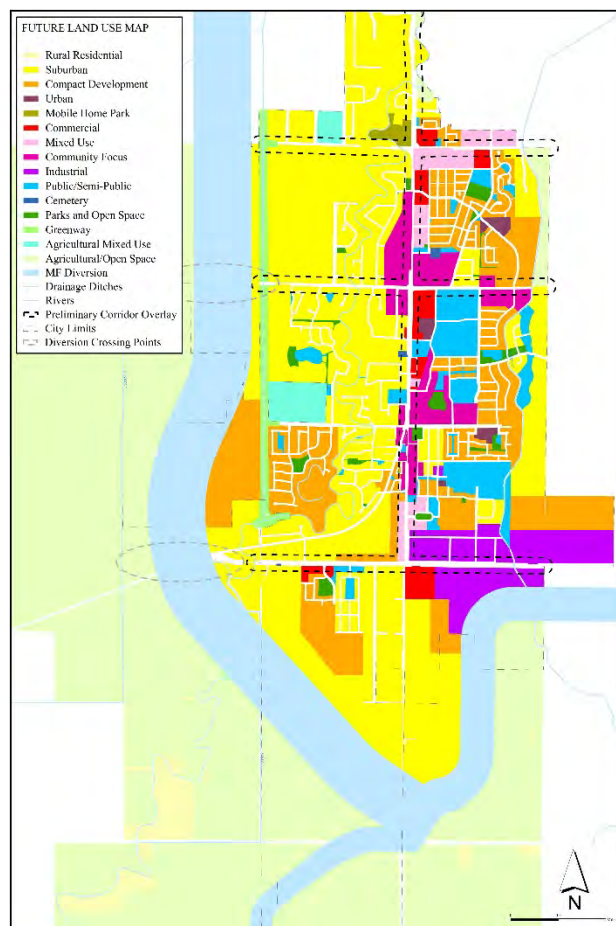
The Future Land Use Map (Figure 20) for Horace outlines various land use categories, each reflecting the community's vision for development. The same map is utilized in the 2045 Horace Comprehensive Plan, adopted in 2020. The comprehensive plan and future land use map guide growth and provide the foundation for the city's official zoning map. In the comprehensive plan, the main considerations to establish future land use are environmental conditions, historical growth patterns, and public service availability.

Since adoption of the 2045 Horace Comprehensive Plan, the city has faced enormous growth and development pressure. City staff and policymakers have identified several areas of the comprehensive plan and future land use map that may not align

with current community values. Each future land use category has a unique purpose and guides the density/intensity and scale of development. Therefore, it is important to consider how the future land use map correlates to the number of housing units and commercial/industrial floor space. Categories should be located and “right-sized” according to Horace’s unique growth needs and market opportunities. It is also important to consider how different areas interact with one another. For example, strip commercial development along CR 17 or large mixed-use nodes north of downtown could detract from opportunities within downtown.

Minor changes to the adopted future land use map have occurred over time to support development opportunities that meet City goals. However, the City is considering a more thorough update to the comprehensive plan in the near term.

Figure 20. Future Land Use



Future Land Use Categories:

- **Agriculture/Open Space:** Rural areas focused on agricultural uses, receiving minimal services. Density: 1 unit/acre.
- **Agriculture Mixed Use:** Traditional agricultural and compatible commercial uses, with limited services. Density: Up to 1 unit/acre.
- **Urban Residential:** Features higher-density housing, such as apartments, and small commercial spaces. Density: 5-14 units/acre.
- **Compact Residential:** Includes various housing types and small commercial uses, with a focus on open space and transitions to lower densities. Density: 3-5 units/acre.
- **Low Density Residential:** Predominantly single-family homes, maintaining a low-density character. Density: 1-3 units/acre.
- **Rural Residential:** Low-density, single-family homes in agricultural areas, with minimal public services. Density: Up to 1 unit/acre.
- **Mobile Home Park:** Existing mobile home parks within the city.
- **Mixed Use:** Combines residential and commercial uses in flexible designs, promoting walkability. Density: Up to 14 units/acre.
- **Public/Institutional:** Publicly owned lands for schools, utilities, and governmental facilities, subject to local review. Density: Varies.
- **Commercial:** Retail and office spaces located near major roads and existing commercial areas.
- **Community Focus Areas:** High-density residential and commercial uses with design guidelines for compatibility. Density: Up to 14 units/acre; FAR: 0.5-2.0.
- **Industrial:** Areas for light manufacturing and warehousing, including some commercial uses.

- **Diversion Channel:** Areas related to the F-M Diversion Project, maintaining existing uses with potential greenway concepts.
- **Greenways:** Buffer zones along rivers and drainageways, with specific zoning requirements.
- **Park and Open Space:** Designated for recreational use, with locations determined based on community needs.

around a large public open space. Single-family residential and limited high density residential uses are also proposed.

Partial platting of the subdivision has occurred but no infrastructure or development has been constructed.

Development Near Downtown Horace

Development is occurring across the city, including new single-family homes, multi-family housing, commercial businesses, institutional and civic uses, and industrial activities. Two developments are near the Horace Downtown Plan focus area: Lakeview Addition and Sparks Addition.

Lakeview Addition

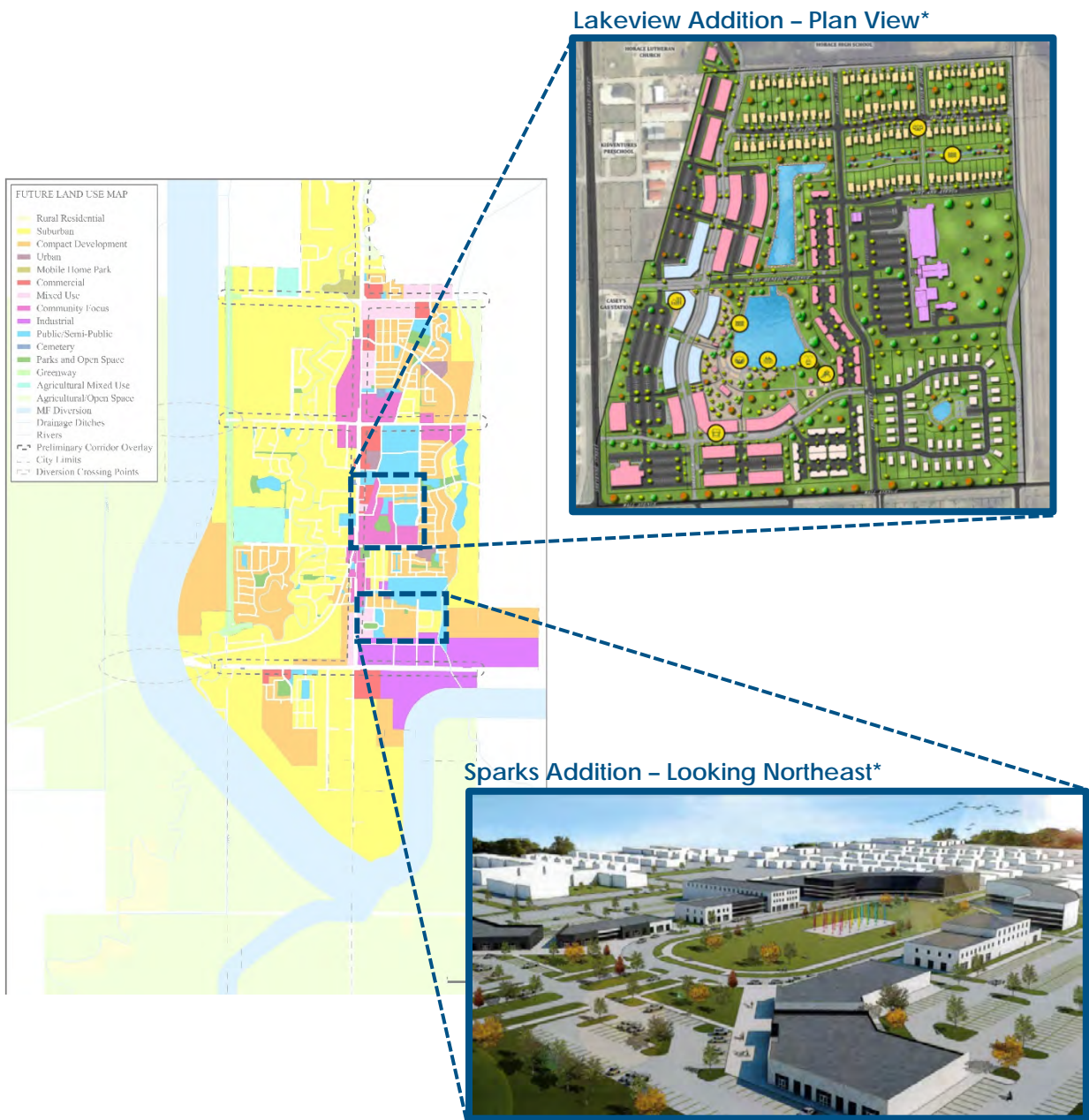
As shown in **Figure 21**, the Lakeview Addition proposes a new commercial and mixed-use town center district centered around a large stormwater retention area and the St. Benedict's Catholic Church. Lakeview Drive is proposed to provide a pedestrian-focused commercial thoroughfare. Lakeview Addition also provides single-family residential on the north and southeast sides of the area, with limited high-density residential proposed near the northwest corner of the intersection of Wall Avenue S. and Front Street.

Some of the property has been platted but development has been limited to the single-family neighborhood on the north and the St. Benedict's Catholic Church property.

Sparks Addition

Sparks Addition is proposed south of downtown, just north of the industrial area along 100th Avenue S. The developer proposes commercial, mixed-use, and civic uses along the east side of CR 17 and centered

Figure 21. Planned and Developing Areas near Downtown



*Both additions provide conceptual renderings to convey general ideas about each; not to be used for development or construction standard.

Figure 22. Downtown Adjacent Development

Figure 24. Parks and Natural Resources

RECREATION & NATURAL RESOURCES

Parks

The Horace Park District, established in 1972, plays a vital role in managing the city’s parks and summer recreation programs. The park system includes three (3) large Community Parks and five (5) smaller Neighborhood Parks, providing diverse recreational opportunities for residents. See **Figure 24**.

Table 2. Horace Community Parks

Park	Acres	Description
Freed Park	4.28	Remodeled in 2024. Features a baseball diamond, batting cages, playground, pickleball courts, basketball hoop, outdoor shelter, concessions, picnic area, and restrooms.
Horace Lions Park	1.98	Offers playground equipment and baseball diamond.
Meadowlark Park	5.89	Includes a playground, lighted hockey rink, warming house, picnic shelters, junior soccer field, youth baseball/softball field, tennis court, basketball court, connected multi-use paths, and a gazebo.

Figure 23. Freed Park



Recreation & Geography

- Park
- Stormwater Drains
- Rivers
- Red River Diversion Centerline

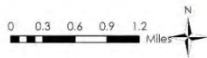


Table 3. Horace Neighborhood Parks

Park	Acres	Description
Adelman Park	0.37	A small area with playground equipment and basketball hoop.
Arrowwood Park	0.30	Features playground equipment and picnic table.
Independence Park	3.19	Contains a horseshoe pit, playground equipment, and shelter.
Maple Grove Park	1.57	Offers a sitting bench with a pond view.
Southdale Farms Park	0.39	A small area with playground equipment.
Willow Court Park	0.13	A small area with playground equipment.

Natural Resources

The Sheyenne River is a vital resource that flows through Horace, adding to the city's unique character and ecological diversity however, like most waterways in the Red River Valley, the Sheyenne River is prone to flooding. Spring flood events are common in the Red River Valley cause by snowmelt. Because the valley is so flat, as rivers like the Sheyenne River rise with snowmelt, the water breaks the banks and flows significant distances. Floodwaters from overland flooding can stretch for miles, given the extremely flat topography.

Historically, most of Horace was in the 100-year flood plain but in 1992, the United States Army Corps of Engineers completed a large flood protection project, the Sheyenne River Diversion. The Sheyenne River Diversion provides permanent flood protection for the City of Horace and City of West Fargo to the north. The Sheyenne River control structure operates by diverting rising water levels to the Sheyenne Diversion channel which flows north around Horace and West Fargo on the western municipal boundaries.

The Sheyenne River is considered a riparian greenway and plays a key role in filtering pollutants, mitigating floodwaters, providing habitat for wildlife, and providing opportunities for recreation. Within the city, riparian areas also include 'lost river' formations including old oxbows where the Sheyenne River once flowed, but no longer do, due to change in course. The riparian greenway is evident in Horace by the dense vegetation and mature tree canopy. Horace's riparian areas also have higher land values than the surrounding plains and are seen as an amenity for private development. Most private development along the Sheyenne River in Horace occurred after completion of the Sheyenne River Diversion in 1992. However, there are a few naturally elevated areas along the river that were developed prior.

Public access to the Sheyenne River is limited due to private ownership however, flood control and stormwater infrastructure provide opportunities for public open space. At the local level, stormwater infrastructure such as retention ponds provide walking paths and open space. Regional infrastructure, such as the Sheyenne River Diversion and Drain 27, provide opportunities for regional trail connections to West Fargo, Fargo, and beyond. Furthermore, Horace is in proximity to the Red River Diversion channel, currently being constructed to protect the entire FM region from flooding.

As outlined in the Red River Diversion Recreation Plan, Horace could connect to 30+ miles of continuous trail, super-structures such as the channel inlet structure or Sheyenne River aqueduct, interpretive sites, and/or other natural areas that may be associated with the multi-billion-dollar regional flood control project. See **Figure 30**.

ECONOMIC DEVELOPMENT

Horace has an informal business development group however, the group has not met since 2022. *Horace 2045*, the City's comprehensive plan, includes goals and policies adopted by the city to support economic development in town. The Economic Vitality chapter of the plan lists goals to

- Enhance economic prosperity and improve quality of life in Horace.
- Encourage City of Horace to have an active role in economic and business development

Furthermore, *Horace 2045*'s transportation goal, T-6, ties economic development to the multimodal transportation network in Horace:

Promote transportation projects that support regional economic goals, support freight movement, and promote projects that can be financially sustained for the long term.



HOW DO WE USE THIS PLAN?

The Downtown Neighborhood Plan is actionable, to be used in the following ways:

ESTABLISH POLICY



Policy refers to the City's official written guidance or official stance on a particular topic. *The Plan serves as an official guide for the future of downtown and multimodal connections*, to provide transparency and direction for future city leaders, residents, business owners, and visitors.

IDENTIFY & PRIORITIZE IMPLEMENTATION STRATEGIES



Clear implementation steps help the city's vision, goals, and objectives included in the Plan become more *achievable*. Clear identification and prioritization of projects and other plan recommendations will *provide a step-by-step framework to put the Plan into action*.

PROVIDE GUARDRAILS



The Plan allows Horace to *sustain community identity and preserve the key things that make the community such a great place to live*. Public engagement and feedback, collected throughout development of the Downtown Neighborhood Plan is reflected in every section of the document. The publicly supported future vision for downtown Horace sets *guardrails or ground-rules for future development, setting expectations of what the community wants for Horace's future*.

Additionally, the Downtown Neighborhood Plan may be used to:

- Review robust public engagement results and public sentiment.
- Identify challenges and opportunities downtown and across the community.
- Understand community identity.
- Envision the future.
- Prioritize strategies and locations for implementation.
- Explore funding strategies.

PARTNERSHIP

The Downtown Neighborhood Plan was developed through a partnership between the Fargo-Moorhead Metropolitan Council of Governments (Metro COG) and the City of Horace to address development and transportation connections to and from Downtown and the neighborhoods surrounding it as the community grows. The goal of the Plan is to develop consensus around a long-term vision for the city’s core and identify implementation strategies and project priorities to further the vision while preserving what makes Horace such a desirable place to live.

TIMELINE

The Plan kicked off in Summer 2024 with an approximate year-long delivery schedule (Figure 25). The Plan was split into phases organized around answering the following questions:

- Phase 1 – What is the purpose of this plan for Horace?
- Phase 2 – Who do we want to be?
- Phase 3 – What could it look like?
- Phase 4 – What does this mean for our Town?
- Phase 5 – What is the implementation strategy?

Figure 25. Plan Development Schedule



PUBLIC ENGAGEMENT

Public engagement is a core part of Plan development, with very robust focus given to ensure community voices were heard and incorporated

into deliverables herein. Detailed public engagement summaries, including raw survey results are included in **Appendix B**.

Study Review Committee

The Plan development team worked with a Study Review Committee comprised of various professionals from both Metro COG and the City of Horace, residents, and appointed or elected officials. The SRC was involved through the entire duration of the project’s development and provided:

- Thoughts about overall Plan development and anticipated deliverables.
- Review of key materials and deliverables.
- Ideas to better engage Horace residents/stakeholders.
- Local perspectives and insight.
- Technical perspectives and insight.

Five (5) SRC meetings were held at critical milestones, covering the following topics:

SRC #1 – Kickoff

SRC #1 was the kickoff meeting for the project and established the project process and ensured that all SRC members and the project team (City of Horace, Metro COG, Consultant team) shared a common understanding of the goals for Plan development and desired outcomes. There was critical discussion regarding the public engagement and communication strategies that work best in Horace.

SRC #2 – Walk-Audit

SRC #2 included a walk-audit and golf cart tour of Downtown Horace, adjacent neighborhoods, other destinations, and multimodal transportation infrastructure. The walk-audit was used for the SRC to share challenges and opportunities for the Plan and to allow the Project team to gain a more thorough understanding of the downtown area and established characteristics of Horace.

Figure 26. SRC #2 Walk-Audit



SRC #5 – Final Concept & Strategies

SRC #5 was held to review public engagement round two (2) results regarding the three (3) downtown concepts and multimodal transportation strategies including strategic connections and intersections. The SRC discussed and suggested refinements of the final vision for downtown Horace and multimodal transportation strategies.

SRC #3 – Engagement Summary

SRC #3 shared public engagement round one (1) results including online survey results and the preliminary draft of the Community Snapshot document. Results and key themes were shared from the public engagement push conducted around Bean Days 2024. The SRC discussed takeaways and how people's responses could be used to develop a responsive, publicly driven Plan.

SRC #4 – Concept Refinement

SRC #4 was held as a focused workshop to go over preliminary concepts for the future of downtown and multimodal transportation strategies. The SRC discussed and verified each concept and strategy was in alignment with public engagement results from the first round, and suggested refinements to each ahead of public debut at the second round of engagement.

Study Review Committee (SRC) Members

Jace Hellman, City of Horace
Michael Maddox, Metro COG
Chelsea Levorsen, Metro COG
Joel Luing, City of Horace
Naomi Burkland, Horace City Council

Ron Erickson, Horace Planning Commission
Paige Shockman, Horace Park District
Rachel Lee, Citizen Representative

Round 1 Engagement

Public engagement Round one (1) included two significant efforts to collect feedback from Horace residents about the future of downtown:

- Online Community Survey #1
- Bean Days – In-Person Pop-Up Booths

Community Survey



September 3rd – October 1st, 2024



Online/City Hall



502 Respondents

Survey questions were centered around the present and future of Horace including questions about what is most important to preserve about the community, community character, community values, and current and desired ways to get around.

Bean Days Pop-Up



September 6th, 2024



Vendor Fair & Kids' Fest



300+ Participants

At Bean Days, the project team participated in the parade to pass out project flyers and candy, and hosted two (2) booths at different events:

- Vendor Fair on Main Street
- Kids' Fest at Meadowlark Park

The all-day event allowed the project team to talk to residents and visitors about the Downtown Neighborhood Plan, ask key questions, and point people to the community survey to provide additional feedback. Questions asked at the Bean Days pop-up events paralleled several questions asked in the community survey.

Figure 27. 2024 Bean Days Parade – Downtown Plan Float

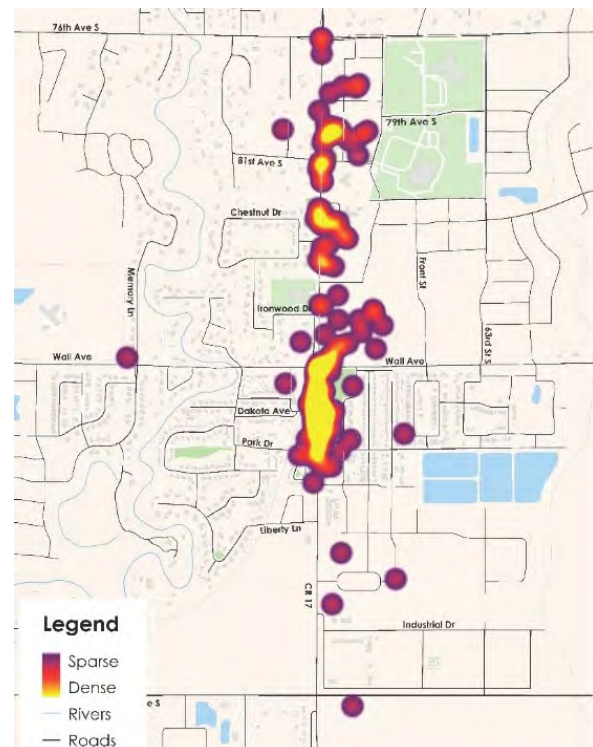


Round 1 Takeaways

WHERE IS DOWNTOWN?

With over 590 points placed between the Bean Days pop-up event and the community survey, most of the community believes downtown Horace is somewhere along Main Street, primarily focused between Park Drive and Wall Avenue:

Figure 28. Heat Map Results - Where is Downtown?



WHAT FITS HORACE BEST?

A visual preference survey allowed people to provide feedback on the downtown environments they believed fit Horace the best. The top four (4) preferences, shown in **Figure 29**, varied slightly by Beans Days and Online Survey participants.

The Online Survey included vision statements for the future of Horace. Results below (**Figure 30**) are split by how people answered the 'How long have you lived in Horace?' Of the 17 vision statements included in the survey, the top five (5) show overlap between residents both old and new.

Figure 29. Visual Preference Results - Bean Days vs. Online Survey



Figure 30. Visual Preference Results – New Residents vs. Established Residents



CURRENT AND DESIRED WAYS OF GETTING TO/FROM DOWNTOWN

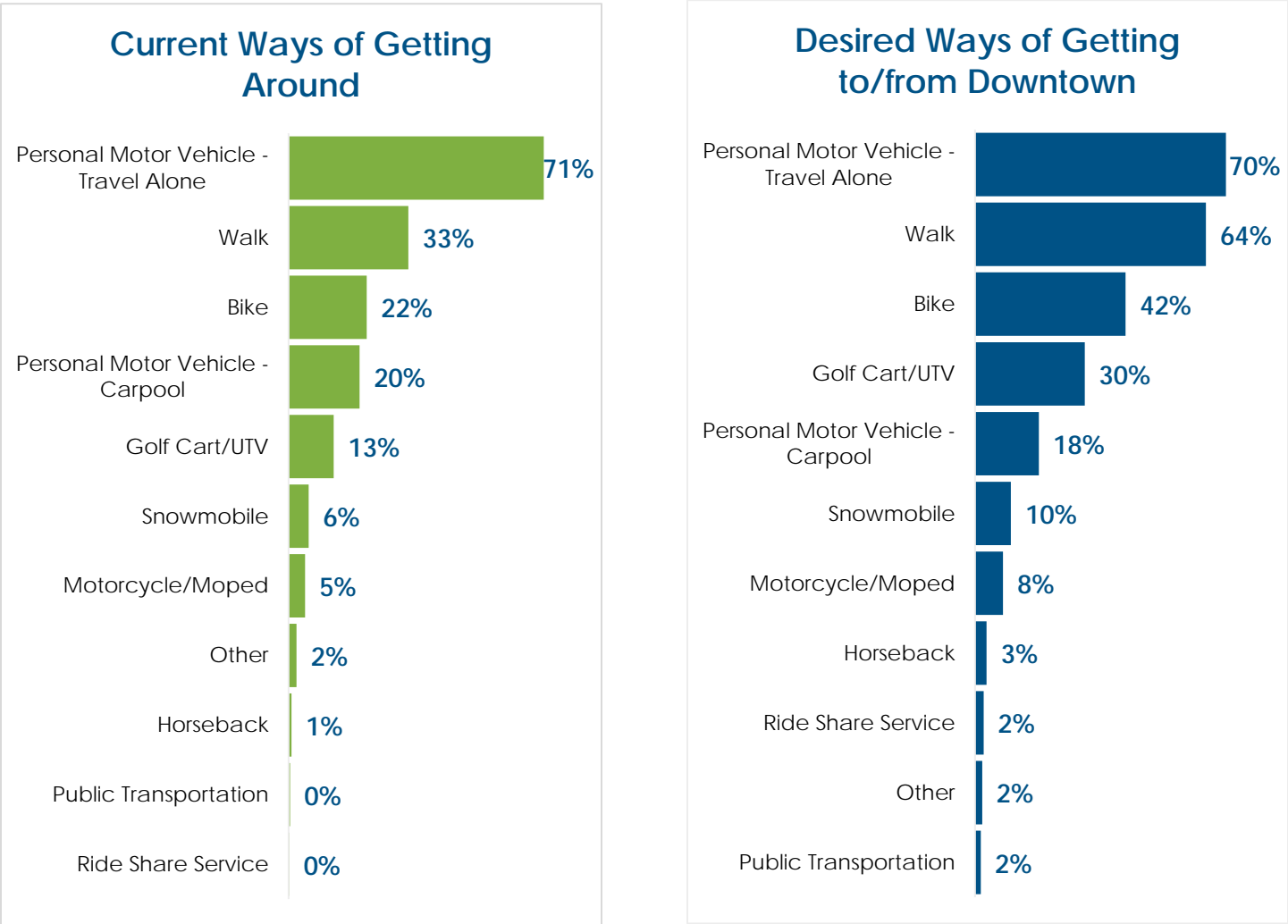
In the round one Community Survey, people were asked about how they currently get around town and then how they'd prefer to get to and from downtown in the future.

Comparing responses between current ways of getting around versus desired ways to get around, there is a strong desire by Horace residents for alternative travel modes such as walking and biking (see **Figure 31**). For example, walking increased by 31% and biking increased by 20% from current to desired ways of getting to and from downtown. The only desired travel mode that decreased was

personal motor vehicle showing a prioritization and desire of other ways to get around town.

42% of Round 1 survey-takers want Horace to be accessible by walking, biking, and driving, the third most supported vision statement behind small town and rural identity (out of 17 vision statements included in the survey).

Figure 31. Round 1 Survey Responses: Transportation Modes



Stakeholder Meetings

The project team conducted specific outreach to key stakeholders of the Downtown Neighborhood Plan. Stakeholders were identified as strategic partners in plan development and for implementation of future Plan recommendations. Additionally, property and business owners along Main Street were invited to participate in stakeholder meetings to discuss opportunities and challenges of the plan (**bold** names were met with). Stakeholders included:

- Horace Lion's Club
- Senior Center
- Big Erv's
- **Chelsey Johnson & Brandon Huseby**
- Hopewells Auto
- **Southern Valley Fire & Rescue**
- Dabbert Custom Homes
- **Dwyer Law**
- **Metro Flood Diversion Authority**
- **Horace Park District**
- **Cass County**
- Horace Elementary, Middle, and High School
- **North Dakota Department of Commerce**
- **North Dakota Department of Transportation**
- **Horace Special Assessment Commission**
- **Horace Engineering & Public Works**
- **Horace Planning Commission**

Summaries of stakeholder meetings can be found in **Appendix B**.

Round 2 Engagement

Public engagement round two (2) provided two additional ways to collect feedback from Horace residents about the future of downtown:

- Online Community Survey #2
- Public Workshops

The second round of public engagement for the Plan presented options and concepts for the future

of downtown, and future connections and multimodal transportation strategies.

Round 2 Public Workshops



January 22nd & January 23rd, 2025



Fire Hall Event Center



Approximately 150 Participants

In January 2025, the project team hosted two (2) separate public workshops for people to provide feedback on the concept-level strategies and recommendations for downtown Horace and community-wide connection strategies.

The two (2) separate workshop days and times provided people more flexibility and opportunity to participate. The Fire Hall Event Center was setup with various boards and feedback displaying three concepts for future downtown Horace, multimodal connection routes, multimodal intersections, and scale of development and infrastructure.

Figure 32. Public Workshops - January 2025



Round 2 Community Survey



January 22nd – February 18th, 2025



Online/City Hall



327 Respondents

Survey questions were centered around the concepts for future downtown Horace, multimodal connection routes, multimodal intersections, and scale of development and infrastructure.

Round 2 Takeaways

DOWNTOWN STRATEGIES

Three (3) separate concepts were developed for downtown Horace, based upon public feedback received in the first round of public engagement.

The Downtown Horace concepts all envision a future with a strong, appropriately-scaled commercial core in the community-identified “downtown” area. This strengthened core connects to planned commercial development in adjacent developments.

Each concept is designed to understand what residents mean by “small town”. Locations are shown at varying levels of transformation to help understand the public’s appetite for change. They can also be helpful to visualize how Horace might develop over time.

Figure 33. Concept 1 - Main Street Looking North



Concept 1

Concept 1 (Figure 37) celebrates the center of downtown as the hub of community-focused activities in Horace.

Concept 1 Includes:

Retaining a portion of the grain elevator
Farmer’s market event space
Green space for community events
Veteran’s Memorial
Parking on-street and on grain elevator lot
Minimal commercial development on grain elevator lot
Infill development within downtown
Retrofitting of existing buildings
Sidewalks connecting commercial core, community space, and senior center
Mid-block crossing to Freed Park
Mini roundabout at Wall Avenue and Main Street intersection
Parking and sidewalks on Wall Avenue
Main Street frontage stays as-is

Concept 2

Concept 2 (Figure 38) focuses on expanding commercial development in the heart of Horace's downtown.

Concept 2 Includes:
Reimagined "grain elevator" style building
Primarily commercial development
Small community space
Small Veteran's Memorial
Parking on-street and behind buildings on the grain elevator lot
Infill development within downtown
Retrofitting existing buildings
Sidewalks connect to surrounding neighborhoods
Mid-block crossings to Freed Park
Four-way stop at Wall Avenue and Main Street intersection
Main Street frontage road only has access from Ironwood Drive (access closure at Wall Avenue)

Figure 34. Concept 2 - Redeveloped Grain Elevator Block



Concept 3

Concept 3 (Figure 39) celebrates the agricultural roots of Horace in the central commercial hub by rebuilding a grain elevator-inspired structure and retaining its diagonal layout on the site.

Concept 3 Includes:
Dense commercial development on grain elevator lot
Downtown trail connection
Farmer's market event space
Parking on Main Street and behind commercial buildings
Infill development within downtown
Redevelopment of existing historic buildings
Sidewalk connections throughout downtown
Main Street frontage road eliminated and redeveloped
On-street parking on Main Street and Wall Avenue

Figure 35. Concept 3 - Main Street Looking North



Feedback on Downtown Concepts

Community feedback was collected at multiple public meetings and online surveys. The community preferred Option 1 with Option 3 being a close second. Generally, respondents were enthusiastic about the idea of a downtown but expressed some skepticism and concern about any redevelopment looking like Fargo or West Fargo. Key themes include:

- **There is a strong desire to preserve the small-town feeling of Horace.**
- **Residents want to resist the encroachment of a “city” feel.**
- **There is a strong desire to preserve green spaces and trees.**

There are mixed opinions about preserving or removing the grain elevator with strong proponents on each side. While some called it a dangerous and costly fire and rodent hazard, the majority preferred to keep some semblance of the grain elevator due to its iconic status. This comment reflects a popular theme:

“I think we should do our best to keep the grain elevator as it has become an icon of the town. I remember growing up here and thinking how cool it was as a kid. Maybe it can be repurposed into a museum or learning center? A farmers market area would be great to attract the locals and it would fit our character well...”

Other residents feel excited about the concepts and have a strong vision for the future of Horace:

- **Interest in balanced downtown development that includes commercial spaces and community areas.**
- **Desire for unique place-based building designs.**

“Consider bringing Seniors and Children together with family friendly park with children's playground equipment as bridge. Seniors and Children are a great natural fit. Kids have lots of energy Senior have lots of wisdom and patience. By placing a park between the grain elevator and the senior center it could be what ties all generations together and invites everyone to crossover. Working parents may arrange to meet a loved one at the senior center following a day of activities, they all go to park to watch kids having fun, then everyone walks downtown to grab a bite beer or root beer for the kids. Parents with older kids may leave them at the park with some sense of security knowing there will be watchful eyes nearby when people are at the senior center.”

A small minority didn't feel that a downtown was necessary:

“Horace does not need a “downtown”. Nobody wants expensive downtown stores. We need more fast food restaurants and a grocery store”

“We only want community assets not more businesses”

What should Horace's downtown be?

Most respondents supported retail expansion downtown with some restrictions and preferences.

- **Desire for a mix of building styles that reflect a small-town, rural feel.**
- **Some commentors want the downtown area closer to the schools.**
- **Limit downtown building height to one or two stories**
- **Emphasis on local businesses over chain businesses.**
- **Emphasis on attracting essential businesses. Many residents mentioned a grocery store and small hardware store.**

The community expressed several concerns that generally fit into two categories:

1. the high cost of special tax assessments, and
2. the fast pace of development / encroachment by Fargo / West Fargo.

TRANSPORTATION STRATEGIES

The transportation strategies were presented to identify priority locations for multimodal infrastructure. Transportation strategy locations were identified by proximity and each location's relationship not only to downtown Horace, but also to each location's potential for community-wide connections, between downtown and neighborhoods. Transportation strategies were organized by strategic:

- Routes (Figure 40)
- Intersections (Figure 41)

Routes & Intersections

Workshop participants and survey respondents prioritized routes and intersections from first or highest priority, to last or lowest priority. People were also asked to suggest routes or identify intersections which were not already identified. See **Table 5** for route prioritization results and **Table 6** for intersection prioritization results.

Table 4. Strategic Routes - Public Prioritization

Public Rank	Route ID #	Route Description
1	R.4	CR 17/Main St. north of Wall Ave.
2	R.5	Drain 27
3	R.1	Park Drive from Boxelder Cir.to Drain 27
4	R.2	Wall Ave. from Main St.to 57 th St.
5	R.7	Front St. from Wall Ave. to 82 nd Ave.
6	R.6	Cross-town connection (off-street) from 100 th Ave S. to Wall Ave.
7	R.3	CR 17/Main St. from south of 100 th Ave. to Park Dr.
8	R.8	64 th Ave. S. from Sheyenne Diversion Channel to 57 th St.
9	R.9	76 th Ave. S. from Sheyenne Diversion Channel to 57 th St.

Table 5. Strategic Intersections - Public Prioritization

Public Rank	Intersection ID #	Intersection Description
1	I.1	Main St. & Wall Avenue
2	I.2	Main St. & Ironwood Dr.
3	I.3	CR 17/Main St. & Lakeview Dr.
4	I.4	Main St. & Center Ave.
5	I.5	Main St. & Park Dr.
6	I.7	CR 17 & 100 th Ave. S.
7	I.9	Wall Ave. & Front St.
8	I.10	Wall Ave. & Nelson Dr./Future Lakeview Dr.
9	I.6	CR 17 & Liberty Ln.
10	I.8	CR 17 & 63 rd St. S.
11	I.11	CR 17 & Chestnut Dr.
12	I.12	63 rd St. S. & 79 th Ave. S.

INFRASTRUCTURE & DEVELOPMENT INTENSITY

Based on the first round of engagement, people frequently said Horace's small-town character is a key part of the community's identity. The second round of engagement further identified types of

varying intensities, asking: "What scale fits the character of Horace best?" See **Figure 36** for public response; there is a clear preference for the left-side of the scale, or lower intensity infrastructure and development. The exercise also helped people think about the relationship between land use development and transportation.

Figure 36. Infrastructure & Development Intensity Results



Figure 37. Downtown Concept 1



Figure 38. Downtown Concept 2

Downtown Concept #2



Figure 39. Downtown Concept 3

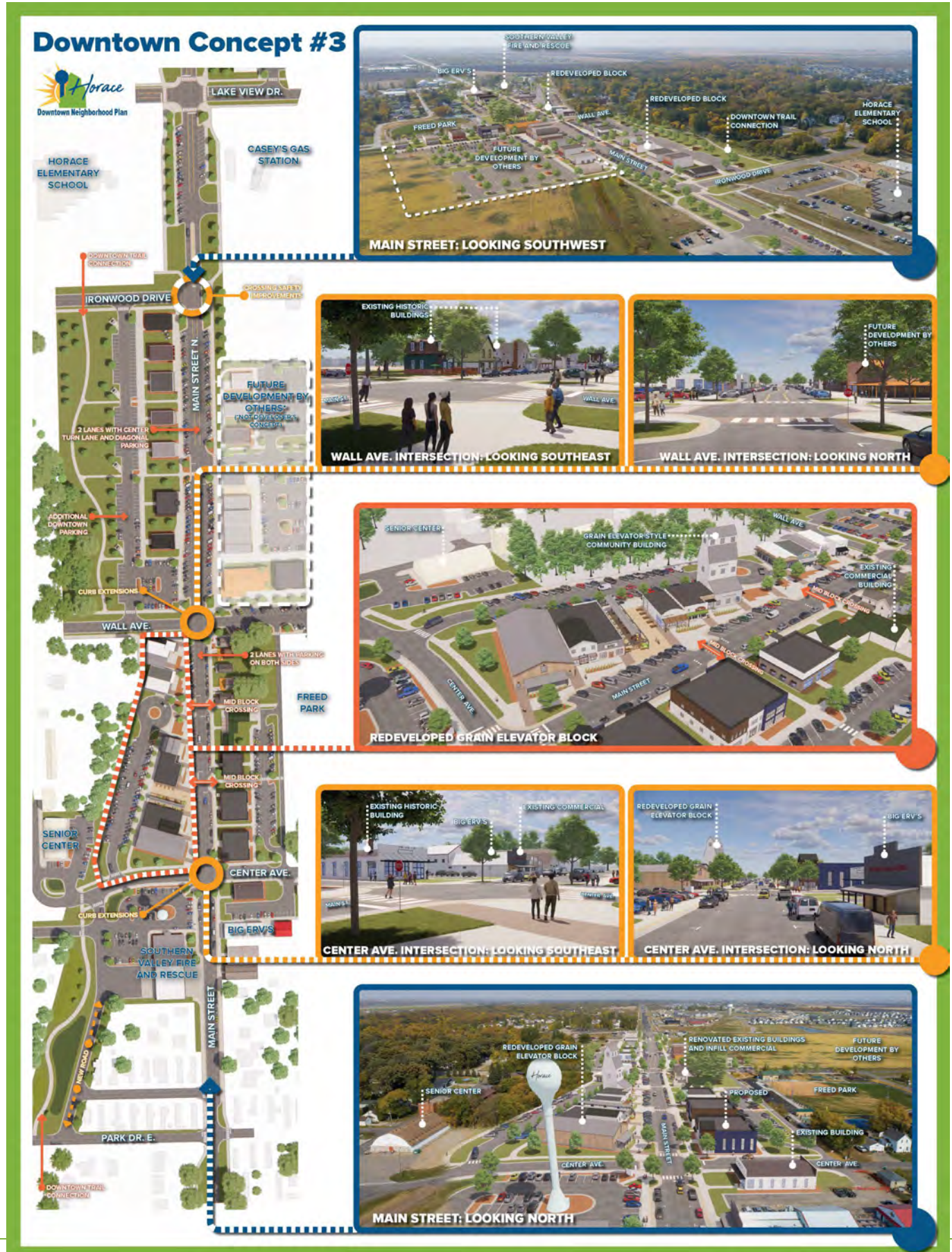


Figure 40. Multimodal Routes

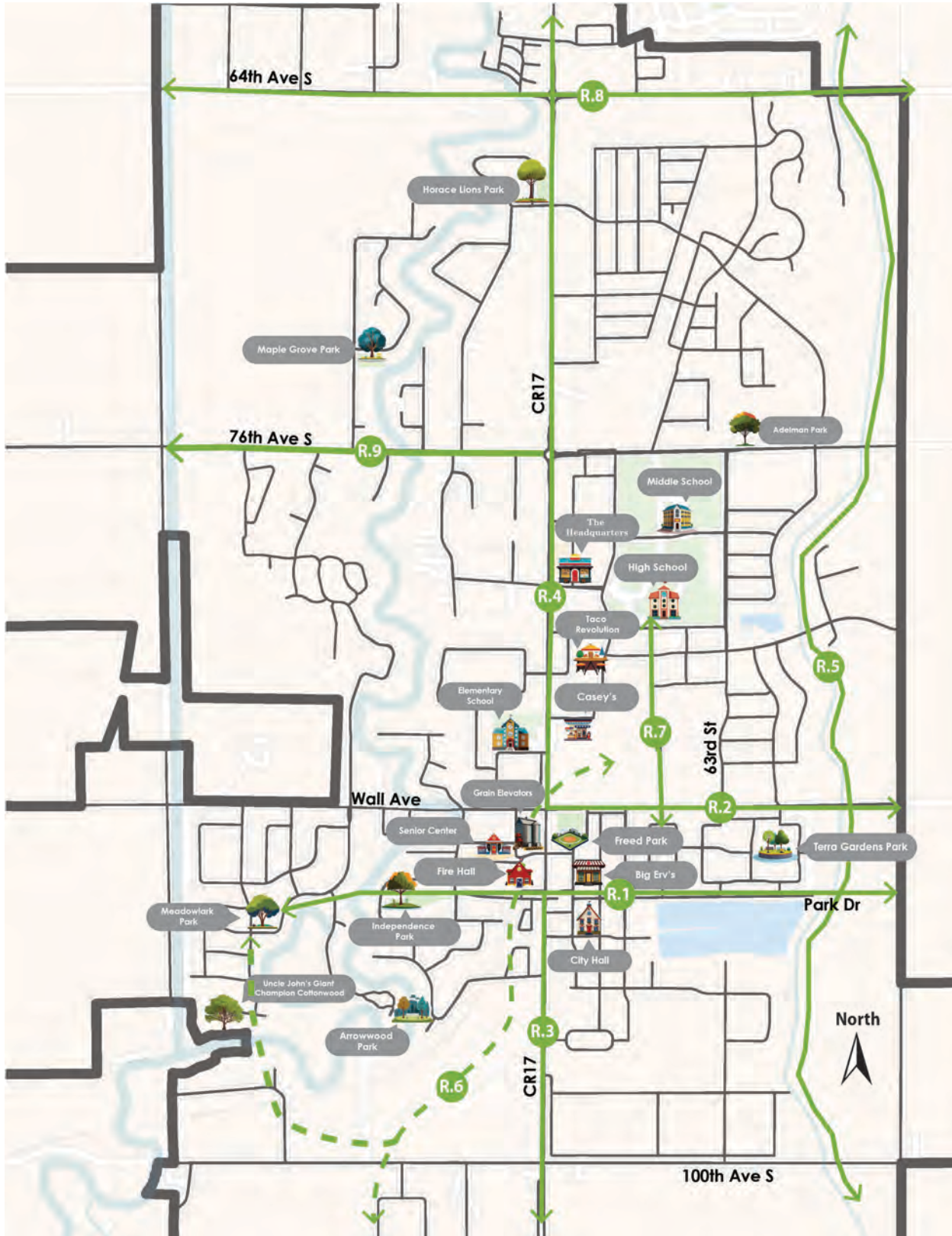
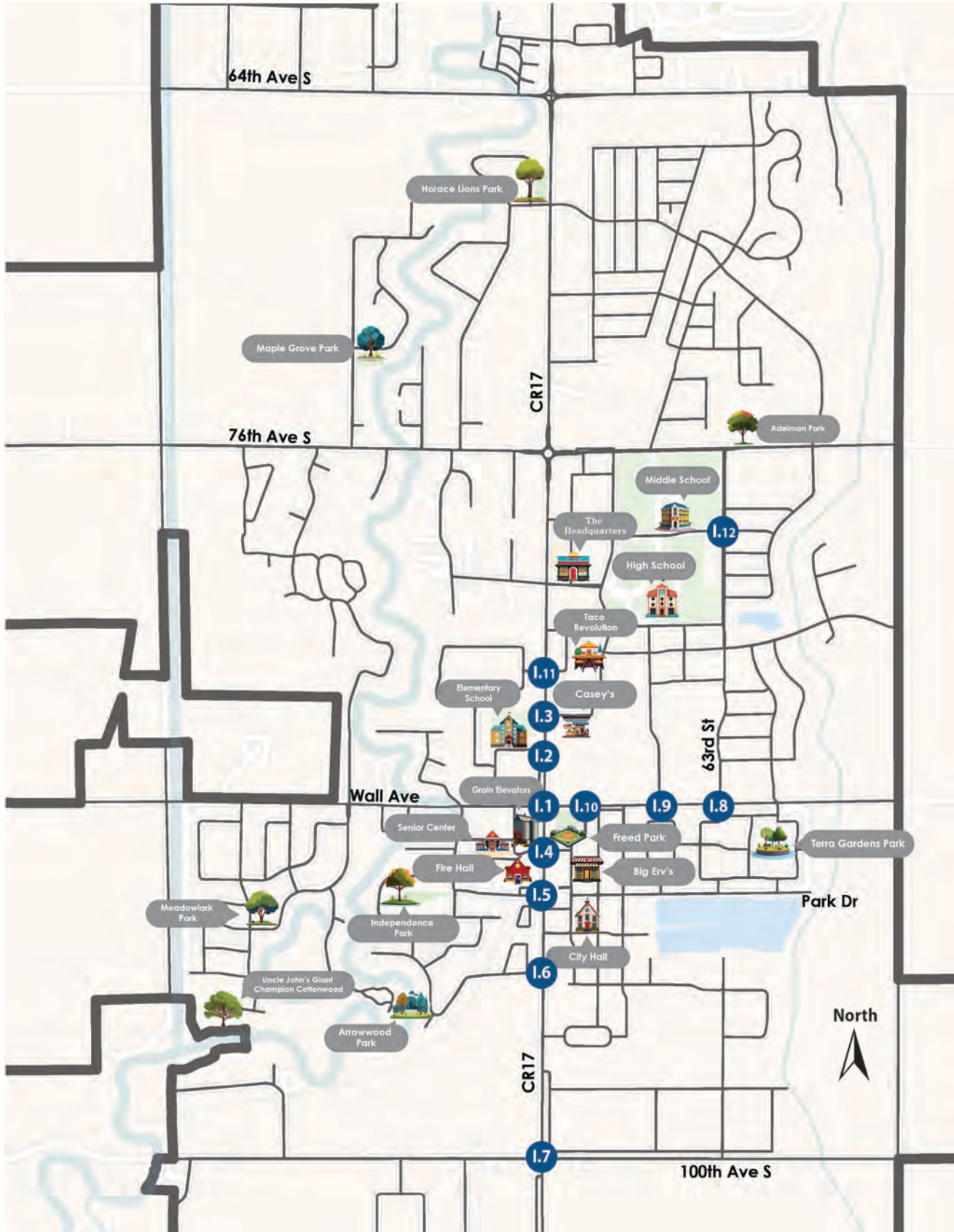


Figure 41. Multimodal Intersections



GUIDING PRINCIPLES

The following five (5) Guiding Principles were consistent themes of public engagement for the Downtown Neighborhood Plan, **as gleaned from the nearly 1,300 individual responses to engagement activities and surveys as part of the Plan's development.** The project team ensured that the strategies stayed true to Horace's identity by underlaying the five (5) principles in the Plan's goals, recommendations, and implementation strategies. Guiding Principles of the Plan describe the qualities that are most important to Horace community members – a small, active town with a high quality of life, and build the foundation for the city to manage and respond to growth and development in a clear, effective way that residents will be proud of.

WHAT IS SMALL TOWN CHARACTER?

Horace residents consistently associate small town character with lower intensity development and infrastructure. For example, and as also identified in Figure 36, on-street bike lanes, big box stores, and traffic lights are less desirable to the public's future small town vision for Horace. Less intense development and infrastructure was consistently preferred such as, bike paths, small commercial businesses, and stop signs.



Rural Small Town Identity

- ENSURE THAT THE TOWN'S GROWTH AND DEVELOPMENT MAINTAIN A SMALL TOWN CHARACTER.



A Community Accessible by Walking, Biking, and Driving

- FOCUS ON SAFETY FOR CHILDREN GOING TO AND FROM SCHOOL.
- STRENGTHEN SAFE CONNECTIONS BETWEEN NEIGHBORHOODS AND COMMUNITY DESTINATIONS LIKE SCHOOLS, PARKS, AND BUSINESSES.



Utility Improvements and Maintenance

- PRIORITIZE MAINTAINING EXISTING INFRASTRUCTURE OVER EXPANDING INFRASTRUCTURE TO ENABLE MORE DEVELOPMENT.



High Quality of Life

- MAINTAIN A SAFE AND QUIET RURAL COMMUNITY WITH STRONG SOCIAL CONNECTIONS AND RIGHT-SIZED AMENITIES.



Activities for Everyone

- INVEST IN AMENITIES THAT BRING PEOPLE TOGETHER, STRENGTHEN COMMUNITY TIES, AND MAKE HORACE A GREAT PLACE TO LIVE.

An aerial photograph of downtown Horace, Michigan, showing a grid of streets, various buildings including a church with a steeple, parking lots, and green spaces. The image is overlaid with a semi-transparent teal filter.

REALIZING THE PLAN

This chapter of the Plan provides the refined vision for downtown, recommendations, and strategies related to downtown, land use, and transportation. Realizing the Plan is organized by:

- Transportation Strategies, and
- Cultivating Downtown Horace (Downtown Strategies).

The chapter leads directly into the Implementation chapter.

TRANSPORTATION STRATEGIES

Transportation implementation strategies resulted from existing transportation system conditions and Guiding Principles as derived from public feedback. The Downtown Neighborhood Plan's transportation strategies are organized into three (3) distinct steps: (1) Adopt a Complete Streets Policy; (2) Develop Transportation Strategy Guidance including Street Typologies and Intersection Strategies; and (3) Equip Transportation Strategies including Multimodal Routes and Intersection Strategies.

Transportation strategies for downtown Horace are established to complement community members' desire for a more traditional small-town downtown, future development, and community-wide multimodal connectivity. There are several key locations to call out with a priority on multimodal infrastructure that accommodates people driving, walking, and biking.

A functional, multimodal transportation system that enhances quality of life and sustains Horace's character is no easy task. With limited space and funding, the City should focus on transportation elements that the community would like to see, based on feedback. The Downtown Neighborhood Plan explores the role streets play downtown and the City as a whole to develop more responsible corridor design. Also, there are other considerations the City should respond to:

- Not all streets can have dedicated bike and pedestrian infrastructure
- Streets will need to be cleared of snow (plowing) and maintained long-term
- Streets will need to balance the needs of truck traffic, emergency services, and the people who live and travel in the neighborhood

The following considerations provide a thoughtful framework for future transportation strategies in downtown Horace and across the city.

Future Growth & Existing Network

Multimodal strategies identified in this section may be used for new streets and the existing system however, for the existing street network, certain strategies may be more difficult to implement than on a new street with a blank slate. For example, not all existing streets may be able to reflect the vision of the downtown neighborhood plan because of real-world conditions and existing development. However, the strategies may be very beneficial for infrastructure that doesn't exist (such as bike & pedestrian infrastructure) and when considering options for street reconstruction and rehabilitation projects.

Operations & Maintenance

Envisioning a functional, multimodal street is one consideration but the City of Horace will also be responsible for long-term operations and maintenance. The transportation network will see regular street sweeping and snow removal therefore, all strategies must consider space for maintenance vehicles and snow storage.

Land Use Integration

Most often referred to as context-sensitive design, land use integration is the process by which transportation strategies may differ based on the surrounding context and development. For example, a downtown arterial street should be different from a commercial arterial street. Strategies developed as part of the Plan prioritize land use for multimodal infrastructure recommendations and street layout, to ensure future project(s) align with the need of surrounding neighborhoods.

Multimodal Accessibility

The Horace community has asked for more dedicated space to safely walk and bike however, there is validated concern about the future character of the city from community-members as explosive growth continues to drive change. There are simple ways to accommodate more bike and pedestrian mobility including, sidewalks, shared use paths, and improved crossings. Each may be appropriate depending upon the space and other network conditions such as vehicular traffic. Strategies provided herein provide safe accommodation tailored for the context and character of the city.

STEP 1

Adopt a Complete Streets Policy

As heard through public engagement opportunities, people in Horace wanted safe routes to walk and bike, even to school. Although the ability to walk to school is not required by the district's bussing policy, walking and biking to important destinations in Horace are hallmarks of small-town life. Complete streets are a major component of the Plan's Guiding Principles, prioritizing:

- Safety and accessibility for all residents
- Quality of life
- Economic development
- Small-town and rural character

To help the City implement the Plan and realize the community-supported vision for Downtown Horace and other Plan recommendations, a critical strategy involves crafting a Complete Streets Policy; **an official statement of the City about the importance of Complete Streets and how the City and community**

can implement a citywide Complete Streets approach.

A Complete Streets Policy for Horace will ensure that an approach is carried forward for future planning, designing, building, operating, and maintaining streets that enable safe access for all people who travel in the city; including pedestrians, bicyclists, motorists, freight carriers, and emergency services. The Complete Streets Policy should be tailored to Horace's character, practicality of implementation, and based on public feedback received through this Plan's development.

The following framework may be used by the city to tailor a right-sized Complete Streets Policy that aligns with the Downtown Neighborhood Plan vision and goals, and future projects citywide. The 10 policy components below are not requirements, but recommended for a clear, and strong policy. The components may be treated as an outline to a simple, straightforward policy document that should be 10 pages or less:

0. Introduction
1. Establish Commitment & Vision
2. Prioritize Vulnerable Users
3. Apply to All Projects & Phases
4. Allow Only Clear Exceptions
5. Outline Clear Coordination
6. Adopt Design Standards or Guidelines
7. Integrate Land Use & Future Land Use
8. Measure Progress
9. Set Criteria for Choosing Projects
10. Create a Plan for Implementation

An example of a strong Complete Streets Policy is provided in **Appendix C**.

Complete Streets Policy Framework

1. Establish Commitment & Vision

How and why does Horace need complete streets?

The answer is found directly in feedback and priorities established through community engagement. Include the intent of the policy to guide the design, construction, use, and maintenance of roadways, shared use paths/trails, and sidewalks in Horace to create a comprehensive, integrated transportation network that is safe, accessible, comfortable, accommodating, and welcoming to all users. This section of the policy should call out all users of the transportation network and all applicable transportation modes including pedestrians, bicyclists, motorists, commercial vehicles or freight, and emergency vehicles.

Describe the Vision:

- **Safe, accessible community for people walking, biking, and driving.**
- **Increase connectivity to neighborhoods, schools, and community destinations.**

2. Prioritize Vulnerable Users

Horace is home to active, young families and an aging population. All users and transportation modes benefit from Complete Streets improvements. Include a specific call out of vulnerable users of the transportation system in Horace as those who are at greater risk of serious injury or death when involved in a crash; and those who may experience the transportation network differently than others given their physical stature, ability, mode of transportation, or otherwise.

Identify Specific Vulnerable Users in Horace:

- **People walking, biking, or using other alternate modes of transportation.**
- **Younger and older residents.**
- **People with disabilities.**

3. Apply to All Projects & Phases

On what projects or streets does a Complete Streets policy apply? The answer is a holistic approach to apply complete streets to every project and place. Describe the application of the Complete Streets policy across transportation projects, to varying degrees but thoughtful in each. For example, spell out what activities should consider Complete Streets, where the policy applies, and when guidance should be based upon context. The Complete Streets policy should not allow a 'one size fits all' approach.

Describe How Complete Streets Applies:

- **Every transportation project**
- **Every place.**
- **Every place of work.**

Complete Streets Policy Framework

4. Allow Only Clear Exceptions

Complete Streets policies are comprehensive and apply to all streets and phases of projects. However, certain exceptions can and should be made as narrowly and clearly as possible. Examples of exceptions include, but are not limited to:

Provide Clear, Accountable Exceptions:

- On streets where specific users are prohibited (e.g. Interstate freeways, pedestrian malls, off-street trails)
- Cost is excessively disproportionate to the need and probable use.
- Emergency repairs that may require immediate, rapid response (e.g. watermain break, etc.) although; temporary accommodations for all modes should be made.

5. Outline Clear Coordination

Coordination is key and Horace isn't alone. Outline key coordination for private developers, City departments, partners, and other government agencies. Establish collaboration and identify coordination efficiencies to align and coordinate projects.

Specify Collaborators:

- Private Developers
- City of Horace Departments
- Cass County
- FM Metro COG
- State of North Dakota
- Horace Park District
- West Fargo Public Schools
- Utilities/Utility Companies

6. Adopt Design Standards or Guidelines

How does Horace go from policy to implementation? To bring the Complete Streets policy to life, planners and engineers will need to know how to use best practices to design streets, turning policy into pavement. Standards may include, as applicable, those included in key documents that focus on Horace's character and public feedback, and other literature that provides best practices and guidance.

Provide Guideline References:

- *Downtown Neighborhood Plan Transportation Strategy Guidelines.*
- *Horace 2045 Comprehensive Plan*
- *American Association of State Highway & Transportation Officials (AASHTO)*
- *American Planning Association (APA)*
- *Federal Highway Administration (FHWA)*
- *Institute of Transportation Engineers (ITE)*
- *National Association of City Transportation Officials (NACTO)*
- *U.S. Access Board*

Complete Streets Policy Framework

7. Integrate Land Use & Future Land Use

Horace's streets don't exist in a vacuum. The strongest Complete Streets policy acknowledges the integration of land use planning to synchronize the community's desires for living in Horace today and into the future. Provide high-level guidance on land use context and outline flexibility of strategy selection as determined by said context. Land use context is built into the Downtown Neighborhood Plan's transportation strategies and recommendations.

Provide a Context-Sensitive Policy:

- **Evaluate functional classification and future functional classification of streets.**
- **Establish street typologies that respond to functional classification and context such as existing and future land use.**
- **Consider land development code and zoning district impacts to the transportation network (e.g. multimodal traffic generation, off-street parking requirements, setback requirements, community character, etc.).**

8. Measure Progress

How will Horace know if a Complete Streets policy is working? The City should establish measures to evaluate performance, define who is responsible for tracking, and make progress clear to residents. Performance measures should be easy to track and will allow the City to evaluate progress of implementing the policy.

Identify Specific Performance Measures:

- **Number and severity of crashes**
- **Injuries and fatalities for all modes**
- **Emergency vehicle response times**
- **Number of students who walk or bike to school**
- **Length of shared use path/sidewalk network**
- **Public sentiment on development applications (i.e. is Horace getting**

9. Set Criteria for Choosing Projects

Horace has a process by which transportation projects are funded and built. Establish basic criteria to evaluate and prioritize projects based on how they forward the community's vision and goals for the future. Incorporate into current project prioritization and selection processes, provide flexibility for decision-making, and provide public transparency. Highlight how project decisions will be made and how projects will be evaluated.

Provide Project Prioritization Criteria:

- **Consider safety prioritized over mobility**
- **Prioritize vulnerable users**
- **Strike a balance, not all strategies for all travel modes can fit within the public right-of-way**

Complete Streets Policy Framework

10. Create a Plan for Implementation

A formal commitment to Horace's Complete Streets approach is just the beginning. A strong policy also spells out specific steps for implementation that will make an impact on what gets built and where. Outline high-level steps to implement the Complete Streets policy moving forward.

Facilitate Implementation:

- Establish a committee or identify implementation oversight.
- Revise or update related procedures, plans, regulations, and other processes to reflect the policy.
- Identify new design policies and guides or revise existing policies to reflect current state of best practices in multimodal transportation design.
- Promote public information and visibility of the policy through workshops, public events, or other training opportunities.

STEP 2

Develop Transportation Strategy Guidelines

People voiced concern about the cost of transportation/infrastructure in Horace and how future projects will impact the city's character. Strategy Guidelines will help the city right-size infrastructure, understand cost based on context, and respond to local character.

Future projects must respond to the needs and context of Horace's neighborhoods. For example, an arterial serving a strip-commercial development cannot look the same as an arterial serving downtown because the areas have different development context and serve different transportation users. For downtown Horace, a five-stage decision-making process defines the future function and character of streets in the study area and across the city. The process should be used to establish transportation strategy guidelines citywide:

Stage 1 – Development Context

Determine the desired or future development context. Transportation strategies must respond to the needs of the neighborhood, and understanding the future land use and potential development of the area is an essential first step.

Stage 2 – Identify Roadway Users

Determine the street's role in the larger network. Arterials, collectors, and local neighborhood streets have different functional purposes, therefore, must look differently.

Stage 3 – Street Typology

The results of Stage 1 and Stage 2 determine the street type. For the Downtown Neighborhood Plan, six categories are established and detailed below.

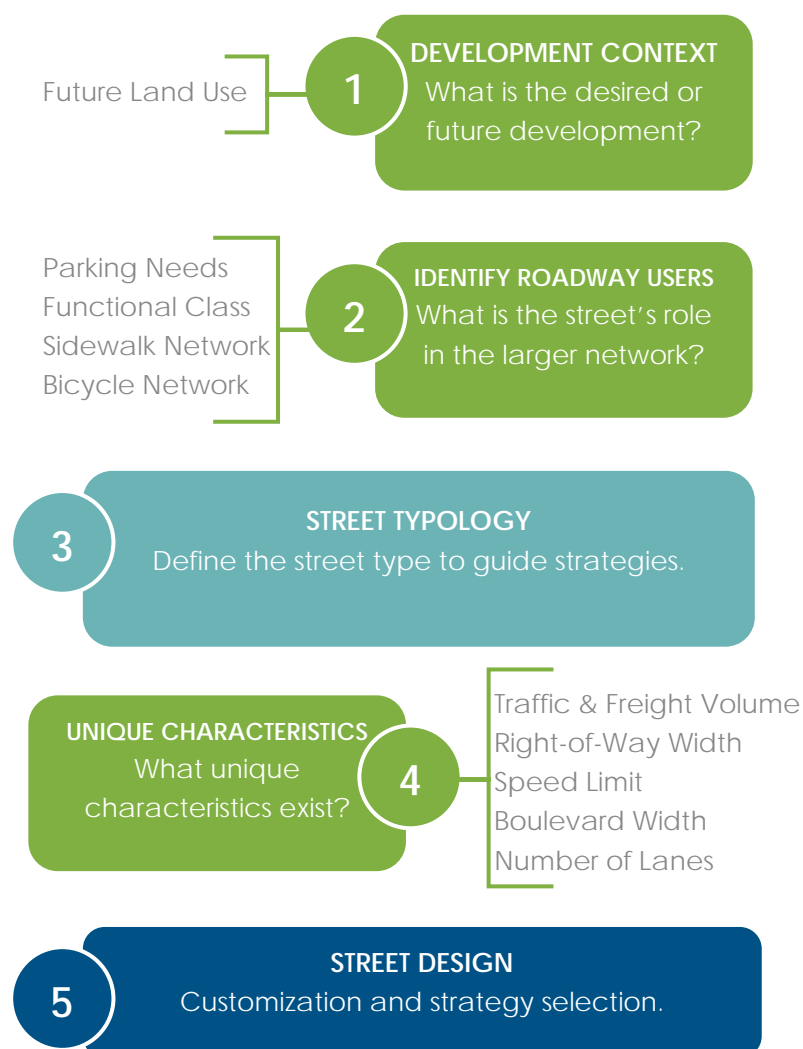
Stage 4 – Unique Characteristics

Determine unique characteristics for consideration. For example, what is the projected traffic volume? What about freight and commercial truck activity? How fast will vehicles travel? These characteristics will point to specific strategies for the street.

Stage 5 – Street Design

Customize design selections for strategic multimodal routes and intersections by responding to unique characteristics of the street and local context.





Figure 42. Transportation Strategy Guideline Stages & Considerations



Street Types

The six (6) street types established by the plan are listed below and mapped in **Figure 43**. Typologies are reflective of the future vision for downtown, strategic multimodal routes, and strategic multimodal intersections. **Figure 43** and **Table 6** should be updated as growth and development occur, as new streets are platted/created, and as definitions of street types continue to be established and refined in Horace.

Table 6. Horace Street Typologies

1 DOWNTOWN ARTERIAL							
Traditional Main Street. The downtown arterial is appropriate for a wide range of land uses with publicly-supported development intensity; including single-family residential, institutional uses (e.g. city offices, public library), retail, office, parks & open space, and small-scale entertainment destinations. Typically, these streets have higher traffic volumes and lower travel speeds, serve regional traffic, and include significant bicycle and pedestrian traffic; all situated in traditional, walkable scale downtown development.							
Downtown Arterial Characteristics <ul style="list-style-type: none">▪ Typically minor arterials▪ Development fronts the street▪ Multimodal access connects to surrounding neighborhoods▪ Prioritizes wide sidewalks▪ No on-street bicycle facilities (i.e. bike lane)▪ Supports vision for Downtown▪ Prioritizes multimodal safety and accessibility			Downtown Arterial Considerations <ul style="list-style-type: none">▪ Typically wide sidewalks both sides of street▪ Prioritizes pedestrian safety and mobility▪ Curb extensions at intersections provide traffic calming opportunities and prioritizes pedestrian safety▪ Rectangular rapid flashing beacons (RRFBs), curb extensions, and other intersection safety features are prioritized in high pedestrian areas, including mid-block crossings▪ On-street parking is parallel or diagonal, depending upon land use density and space▪ Driveways should be removed and consolidated where feasible to minimize turning-movement conflicts				
 AADT	Up to 20,000	 Lanes	2 Lane* ^P	 speed	25 mph		
				 ROW	80' to 135'		
Route Strategy: Continuous Wide Sidewalk, both sides of street.							
Crossing Strategy: Every Intersection or 1/4-mile, whichever is less; Mid-block crossings as applicable in high-pedestrian locations; Focus on traffic calming and pedestrian visibility.							

*unless noted otherwise

^P On-street parking both sides, typical

2

MIXED USE ARTERIAL

Sometimes referred to as **highway commercial**, serves as a cross-town link and business corridor where people live, shop, dine, and work to support economic activity. Mixed use arterials respond to a wide range of land uses, density, and intensity including but not limited to single-family residential, multi-family residential, retail, office, and entertainment destinations. Typically, these streets have higher traffic volumes with higher travel speeds and may include significant traffic generators such as anchor developments (i.e. corporate campuses, medical centers/clinics, etc.). Mixed Use Arterials prioritize multimodal access to surrounding neighborhoods and provide critical mobility to regional destinations.

Mixed Use Arterial Characteristics

- Typically minor arterials with highest traffic volumes in Horace
- Generate substantial vehicular traffic and serve as major commuter routes
- Multimodal access connects to surrounding neighborhoods
- Typically high volumes of off-street parking
- Prioritizes shared use paths, no on-street bicycle facilities

Mixed Use Arterial Considerations

- Typically warrants bike and pedestrian facilities on both sides of street
- Typically warrants higher-intensity, protected bicycle and pedestrian crossings at strategic locations
- Driveways should be removed and consolidated where feasible to minimize turning-movement conflicts
- Landscaped medians may provide traffic separation, access management, and space for dedicated turn lanes at intersections
- On-street parking is not provided



20,000 to 30,000



Lanes

3 to 5 Lane



speed

Up to 45 mph



ROW

135'

Route Strategy: Continuous Shared Use Path, both sides of street.

Crossing Strategy: Every Collector or Arterial Intersection or 1/2-mile, whichever is less; Mid-block crossings should only be considered with HAWK Signal or Grade Separation; Prioritize protected pedestrian crossings.

3

DOWNTOWN COLLECTOR

Traditional Downtown Street. Like the downtown arterial typology, this typology reflects and supports the future vision for downtown Horace and focuses on small-town community character and economic development. The downtown collector responds to a wide range of land uses with a publicly-supported intensity as established through significant community feedback including single-family residential, institutional uses (e.g. city offices, public library), retail, office, parks & open space, and small-scale entertainment destinations. These streets typically connect people between downtown Horace, adjacent neighborhoods, and arterial or regional routes. Downtown collectors have lower travel speeds and include significant bicycle and pedestrian traffic generators situated in traditional, walkable scale development.

Downtown Collector Characteristics

- Development fronts the street
- Multimodal access connects to surrounding neighborhoods
- Prioritizes wide sidewalks, no on-street bicycle facilities
- May include bike and pedestrian facilities on one side of the street, depending upon development and size of pedestrian zone

Downtown Collector Considerations

- Typically wide sidewalks both sides of street
- Prioritizes pedestrian safety and mobility
- Curb extensions at intersections provide traffic calming opportunities and prioritize pedestrian safety
- Rectangular rapid flashing beacons (RRFBs), curb extensions, and other intersection safety features are prioritized in high pedestrian areas, including mid-block crossings
- On-street parking is parallel or diagonal, depending upon land use density and space
- Driveways should be removed and consolidated where feasible to minimize turning-movement conflicts

 AADT	Up to 20,000	 Lanes	2 Lane ^P	 speed	25 mph	 ROW	80' – 100'
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Route Strategy: Continuous Wide Sidewalk or Shared Use Path, both sides of street.

Crossing Strategy: Every Intersection or 1/4-mile, whichever is less; Mid-block crossings as applicable in high-pedestrian locations; Focus on traffic calming and pedestrian visibility.

^P On-street parking both sides, typical

4

RESIDENTIAL COLLECTOR

Connect residential neighborhoods to community destinations such as schools, parks, religious institutions, and arterial or regional routes. Residential collectors carry more traffic than residential neighborhood streets while still prioritizing slower speeds and multimodal accessibility. Safety is a priority for all modes including those walking, biking, and driving. These streets provide the main route to neighborhood destinations. Multimodal traffic is accommodated through a separated shared use path on at least one side of the street, and standard sidewalk on the other side of the street, as applicable. Intersection safety is a priority, with additional crossing enhancements expected near parks, schools, and other destinations.

Residential Collector Characteristics

- Higher traffic volumes than neighborhood streets
- Shared use path on one side of road
- On-street parking may or may not be provided and should be considered strategically; underutilized on-street parking will visually open the road and naturally increase vehicular speeds

Residential Collector Considerations

- Rectangular rapid flashing beacons (RRFBs), curb extensions, and other intersection safety features are prioritized in high pedestrian areas, including mid-block crossings
- May include continuous center left turn lane or turn lanes at designated intersections

 AADT	Up to 7,000	 Lanes	2 to 3 Lane ^P	 speed	25 mph	 ROW	125'
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Route Strategy: Continuous Shared Use Path, one side of street; Sidewalk, other side of street.

Crossing Strategy: Every Intersection or 1/4-mile, whichever is less; Mid-block crossings as applicable in high-pedestrian locations; Focus on traffic calming and pedestrian visibility.

^P On-street parking may be used as a traffic calming technique however, only if parking is utilized. If on-street parking is provided and underutilized, travel lanes will appear wider and traffic speeds will increase.

5

MIXED USE COLLECTOR

The mixed use collector typology **provides direct access to a range of land uses** including, but not limited to single-family residential, multi-family residential, retail, office, and entertainment destinations. These streets prioritize multimodal access to the surrounding neighborhood, providing essential bike and pedestrian connections.

Multimodal traffic is accommodated through a separated shared use path on at least one side of the street, and standard sidewalk on the other side of the street, as applicable. Intersection safety is a priority, with additional crossing enhancements expected near parks, schools, and other destinations.

Mixed Use Collector Characteristics

- Shared use path on at least one side of street
- Typically high volumes of off-street parking

Mixed Use Collector Considerations

- Driveways should be removed and consolidated where feasible to minimize turning-movement conflicts
- Typically warrants higher-intensity, protected bicycle and pedestrian crossings at strategic locations

 AADT	20,000 to 30,000	 Lanes	3 to 5 Lane	 speed	Up to 35 mph	 ROW	135'
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Route Strategy: Continuous Shared Use Path, one side of street; Sidewalk, other side of street.

Crossing Strategy: Every Collector or Arterial Intersection or 1/2-mile, whichever is less; Local street intersection and mid-block crossings should only be considered with Push-Activated RRFBs and based on high-pedestrian areas/land use context; Prioritize high-visibility pedestrian crossings.

RESIDENTIAL NEIGHBORHOOD STREET

Residential neighborhood streets are low speed, low traffic volume street connecting residential properties to nearby destinations. Residential housing is typically single-family however, may include limited medium and high-density housing units such as townhomes and apartments. These streets may also provide access to small-scale businesses, retail, offices, institutions, parks and open space, and schools. Safety is a high priority on residential neighborhood streets, with a focus on low traffic volumes and speeds that minimize the risk of crashes and maintain small-town community character of people walking, biking, playing, and living along neighborhood streets. The characteristic differences of the city's residential neighborhood streets and considerations for each are described separately below:

Rural Residential Neighborhood Street**Characteristics:**

- Very narrow street width
- Typically no on-street parking
- Typically ditch/swale stormwater drainage (no curb & gutter)
- No bike or pedestrian facilities (e.g. no sidewalk)

Considerations:

- i) Lowest infrastructure cost
- ii) Character is unique to Horace
- iii) Future implementation may be considered as low-impact development (LID) however, must include strict design standards
- iv) Low speed and traffic volume result in the safest streets in Horace, based on historical crash data
 - Warrants preservation based on safety, long-term fiscal responsibility, and desired community character
 - Consider strategic implementation of yield-street pavement markings and signage (on-street walking and biking is commonplace)
 - Traffic calming strategies not recommended as the narrow street width keeps vehicular speeds low

 AADT Up to 400



2 Lane

 speed 20 mph

 ROW 70'

Route Strategy: Continuous Yield Street as applicable.


Crossing Strategy: Crosswalk Striping as necessary in high-pedestrian areas; Focus on traffic calming and pedestrian visibility.

Urbanized Residential Neighborhood Street [pictures @ bottom]**Characteristics:**

- Narrow street width
- Typically includes on-street parking on at least one side of the street
- Typically underground storm sewer (with curb and gutter)
- Standard sidewalk on at least one side of the street

Considerations:

- Typically low speed and traffic volumes however, speed is impacted by utilization of on-street parking and visual width or driving lanes
- May include strategic shared use paths or wide sidewalks to prominent neighborhood destinations (e.g. neighborhood park)
- May include pedestrian crossing enhancements at strategic locations near prominent neighborhood destinations
- Traffic calming strategies such as curb extensions and raised crosswalks may be recommended to sustain low speeds, as necessary

 AADT Up to 400

2 Lane^P

 speed 20 mph

 ROW 70'

Route Strategy: Continuous Sidewalk, at least one side of street.

Crossing Strategy: Crosswalk Striping and Curb Extensions as necessary in high-pedestrian areas; Focus on traffic calming and pedestrian visibility.

Typical Rural Residential Neighborhood Street:

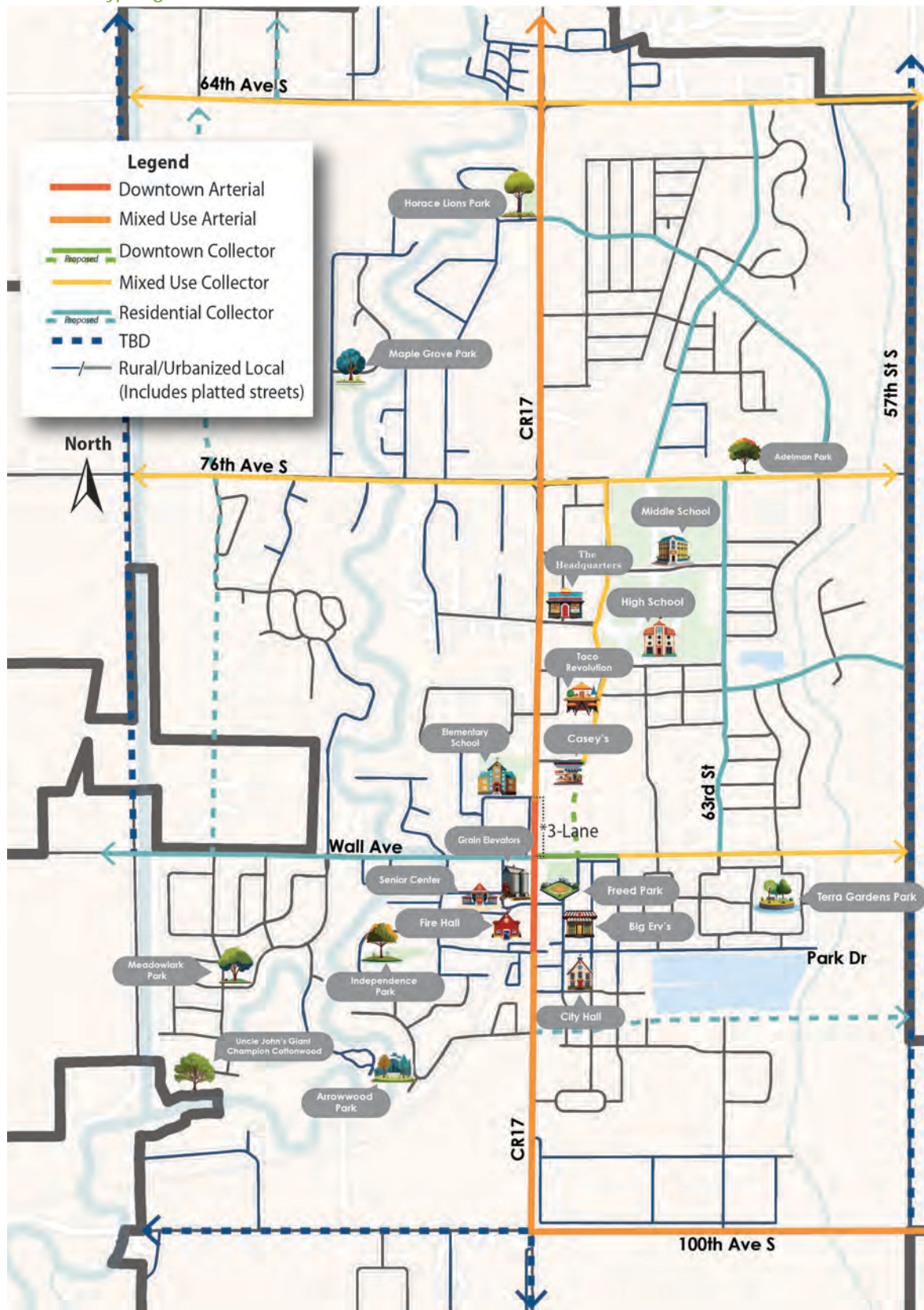


Typical Urbanized Residential Neighborhood Street:



^P On-street parking at least one side, typical; however, underutilized on-street parking will increase traffic speeds.

Figure 43. Horace Street Typologies



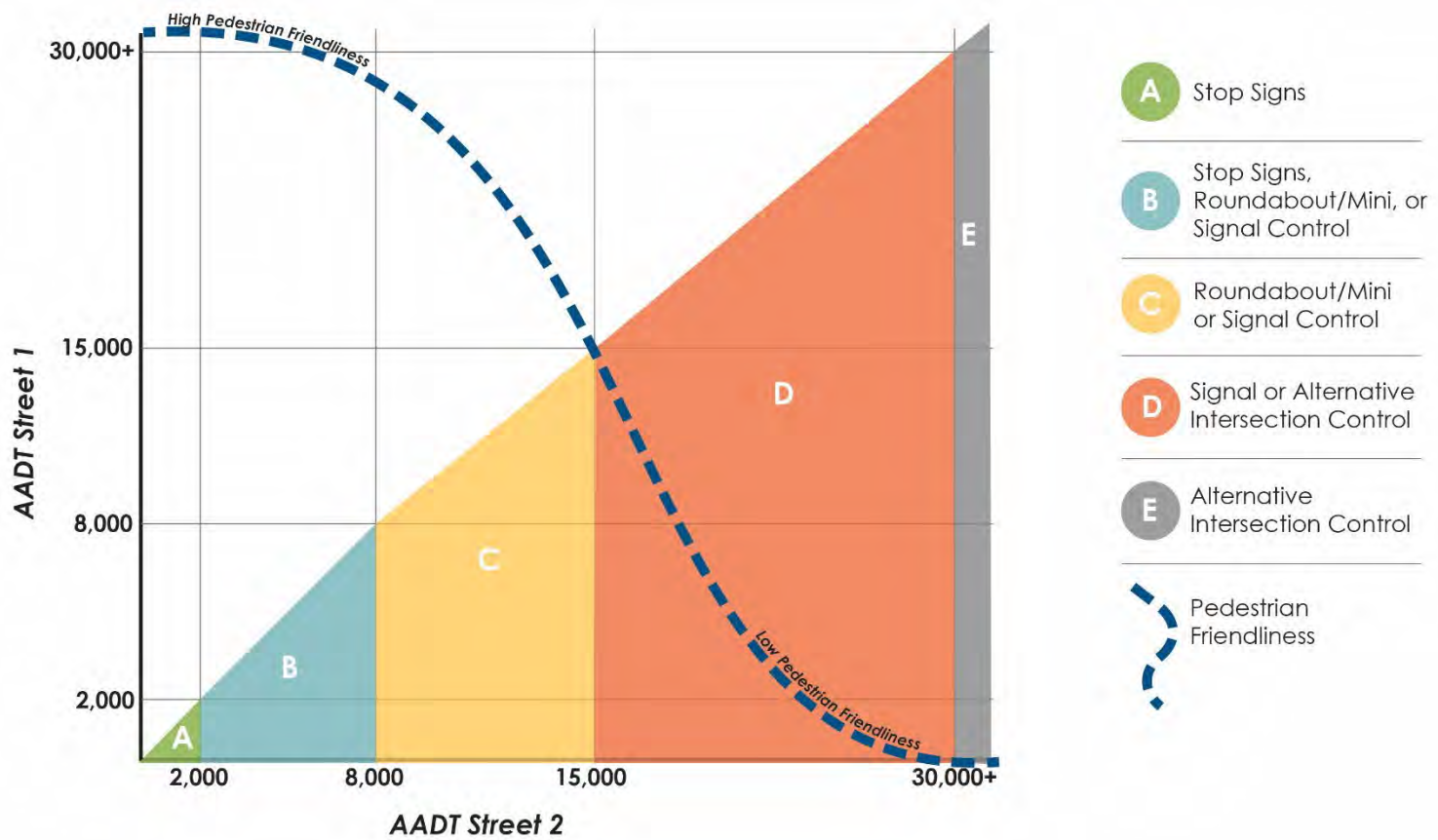
Intersections

There are several intersections in the study area that may significantly impact people’s travel experiences in Horace. The City must focus on right-sizing intersections and intersection control, as poorly designed or oversized intersections can be very unsafe for people walking and biking and can be less efficient for drivers. Appropriate intersection configuration will enhance the safety and comfort of all users traveling in Horace.

There are numerous ways to control traffic and improve safety at intersections. **Figure 44** provides guidelines to consider when identifying the appropriate intersection control by Annual Average

Daily Traffic (AADT) however, some intersection options may not be appropriate depending upon the street type and any unique characteristics of the location. Intersection guidelines below are adapted from NDDOT’s Traffic Operations Manual and should be treated as general guidance; specific intersection options will depend on each location’s individual context.

Figure 44. Intersection Strategy Matrix



*Alternative Intersection Control is not necessary, or acceptable in Horace.

Intersection Control

General intersection control strategies are identified below. Selection of the specific strategy will vary by context such as daily traffic or AADT, adjacent land use and future land use, and other factors. Intersections of all strategies may prioritize complete streets and multimodal intersection strategies; however, generally higher traffic volumes lead to less pedestrian friendliness and high-intensity infrastructure options.

SIGNAL CONTROL

Traffic signals are recommended intersection strategies when traffic volumes and speeds reach a certain threshold, typically on higher-intensity streets such as arterials. Signals can provide essential, safe options for multimodal crossing and are fully customizable to optimize mobility of people walking and biking. In downtown Horace, traffic signals may not support a pedestrian-friendly environment and should only be considered if optimized for a slow speed, multimodal environment. There is some public support for signal control in Horace.



ROUNDAABOUT/MINI ROUNDAABOUT

Roundabouts are a very safe intersection strategy and reduce congestion on various street types. Mini roundabouts are typical on slow speed, low traffic volume roadways. In downtown Horace, roundabouts can improve multimodal safety by making pedestrian-crossing easier and improving visibility. However, roundabouts can be more challenging to implement at existing intersections as they require more space. There is some public support for roundabouts in Horace; however, as provided in Concept 1 feedback, roundabouts are not supported downtown.



STOP SIGNS

Stop controlled (two-way, all-way) intersections are characteristic of slow speed, low traffic volume roadways with similar AADTs. In downtown Horace, stop signs could improve multimodal safety by ensuring slow traffic speeds, characteristic of a walkable, small-town downtown. The public preference for intersection control in Horace, is stop control.



Figure 45. Multimodal Route Strategies

STEP 3

Equip Multimodal Strategies

Multimodal strategies improve accessibility and mobility in downtown Horace, and across the city. As a *complete streets* focused Plan, there are numerous locations in the study area and across the city which lack infrastructure for people walking and biking for example, neighborhoods in proximity to downtown have no sidewalks or paths for people walking or biking to downtown or other parts of town. There are gaps in multimodal infrastructure across the city however, gaps are prevalent to and from downtown and between old and new neighborhoods. Given the central location of downtown within city limits, the Downtown Neighborhood Plan provides a strategic framework that prioritizes multimodal connections through a center-out approach; an approach that strategically prioritizes investment in multimodal connections to and from the oldest parts of town first (downtown and adjacent neighborhoods).

There are numerous strategies to improve multimodal transportation for those walking, biking, and getting around by other non-vehicular ways. Strategies. For the Plan, multimodal strategies are included that support the overall vision of the Plan, support community character, and are tailored to the public-perception of Horace's identity as a community (based on public feedback). Multimodal strategies identified hereafter are organized into routes or connections, and intersections.

Multimodal Routes

There are many options (Figure 45) and facilities to choose from when implementing *complete streets* along strategic routes in Horace. The following multimodal route toolbox includes pre-vetted, publicly supported options that enhance



Yield Street

Shared local streets with low traffic volumes and slow speeds. No separate bike or pedestrian facilities, drivers yield to people walking and biking on the edge of the



Sidewalks

Pedestrian-only facilities for walking along the street. Typically a minimum of 4.5' wide and separated by at least a 6' wide boulevard to sustain grass and snow storage.



Wide Sidewalk

Intended for downtown corridors. May include a pedestrian zone, furniture zone, and planting area. Sometimes communities prohibit biking on wide sidewalks downtown.



10' Off-Street Trail

Intended for people both walking and biking. Typically off-street, completely separate from traffic. Provides a safe and comfortable option for people of all ages.



10' Shared Use Path (SUP)

Intended for people both walking and biking. Typical along collector and arterial streets. Provides a safe and comfortable option for people of all ages.

connectivity for people walking and biking. Not all options are appropriate in every location in Horace therefore, details and guidance is provided for each to support decision-making of where to apply specific strategies.

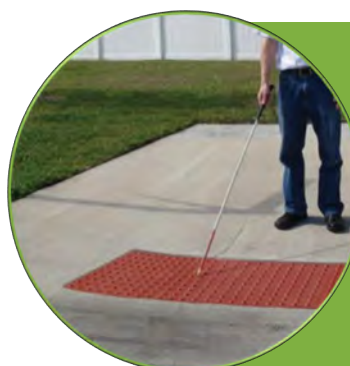
Multimodal Intersections

Implementing *complete streets* at intersections do not reflect a single intersection configuration but do provide options of multimodal strategies that can be used to improve safety for people walking and biking. The following multimodal intersection toolbox includes pre-vetted, publicly supported options that enhance connectivity for people walking and biking in Horace.

Multimodal intersection strategies included in the Plan range from simple, low-cost strategies to higher-intensity, major investments as treatments should vary depending upon the street typology and characteristics. Not all options are appropriate in every location in Horace therefore,

Figure 46. Multimodal Intersection Strategies

details and guidance is provided for each to support decision-making of where to apply specific strategies.



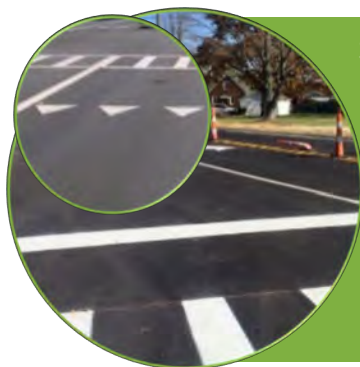
ADA Compliance

Americans with Disabilities Act (ADA) compliance includes standard treatments to increase accessibility and safety for people with disabilities.



RRFBs

Rectangular rapid flashing beacons (RRFBs) is a pedestrian-activated warning device that enhances safety and visibility at uncontrolled intersections.



Stop Bar/Yield Lines

Simple street striping indicating where vehicles should safely stop or yield for people walking or biking through a crosswalk. Typical striping before any crosswalk.



Curb Extensions

Curb extensions extend the pedestrian space into the roadway to improve visibility, reduce turning radii, and slow vehicular traffic. Typical for intersections or mid-block.



Crosswalk Striping

Street striping, typically longitudinal bars at designated crossings for people walking and biking. Drivers are required to yield the right-of-way to pedestrians in crosswalks⁷.



Median Refuge Island

Includes a median space for pedestrians to pause while crossing a street, typically on fast or busy streets with multiple lanes. Typical at mid-block or uncontrolled crossings.

⁷ North Dakota Century Code Title 39-10-28. Pedestrian's right of way in crosswalk.

⁸ Continuous flashing beacons are not recommended in Horace (use only dynamic RRFBs; push or permissive/automatic activation)

Figure 46 Continued. Multimodal Intersection Strategies



Raised Crosswalk

Crosswalk extends across the intersection at the same level as the sidewalk. Improves visibility and accessibility. Also, very effective at slowing or calming traffic.



Bike & Ped Bridge

Accommodate people walking and biking. Extends shared use paths and trails across natural features such as the Sheyenne River.



Streetlights

Used to illuminate intersections and adjacent transportation facilities, increasing visibility and safety for people walking and biking in dark conditions.



Grade Separation

Accommodate people walking and biking. Completely separates vehicular traffic and people walking and biking. Typical for arterials and highways.



Pedestrian Scale Lighting

Used to illuminate streets, intersections, and especially sidewalks and paths, increasing visibility and safety for people walking and biking in dark conditions.



HAWK Signal

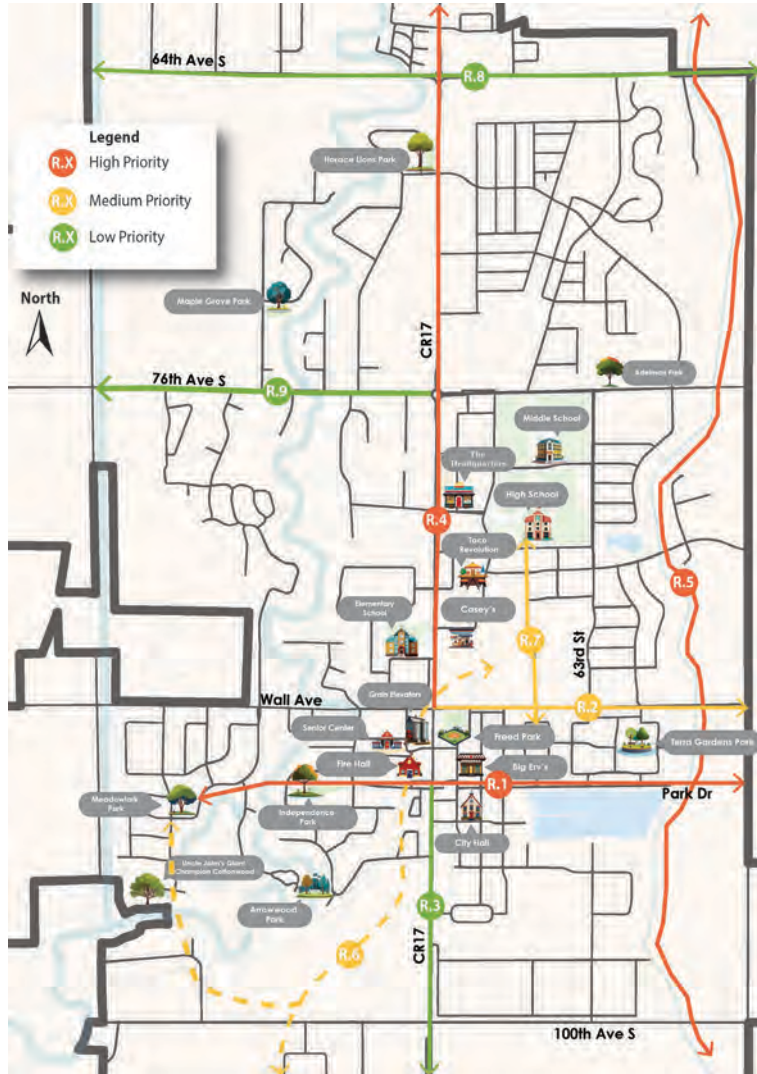
Also called pedestrian hybrid beacons, HAWKs provide a pedestrian-activated traffic signal that stops traffic. Typical for mid-block crossings of fast, busy streets.

Strategies in Action

The following strategies (**Figures 45 & 46**) are listed by Route and Intersection and are ordered by their priority (1 being top priority and so on). Project sheets are provided in **Appendix D**.

MULTIMODAL ROUTES

Figure 47. Multimodal Route Priority



Multimodal routes are listed in order of priority:

R.4 Main Street/CR 17 north of Wall Avenue from Wall Avenue to 52nd Avenue. Strategy for route includes a shared use path on both sides of the street.

- a. Construct multimodal upgrades to existing intersections starting with those nearest to downtown.
 - i. Intersection I.2 Strategies.
 - ii. Intersection I.1 Strategies
 - iii. Intersection I.11 Strategies

- iv. Install HAWK Signal or Construct Median Refuge Island with RRFBs at CR 17 & 81st Avenue S.
- v. Construct a Grade Separation or install a HAWK Signal at CR 17 & 68th Avenue S.
- b. Construct a shared use path to fill gaps in existing network starting with those nearest to downtown.
 - i. East side of Main Street from 81st Avenue S. to 76th Avenue S. (in Horace CIP)
 - ii. East side of Main Street from Wall Avenue to Ironwood Drive, time with Intersection I.1 improvements.
 - iii. East side of CR 17 from 73rd Avenue S. to 52nd Avenue S.
 - iv. West side of CR 17 from 76th Avenue S. to 52nd Avenue S.

R.5 Drain 27 from south of 100th Avenue S. to City of Fargo Trail north of 64th Avenue S. Strategy for route includes an Off-Street Trail on at least one side of the Drain (prioritize west side, nearest to development).

- a. Construct an Off-Street Trail to fill gaps in existing network along Drain.
 - i. East Side of Drain from south of 100th Avenue S. to Wall Avenue S.
 - ii. East side of Drain from south of 76th Avenue S. to City of Fargo Trail north of 64th Avenue S.
- b. Construct multimodal upgrades to existing intersections and plan for future network crossings of the Drain.
 - i. Construct Grade Separation at Wall Avenue S.
 - ii. Construct Grade Separation at 76th Avenue S.
 - iii. Construct Grade Separation at 64th Avenue S.
 - iv. Construct Grade Separation at 100th Avenue S.

- c. Policy: Plan to construct Grade Separations of Drain 27 Trail at Arterial and Collector streets.
- d. Policy: Plan to construct Raised Crosswalks and/or Curb Extensions for Drain 27 Trail at Local streets.
- e. Policy: Plan to construct Drain 27 bike and pedestrian crossings at least every 1/2-mile.

R.1

Park Drive from Boxelder Circle to Drain 27.

Strategy for route includes an Off-Street Trail and Yield Street.

- a. Work to acquire Right-of-Way (ROW) for an Off-Street Trail.
 - i. Both sides of the Sheyenne River, west of Independence Park.
 - ii. Park Drive alignment from Main Street to Nelson Drive.
- b. Construct Bike and Pedestrian Bridge at Sheyenne River west of Independence Park.
- c. Construct Off-Street Trail (may be contingent on ROW acquisition).
 - i. From Boxelder Circle, across the Sheyenne River, and through Independence Park to Sheyenne Drive.
 - ii. From Wild Goose Lane to Drain 27.
- d. Install Yield Street striping and signage.
 - i. From Sheyenne Drive to Main Street.
 - ii. From Nelson Drive to Wild Goose Lane.
- e. Construct multimodal upgrades to intersections.
 - i. Install Conventional Crosswalk or Curb Extensions at Southwood Drive and Sheyenne Drive.
 - ii. Intersection I.5 strategies.

R.2

Wall Avenue from Main Street to 57th Street S.

Strategy for route includes a Sidewalk (north side) and Shared Use Path (south side).

- a. Construct north side Sidewalk/Wide Sidewalk and south side Shared Use Path starting with areas nearest to downtown.
 - i. Construction should be timed with Wall Avenue street reconstruction.
- b. Construct multimodal upgrades to existing intersections starting with those nearest to downtown and those providing connections to the Horace High School and Middle School.
 - i. Intersection I.1 strategies.
 - ii. Construct mid-block crossing to Freed Park with RRFBs and Curb Extensions or Raised Crosswalk.
 - iii. Intersection I.9 strategies
 - iv. Intersection I.8 strategies
 - v. Intersection I.10 strategies
 - vi. Install Curb Extensions and/or Raised Crosswalk at Wall Avenue & 62nd Street S.

R.7

Front Street from Wall Avenue S. to 82nd Avenue

S. Strategy for route includes west side Shared Use Path and east side Sidewalk.

- a. Construct a Shared Use Path to fill gaps in existing network.
 - a. West side of street from Wall Avenue to 83rd Avenue S.
- b. Construct a Sidewalk to fill gaps in existing network.
 - a. East side of street from Wall Avenue to St. Anne Avenue.
- c. Construct multimodal upgrades to existing intersections and plan for future crossings of the street.
 - a. Construct mid-block crossing for existing Off-Street Trail north of St. Anne Avenue with RRFBs and Curb Extensions or Raised Crosswalk.
 - b. Intersection I.9 Strategies
- d. Policy: Plan to construct Conventional Crosswalks and/or Curb Extensions at future street crossings, as development continues.

R.6

Southwest Trail from south of 100th Avenue S. to downtown/ Wall Avenue. Strategy for route includes an Off-Street Trail and Shared Use Path network.

- a. Policy: Plan to construct an Off-Street Trail and/or Shared Use Path from residential neighborhoods south and west of 100th Avenue S an CR 17.
- b. Policy: Plan to align away from major section-line roads such as 100th Avenue S. and CR 17.
- c. Policy: Plan for two primary connections; (1) to Meadowlark Park and (2) to downtown/Wall Avenue.
- d. Work to acquire ROW through existing land development procedures and ROW acquisition, as necessary/feasible.
- e. Policy: Plan for Bike and Pedestrian Bridge at Sheyenne River for connection to Meadowlark Park/Lost River Neighborhood.
- f. Policy: Plan for and construct Grade Separations at Arterial streets.
- g. Policy: Plan to construct Curb Extensions at Collector streets including RRFBs, as applicable.
- h. Policy: Plan to construct Conventional Crosswalks at Local streets.

R.3

Main Street/CR 17 south of Park Drive from Park Drive to south of 100th Avenue S. Strategy for route includes a Shared Use Path on both sides of the street.

- a. Construct multimodal upgrades to existing intersections starting with those nearest to downtown.
 - i. Intersection I.5 Strategies.
 - ii. Intersection I.6 Strategies
 - iii. Construct a Grade Separation or install a HAWK Signal at CR 17 & Sparks Addition, south of Liberty Lane.
 - iv. Intersection I.7 Strategies

- b. Construct a shared use path to fill gaps in existing network starting with those nearest to downtown.
 - i. Both sides of CR 17 from Park Drive to south of 100th Avenue S.
- c. Policy: Plan to time construct with development south of Park Drive.

R.9

76th Avenue S. from Sheyenne Diversion Channel to 57th Street S. Strategy for route includes Shared Use Path on at least one side of the street (south side), and Sidewalk on the other.

- a. Construct a Shared Use Path to fill gaps in existing network along 76th Avenue S.
 - I. South side of 76th Avenue S. from Sheyenne Diversion Channel to 57th Street S.
 - II. North side of 76th Avenue S. from CR 17 to 57th Street S.
 - III. South side of 76th Avenue S. from Cub Creek Parkway to 57th Street S.
- b. Construct multimodal upgrades to existing and future intersections starting with those providing strategic connections to downtown and connections to the Horace Middle School and High School.
 - I. Install a HAWK Signal or Construct Median Refuge Island with RRFBs at Brink Drive/Sunnyside Court & 76th Avenue S. Intersection
 - II. Install a HAWK Signal or Construct a Median Refuge Island with RRFBs at Cub Creek Parkway & Future 66th Street S. intersection (future Grade Separation recommended at Drain 27 Trail just to the east).
 - III. Install a HAWK Signal or Median Refuge Island with RRFBs at 78th Street S. & 76th Avenue S. intersection (timed with development).
- c. Policy: West of CR 17 plan to construct Shared Use Path on at least one side of street. East of CR 17 plan to construct Shared Use Path on both sides of street.

- d. Policy: Plan to construct HAWK Signal or Median Refuge Island with RRFBs every 1/2-mile or at Collector streets, whichever distance is less.

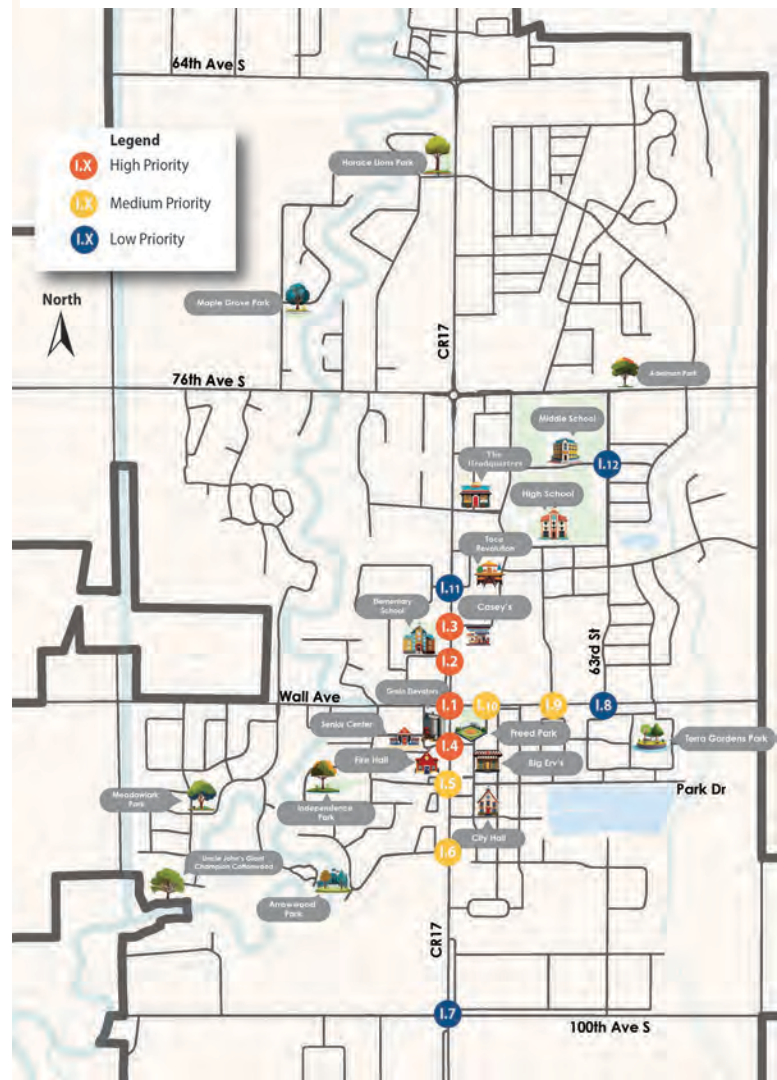
R.8

64th Avenue South from Sheyenne Diversion Channel to 57th Street S. Strategy for route includes Shared Use Path on at least one side of the street and Sidewalk on the other.

- a. Construct a Shared Use Path to fill gaps in existing network along 64th Avenue S.
 - i. South side of 64th Avenue S. from Sheyenne Diversion Channel to 57th Street S.
 - ii. Policy: Consider north side of 64th Avenue S. from CR 17 to 57th Street S. long-term, depending upon future 64th Avenue S. and I-29 interchange and coordination with City of Fargo's network.
- b. Construct multimodal upgrades to existing intersections and future intersections.
 - i. Install HAWK Signal or Construct Median Refuge Island with RRFBs at 64th Ave & future 9th Street W. intersection.
 - ii. Construct Grade Separation or install HAWK Signal at 64th Avenue S. & future 66th Street S. intersection.
- c. Policy: West of CR 17 plan to construct Shared Use Path on at least one side of street. East of CR 17 plan to construct Shared Use Path on both sides of street.
- d. Policy: West of CR 17 plan to construct protected crossings (HAWK Signal, Median Refuge Island/RRFBs) of 64th Avenue S. every 1/2-mile or at Collector streets, whichever distance is less.
- e. Policy: East of CR 17 plan to construct Grade Separations or at least protected crossings every 1/2-mile or at Collector streets, whichever distance is less.

MULTIMODAL INTERSECTIONS

Figure 48. Multimodal Intersection Priority



Multimodal intersections are listed in order of priority:

I.1

Main Street & Wall Avenue Intersection.

Maintain existing intersection control with multimodal improvements.

- a. Upgrade multimodal infrastructure at existing multimodal intersection legs (west side and south side).
 - I. Construct Curb Extensions (construct Demonstration Project).
 - II. Install Crosswalk Striping and Stop Bars.
 - III. Update ADA Compliance.

- IV. Add Pedestrian Scale Lighting.
- b. Construct full build out as multimodal connections are made or with Wall Avenue street reconstruction project (all intersection legs).
 - I. Curb Extensions.
 - II. Crosswalk Striping and Stop Bars.
 - III. ADA Compliance.
 - IV. Pedestrian-Scale Lighting.
- c. Policy: Plan to construct a Wall Avenue eastbound left turn lane (estimated to be warranted by 2031).
- d. Policy: Evaluate Traffic Signal control (estimated to be warranted by 2034) with pedestrian-activated lead intervals and multimodal priority.
- e. Policy: Critical intersection for multimodal connections across town, cannot overbuild and need to right-size. For example, adding turn-lanes or oversizing intersecting streets will reduce multimodal safety and mobility and negatively impact the small-town character of Horace and make the vision for downtown less achievable; prioritize people walking.
- f. Policy: Prioritize as a safe route to school.

I.2 Main Street & Ironwood Drive. Maintain existing intersection control with strategic upgrades to multimodal improvements.

- a. Upgrade existing multimodal infrastructure at existing north leg of intersection.
 - I. Install Push-Activated RRFBs and Construct Median Refuge Island.
 - II. Upgrade/oversize existing signage and striping and install Yield Lines at crosswalk.
 - III. Install Pedestrian-Scale Lighting
- b. Demonstration Project: Remove cut-thru traffic from Main Street's west side frontage Road. Add Jersey Barriers north of the Post Office to close from thru-traffic.
 - I. Construct a permanent closure of the frontage road north of the Post

Office, which should include a turnaround adequate for emergency services and U.S. Postal Service vehicles.

- c. Policy: Only with closure of thru-traffic on Main Street west side frontage road (will not be effective otherwise). Plan to construct an Ironwood Drive eastbound right turn lane (estimated to be warranted by 2029).
- d. Policy: Evaluate All-Way Stop. Traffic Signal may be warranted
- e. Policy: Evaluate vehicle speed reduction in front of Horace Elementary School; Speed limit is posted 25 mph, but data shows 85 percent of vehicles are traveling 37 mph or faster.
- f. Policy: Prioritize as a safe route to school.

I.3 CR 17 & Lakeview Drive. Maintain existing configuration.

- a. Prioritize improvements south (Ironwood Drive) and/or north (Chestnut Drive) of this location.
- b. Policy: Study traffic impacts associated with Lakeview Development on the east side of CR 17 to right-size traffic control and multimodal improvements at this location.

I.4 Main Street & Center Avenue. Convert to 4-Way Stop control with multimodal improvements.

- a. Upgrade existing multimodal infrastructure at all intersection legs.
 - I. Construct Curb Extensions.
 - II. Install Crosswalk Striping and Stop Bars.
 - III. Update ADA Compliance.
 - IV. Add Pedestrian-Scale Lighting.
- b. Policy: Plan to convert to 4-Way Stop Control with timing of downtown development or before.

I.5 Main Street & Park Drive. Convert to 4-Way Stop control with multimodal improvements.

- a. Upgrade future multimodal infrastructure at all intersection legs.
 - I. Construct Curb Extensions
 - II. Install Crosswalk Striping and Stop Bars.
 - III. ADA Compliance.
 - IV. Pedestrian-Scale Lighting.
- b. Policy: Time with routes R.1 and R.3 improvements.

I.7 CR 17 & 100th Avenue S. Convert to Roundabout with multimodal improvements.

- a. Convert to Roundabout with Multimodal improvements on all legs of intersection.
 - I. Median Refuge Islands.
 - II. Crosswalk Striping and Yield Lines.
 - III. ADA Compliance.
 - IV. Streetlights.
- b. Policy: Consider I.7 project ahead of route R.3 construction to prioritize driver safety and mobility.
- c. Policy: Coordinate with goals and objectives of Metro COG's *West Metro Perimeter Highway Study*, as applicable.

I.9 Wall Avenue & Front Street. Maintain existing intersection control with multimodal improvements.

- a. Upgrade future multimodal infrastructure at all intersection legs.
 - I. Construct Curb Extensions.
 - II. Install Crosswalk Striping and Stop Bars.
 - III. ADA Compliance.
 - IV. Streetlights.
 - V. Install RRFBs and/or Median Refuge Island at Shared Use Path connection across Wall Avenue.

- b. Demonstration: Construct a temporary (as feasible) Push-Activated RRFB installation or Pedestrian Crossing Signage.
- c. Policy: Time with routes R.2 and R.7 improvements, and Wall Avenue street reconstruction project (likely a 3-lane at Front Street).
- d. Policy: Prioritize as a safe route to school.

I.10 Wall Avenue & Nelson Drive/Future Lakeview Drive. Convert to 4-Way Stop control with multimodal improvements.

- a. Convert to 4-Way Stop control with multimodal improvements on all legs of intersection.
 - I. Curb Extensions.
 - II. Crosswalk Striping and Stop Bars.
 - III. ADA Compliance.
 - IV. Pedestrian-Scale Lighting
- b. Policy: Consider route R.2 improvements and Wall Avenue street reconstruction project (2-lane at Nelson Drive).
- c. Policy: Critical intersection for multimodal connections between Main Street and Lakeview development, cannot overbuild intersection and need to right-size. For example, adding turn-lanes or oversizing intersecting streets will reduce multimodal safety and mobility and negatively impact the small-town character of Horace and make the vision for downtown less achievable; prioritize people walking.

I.6 CR 17 & Liberty Lane. Maintain existing intersection control with multimodal improvements.

- a. Upgrade future multimodal infrastructure at north leg of intersection.
 - I. Median Refuge Island with RRFBs.
 - II. Crosswalk Striping and Yield Lines.
 - III. ADA Compliance.
 - IV. Streetlights.

- b. Policy: Time with route R.3 improvements and future development along CR 17.

I.8

Wall Avenue S. and 63rd Street S. Maintain existing intersection control with multimodal improvements.

- a. Upgrade existing multimodal infrastructure at east and south legs of intersection.
 - I. Crosswalk Striping and Stop Bars.
 - II. Push-Activated RRFBs at Shared Use Path connection across Wall Avenue (east leg).
 - III. ADA Compliance.
- b. Construct full build out as multimodal connections are made or with Wall Avenue street reconstruction project (all intersection legs). Wall Avenue may be a 3-lane.
 - I. Curb Extensions.
 - II. Median Refuge Island with RRFBs (east leg) and Yield Lines.
 - III. Crosswalk Striping and Stop Bars.
 - IV. Streetlights.
- c. Policy: Time with route R.2 improvements and/or Wall Avenue street reconstruction project. Depending on Wall Avenue street reconstruction project phasing, may prioritize existing crossing improvements ahead of long-term, full build out.
- d. Policy: Prioritize as a safe route to school.

I.11

CR 17 & Chestnut Drive or 81st Avenue S. Maintain existing intersection control with multimodal improvements.

- a. Upgrade multimodal infrastructure at all legs of intersection.
 - I. HAWK Signal (north/south legs) or Grade Separation (north or south leg)
 - II. Crosswalk Striping and Stop Bars or Yield Lines as applicable.
 - III. ADA Compliance.
 - IV. Streetlights.

- b. Policy: Prioritize as a safe route to school.

I.12

63rd Street S. & 79th Avenue S. Maintain existing intersection control with multimodal improvements.

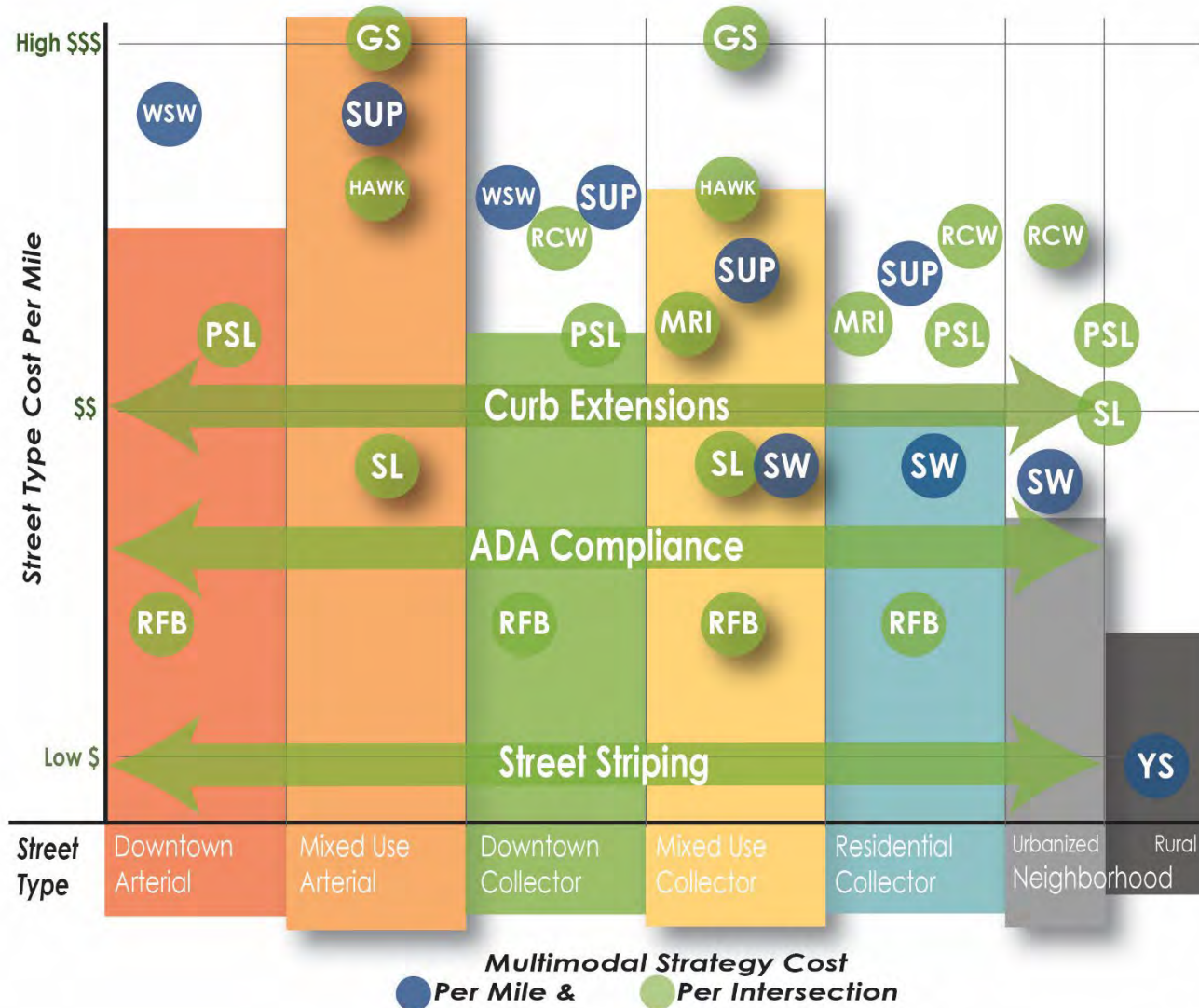
- a. Upgrade existing multimodal infrastructure at east and north legs of intersection.
 - I. Median Refuge Island with RRFBs (north leg).
 - II. Crosswalk Striping, Stop Bars, and Yield Lines as applicable.
 - III. ADA Compliance.
 - IV. Streetlights
- b. Policy: Prioritize as a safe route to school.

MULTIMODAL STRATEGY COST

As shown in **Figure 49** multimodal strategies and street typologies have varying degrees of cost. Street typologies and appropriate multimodal strategies are listed for each. If a multimodal strategy is not included on in the street typology column, the strategy may not be appropriate for that type of street. The higher each typology and strategy is on the chart, the higher the anticipated cost of implementation.

The City of Horace and it's partner agencies should use **Figure 49** as guidance to right-size multimodal infrastructure in the city. Exceptions and other considerations may be made through public input or at the discretion of the City Engineer/City Planner.

Figure 49. Cost of Street Typologies & Applicable Multimodal Strategies



Multimodal Strategies Key

Routes

- Tr** Off-Street Trail
- WSW** Wide Sidewalk
- SUP** Shared Use Path
- SW** Sidewalk
- YS** Yield Street

Intersections

- Br** Bike & Ped. Bridge
- MRI** Median Refuge Island
- ADA** ADA Compliance
- RFB** Rectangular Rapid Flashing Beacons
- PSL** Pedestrian-Scale Lighting
- SL** Streetlights
- GS** Grade Separation
- HAWK** HAWK Signal (Pedestrian Hybrid Beacon)
- RCW** Raised Crosswalk

Street Striping

Includes Stop Bars, Yield Lines, and Crosswalk Striping

FINAL DOWNTOWN CONCEPT

The final concept for Downtown Horace (**Figure 50**) envisions the core downtown (as identified by the community) as the vibrant hub of the City's economic and social life. At its heart, the plan honors Horace's small-town charm and deep agricultural roots by making one of the historic grain elevators a focal point. This central community space, which adjoins the Senior Center, is a central gathering point and will support a farmer's market, a large green space for community events, and a place for a Veteran's Memorial if desired.

The concept emphasizes unique downtown character, walkability, traffic calming, and human-scale development; all hallmarks of successful rural downtowns.

Small town style commercial development is a key component of the plan. New development is proposed for the north side of the grain elevator lot and for vacant lots on the east side of Main Street. Existing and historic buildings on the east side of Main Street will be preserved and potentially redeveloped to maintain the downtown's authentic feel. Building height is limited to two stories, making this district more human-scale.

Strong sidewalk and trail connections link downtown to the new Lakeview commercial development, surrounding neighborhoods, Freed Park, and Horace Elementary School. For better connectivity, mid-block crossings in the downtown core will connect to additional parking at Freed Park.

The intersection of Main Street and Wall Avenue will remain a traditional four-way stop, slowing traffic and enhancing the "gateway to downtown" feeling of the intersection. This intersection could also be a

How to Use the Downtown Concepts

The three (3) preliminary downtown concepts and final downtown concept are intended to be a guide for downtown Horace that reinforce the community's desire for "small-town" character. This is NOT a master plan – successful towns evolve incrementally, not instantaneously; realizing *the vision of downtown is expected to be a long-term endeavor that may take 25-years or more*. Development may ultimately incorporate various elements of one or more concepts, while other elements may be omitted or modified as development occurs. Nonetheless, development proposals should generally conform to the form and character conveyed by the renderings.

mini-roundabout, though residents are skeptical of this option.

On-street parking east of the Wall / Main intersection will provide additional "downtown-style" parking for Freed Park and the future Lakeview development. The Main Street frontage road will be modified to reduce traffic conflicts near the elementary school.



The heart of Main Street: The concept builds on the current social life of the town – Big Erv's, Southern Valley Fire & Rescue, and the Horace Senior Center.



Grain elevator site: The south elevator is retained, while the remainder of the site is redeveloped community space, striking a balance between identity preservation and downtown transformation. Small, distinct buildings preserve Horace's small-town feel and fit in seamlessly with the surrounding neighborhood.



Main Street looking north: Curb extensions and pedestrian crossings improve walkability and pedestrian safety. On-street parking supports adjacent development, buffers pedestrians from vehicle traffic, and reduces vehicle speeds through the heart of downtown.



Wall Avenue intersection looking north: A four-way stop preserves the small-town feel and supports walking and biking. Existing buildings are retained and updated as needed. Existing homes can retain their residential use or transition to commercial space.



Wall Avenue intersection looking southeast: This intersection is the gateway to Downtown Horace – an enticing place to shop, gather with friends, and celebrate community events.



CULTIVATING DOWNTOWN HORACE

How do we get there from here?

The downtowns of the past weren't built overnight; they grew incrementally and organically, shaped by community needs and the ambition of local entrepreneurs. It was a process that demanded time and patience. We can look to Horace's agricultural roots for inspiration on how to move forward. **Cultivation** - the careful preparation of soil to foster healthy growth - is the perfect metaphor for our community development efforts.

This plan offers a big, bold vision for downtown Horace—one that will likely take years to fully realize. But that doesn't mean it's impossible. Horace can achieve this vision through a combination of **small, incremental changes** and a few **larger, catalytic strategies**.

The images in this section illustrate how Policy, Collaboration, and Projects come together to support the community's vision for downtown and forward the goals of the Plan. Each image describes a different focus area to describe both small and large changes that are steps toward a thriving downtown Horace.

BLOCK 1 Focus Area

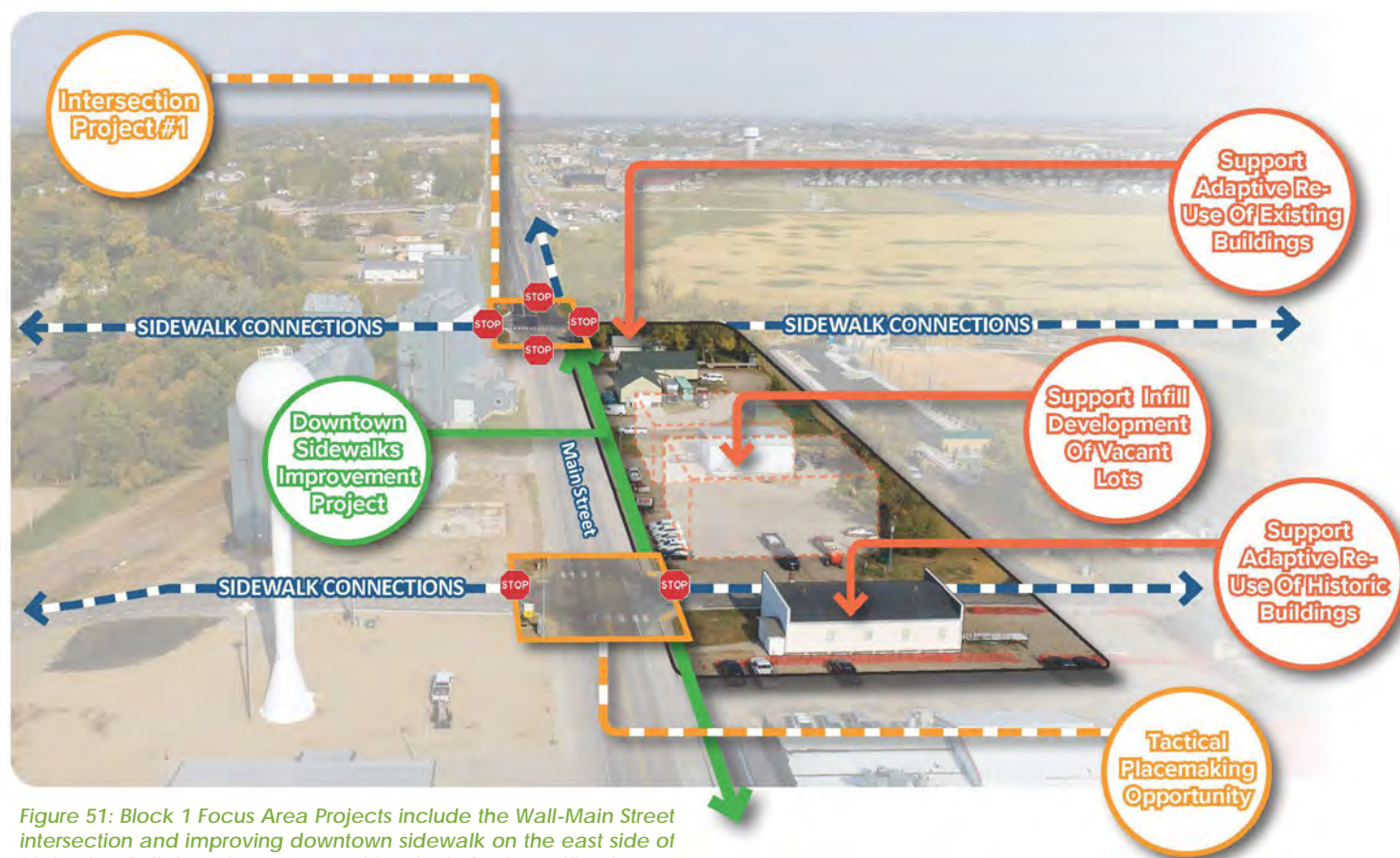


Figure 51: Block 1 Focus Area Projects include the Wall-Main Street intersection and improving downtown sidewalk on the east side of Main St. Collaboration opportunities include beautification at Center-Wall. Policies and incentives can help support both new infill development and adaptive reuse of historic buildings.

Make Small Incremental Changes

These are minor changes to approaches, policies, collaboration, and simple projects that will help lay the groundwork - *cultivate the soil* - for Horace's bold vision.

1. Policy: Allow a diversity of building types.

Downtown development guidelines and zoning help drive the form of a downtown. However, overly prescriptive development guidelines tend to look "strip mall-like" - the type of development that the community would prefer to see less. Broad design guidelines that will help maintain the "small-town" feel that residents prefer include:

- a. Restricting building heights to two stories along Main Street. Exceptions may be made for historical preservation/reference.

- b. Allowing a wider variety of building materials with more focus on small-town feeling building design.
- c. Enacting maximum sign sizes and quantities.
- d. Avoiding large blank walls along primary downtown thoroughfares like Main Street, Wall Avenue, and Center Avenue sidewalks.
- e. Creating an appropriate sidewalk scale with architectural elements like awnings, windows, and building details along downtown sidewalk thoroughfares.
- f. Prohibiting incompatible development types, such as industrial uses, in the downtown area.

BLOCK 2 Focus Area



Figure 52: As Block 2 changes, initial projects could include sidewalk improvements and trail connections. Supporting the redevelopment of the grain elevator property may include policy changes and blighted property incentives.

BLOCK 3 Focus Area



Figure 53: Collaborative efforts downtown include a parking lot update project that improves the functionality of the Fire Station parking lot for all parties. Paving the parking lot will go a long way toward improving downtown aesthetics. A new street between Center St. and Park Dr. will help fire and emergency response. Working with the Senior Center on strong connections to community spaces at the grain elevator lot will help maintain the strong social connections downtown.

2. Policy: Focus on the relationships of buildings to the street.

- a. Buildings should be close to sidewalks and to the street on downtown thoroughfares. Review setback requirements and reduce if needed.
- b. The primary entrance of a building should be oriented toward the street; buildings should be accessible from the sidewalk.
- c. On-street parking must be encouraged over parking lots.
- d. If parking lots are necessary, they should be located behind buildings and should match the scale of the downtown area.
- e. Consider eliminating off-street parking requirements. Allow for shared parking arrangements and for on-street parking spaces to contribute toward parking needs of properties.

- f. Frontage roads should be discouraged. They impair walkability, make traffic operations more rough, lead to access spacing challenges, and diminish the tax base, as just one frontage is developable.
- g. Plan for streetscape improvements that beautify downtown and increase pedestrian comfort, even if they will come at a later time. This means leaving enough space for boulevard tree plantings, bike parking, street lighting, etc.

3. Collaboration: Develop incrementally - together.

Building successful rural downtowns is a

BLOCK 4 Focus Area

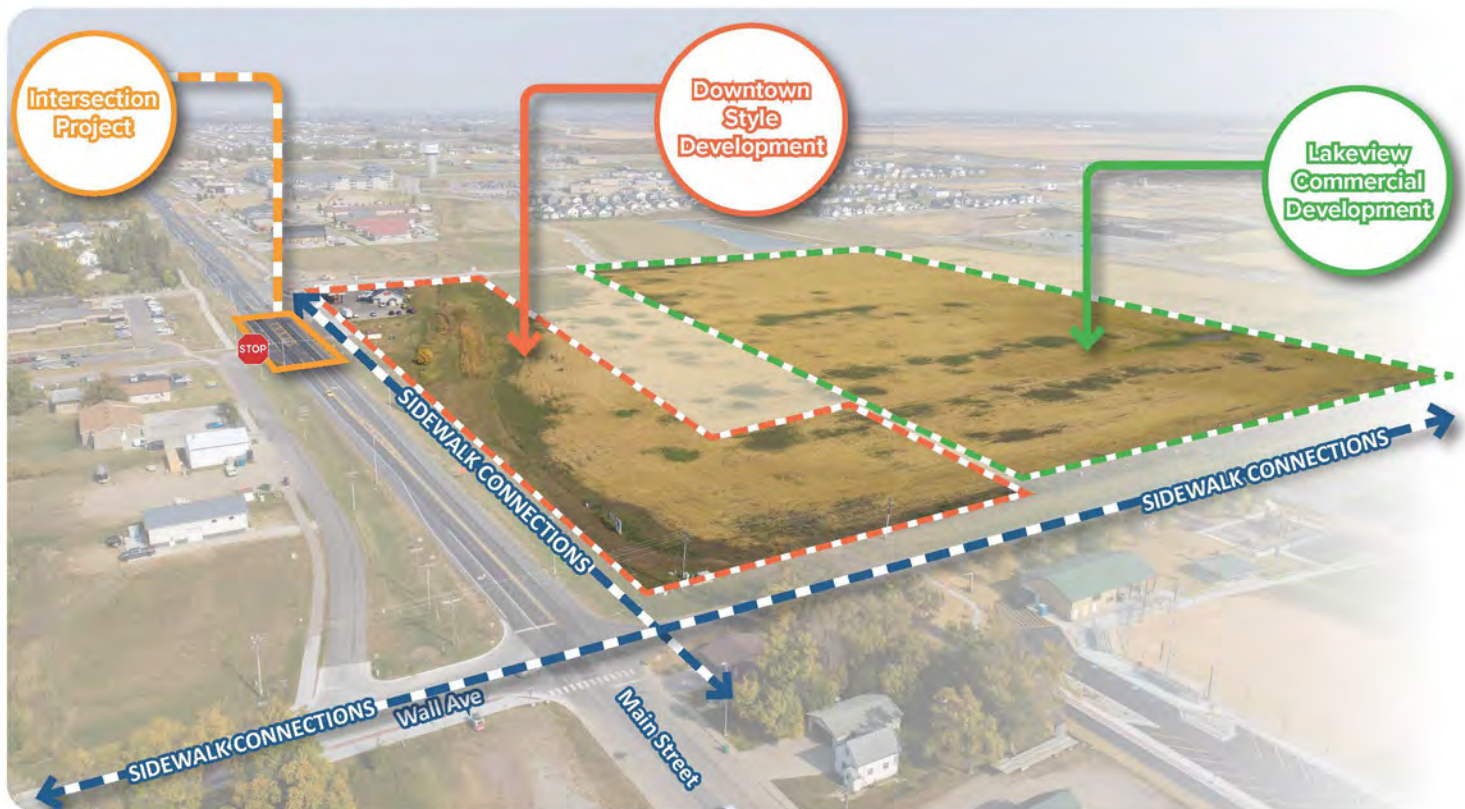


Figure 54; The Lakeview development can contribute to extending the 'small town' downtown district by following updated downtown ordinances. This will help slow traffic near the elementary school and be a transition zone between downtown-style development and the other commercial development at Lakeview.

collaborative effort; a "dance" between local government and the community. Municipal investments in areas like streetscaping and beautification create a more pedestrian-friendly environment. However, these efforts only truly pay off when the business community is prepared to capitalize on the improvements and the broader community is ready to support new ventures. The City of Horace can lay essential groundwork by:

- a. Supporting economic development and entrepreneurs, possible through a more formal, Horace Economic Development Organization.
 - i. Provide organizational support, grant support, or other incentives to draw entrepreneurs downtown.
 - ii. Advocate for Horace's needs with the Greater Fargo-Moorhead Economic

- Development Corporation (GFMEDC) and regional municipalities.
- iii. Retain existing businesses and attract new businesses.
- iv. Market the community's assets.
- v. Actively develop and support new entrepreneurs.
- vi. Build a collaborative network of businesses, community leaders, and governmental entities to develop and implement long-term economic development strategies.
- vii. Collaborate with regional economic development organizations to represent Horace's small town economic development needs and goals.
- b. Supporting community-building opportunities. Continuing to support and take part in Bean Days and other creative

BLOCK 5 Focus Area

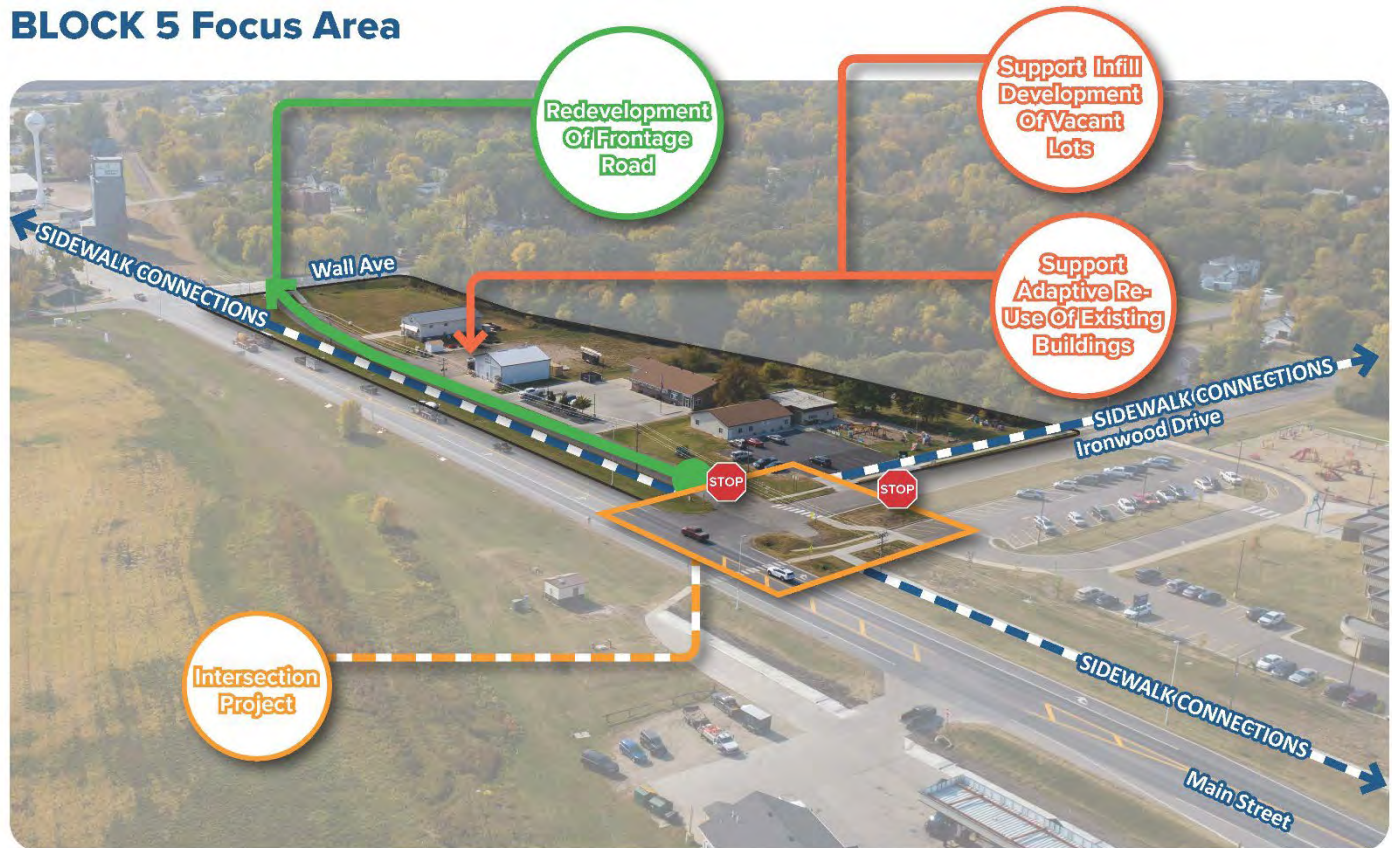


Figure 55: Closing off the Main St. frontage road to Ironwood Drive and rethinking the Main-Ironwood intersection will help with intersection safety near the elementary school.

placemaking activities help to build a more cohesive and collaborative social environment.

- c. Using tax incentives sparingly. Except for blighted properties, local tax incentives are generally a poor tool for downtown. A thriving downtown will consist of compact, valuable properties that punch above their weight in their contributions to the general fund. The revenue potential of these parcels should be tapped, not sapped.
4. **Collaboration: Take advantage of available resources.** Re-building downtown is a complicated interaction between the physical, social, political, and financial environment of a community. State and national organizations are available to help:
 - a. North Dakota Department of Commerce Grants:

- i. **Main Street ND Initiative – Become a ND Main Street Community!**
- ii. North Dakota Community Development & Rural Prosperity helps prepare communities for economic development, diversification, and placemaking to assist in recruiting and retaining talent. They support the renovation of historic buildings, development of mixed use buildings, the revitalization of Main Street, and the creation of Walkable neighborhoods
- iii. Innovate ND; Grants to support customer and market research for entrepreneurs.
- iv. Tourism and Community Enhancement Grants; enhancing tourism, community infrastructure, and historic preservation in ND.

- v. Mainstreet.org Main Street America has tools, strategies, and grants to help communities develop vibrant downtowns. The Project for Public Spaces helps communities create vital community-powered public spaces that highlight community assets, spur rejuvenation, and respond to community needs (Pps.org).

5. **Projects: Think Small!** Change is accomplished in communities through small steps. Tactical Placemaking is a small step, low-cost way to test ideas for everything from multimodal transportation strategy, community-building, to economic development. It gets people out in the common spaces of the community, builds on community identity, and creates unity and civic spirit. Other small towns have used these low-cost ideas to get some energy going in their towns:

Make sure to engage with downtown business owners.

Gravel parking lots downtown drag a lot of dust and gravel onto sidewalks – sweeping sidewalks can have a big impact on the appeal of downtown.



- c. **Paint a mural.** Several downtown structures have blank walls that could use some color. Have a community contest to design a mural and engage with a High School art class to paint it or apply for a grant from the North Dakota Council on the Arts and have a professional artist execute it. Some communities even have artists outline the concept and have residents paint it! If painting a mural is too permanent, create a mural with sidewalk chalk.
- d. **Plant some flowers.** Streetscaping doesn't

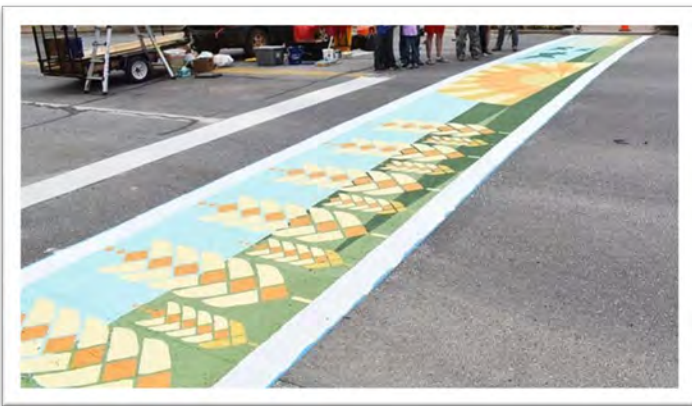


Figure 56: Image credit Enid News and Eagle; Enid, OK

- a. **Paint crosswalks** at main intersections or at the elementary school intersection with murals that recall the community's history and identity. This has the added benefit of slowing traffic and increasing pedestrian visibility at key intersections.
- b. **Host a town cleanup day** and post it widely on social media. This is an easy, low-cost way to get neighbors out meeting each other, promotes a sense of community with the City of Horace, and is convenient for people who can't serve on a long term committee but have a few hours to participate in civic life.



Figure 57: Centerville, SD image credit: Tanya Olson

have to be a major effort. Planters at the corners of Main Street and Center Avenue can be low cost and could be sponsored and cared for by different community groups or

businesses. It's an easy and inexpensive way to express investment in the community.



- e. **Pop-Up Marketplace.** A unique and low-cost economic development idea to activate an empty lot is to create a Pop-Up Marketplace. This can be similar to Farmer's Markets under tents or in a vacant building with multiple "booths". Some communities, like Muskegon, MI in the image above, are even creating small villages of inexpensive garden sheds repurposed as pop-up shops. Entrepreneurs can rent the sheds for a low cost and try out their business idea. This works to stimulate new businesses AND activate the community.



Figure 58: image credit Visit Muskegon

Larger Catalytic Strategies

These are larger strategies, policies, collaborations, and projects that will require a bigger lift to get accomplished. These are strategies that may take

years to accomplish but will ultimately get the community closer to its vision.

- 6. **Collaboration: Special Tax Assessments.** Special tax assessments were a frequently mentioned issue in public engagement comments. Comments indicate that residents may have a poor understanding of the relationship between special tax assessments, desired amenities, and business development and growth.
 - a. Engage with the community regularly about special tax assessments and how they fund desired amenities. The Fiscal Impact Analysis will help get the community on the same page about making smart choices about development and the transportation network.
- 7. **Policy: Highway 17 Commercial Development.** There is a strong preference from the community to avoid more highway commercial development and "strip malls". Many prefer the concentrated commercial development of a traditional small-town downtown. Conversely, the Horace 2045 Comprehensive Plan Future Land Use Plan scatters highway commercial development along Highway 17 north of Wall Avenue. While this type of decentralized commercial development can expand neighborhood access to businesses, it tends to result in auto-oriented, strip mall type development which is challenging to safety walk and bike to. There is particular concern about high-density commercial and housing development northwest of the CR 17 / 76th Avenue S. intersection.
 - a. Revise the *Horace 2045 Comprehensive Plan* Future Land Use Plan and related sections to better reflect community sentiment and preserve the small-town character of Horace.
 - b. Scale back commercial and mixed-use zoned areas to key nodes and intersections; focus development.
 - c. Consider revising zoning requirements for dispersed commercial areas to achieve a

more neighborhood-oriented feeling and neighborhood integration. This could include:

- i. Reduced setbacks.
- ii. Reduced parking requirements.
- iii. Shorter building heights.
- iv. Modified standards for lighting and signage (reducing nighttime impacts to neighborhoods).
- v. Landscaping/buffering to shield business parking from residential uses.

8. Collaboration: Work with the owner of the grain elevator property to retain this area as the heart of downtown Horace. The grain elevator property is at the heart of Horace's community – it's the backdrop for Bean Days activities and a strong reminder of Horace's agricultural heritage. It is an important location to the community and care should be taken with its redevelopment.

- a. This is a prime example of blighted property where targeted incentives would be appropriate.
- b. Designating certain properties as blighted properties or areas will help the community access certain grants to assist with redevelopment costs.

9. Projects: Reconstruct downtown corridors to support private investment. These are visionary projects specific to Main Street and Wall Avenue.

- a. Priority downtown corridors
 - i. Priority 1: Main Street from Center Avenue to Wall Avenue reconstruct as 2-lane with on-street parking and wide sidewalks both sides. Streetscaping elements can be incrementally installed as growth and development continues downtown.
 - ii. Priority 2: Wall Avenue from Main Street to Nelson Drive reconstruct as an urban 2-lane with on-street parking both sides, shared

use path south side, and wide sidewalk along north side. Streetscaping elements can be incrementally installed as growth and development continues downtown and in the Lakeview Addition.

- iii. Priority 3: Strengthen multimodal connection to the Horace Senior Center for people walking and biking.

10. Projects: Implement Downtown Street, Sidewalk, and Intersection priorities from the multimodal transportation toolboxes. These are specific to the downtown study area and may not reflect community-wide priorities.

- a. Priority Downtown intersections
 - i. Priority 1: Wall Avenue & Main Street including the Wall Avenue & Main Street frontage street
 - ii. Priority 2: Main Street & Center Avenue
 - iii. Priority 2: Main Street & Park Drive
- b. Priority Downtown-Adjacent Intersections
 - i. Priority 1: Main Street & Ironwood Drive
 - ii. Priority 2: Main Street & Lakeview Drive
- c. Priority Downtown Connections
 - i. Main Street to Ironwood Drive
 - ii. Wall Avenue east

11. Policy: Adopt comprehensive land use strategies that preserve rural character. Horace is in the process of transitioning from being a "*Resource-dependent Community*"⁹ with an economy dependent on agriculture to an "*Edge Community*"¹⁰ located at the fringe of a metropolitan area with easy access to metropolitan economic opportunities, jobs, and services. Edge Communities have been growing faster than their adjacent metro areas because of land availability and quality of life. The increased availability of developable land due to permanent flood protection has exacerbated the development pressure in Horace. With the elimination of the railroad and grain elevators no

⁹ Putting Smart Growth to Work in Rural Communities: Achieving Economic Vitality through Smarter Growth in Rural Communities and Small Towns; 2010 International City/County Management Association; Mishoveky, Dalby, Bertaina, Read, McGalliard, et al.

¹⁰ Putting Smart Growth to Work in Rural Communities; 2010 ICMA

longer in use, Horace is also transitioning from an industrial-focused downtown to a commercial- and retail-focused downtown.

Like Horace, many Edge Communities want to maintain their rural character while also having economic growth that supports a high quality of life. Metropolitan-style development is not always a good fit for the community. Along with development pressures come resultant challenges, such as housing affordability, traffic congestion, and the sense that something vital about the rural community is being lost. Rapid growth also stretches the ability of the municipality to meet the infrastructure and service needs of the growing community. Residential development without the balanced commercial development to support the local tax base can have poor long-term sustainability.

Communities in this situation understandably seek strategies to manage growth in a way that promotes prosperity but sustains the character of the community in the long run. Careful land use planning and thoughtfully developed ordinances can help communities better control the pace of growth and direct it to locations and sectors that serve community wants and needs best. *Essential Smart Growth Fixes for Rural Planning, Zoning, and Development Codes*¹¹ has some recommendations that are particularly relevant to Horace. This document contains proven tools and strategies specifically to help rural communities determine **how, when, and where** to grow.

Some of the following recommendations expand on strategies from *Essential Smart Growth*.

12. Policy: Adopt form-base zoning standards or overlay zones to promote small-town character in appropriate locations.

- a. Review Zoning and Ordinances for adherence to small-town character for the Old Town and 17/76 zoning districts.
 - i. Review building height min & max to better reflect small town character.
 - ii. Review and expand building materials to include greater building material diversity.
 - iii. Update parking requirements to include on-street parking, reduce off-street parking requirements in certain districts, and allow for shared parking.
 - iv. Add minimum planting area for street trees
- b. Develop a form-based overlay zone in critical areas or corridors, such as the Old Town zoning district, particularly along Main Street north to Lakeview Drive and along Wall Avenue east to Front Street.
- c. Develop a form-based overlay zone in critical areas of the 17/76 zoning district (see Land Use Strategies).

13. Policy: Determine Areas for Growth and Preservation

- a. Identify priority areas for development and incentivize development in those areas. Development incentives, if used, should be applied to clearly defined priorities and have clear approval criteria.
 - i. Downtown core development and redevelopment, particularly along Main Street between Center and Wall.
 - ii. Certain uses or types of development that provide high community benefit, such as a grocery store or hardware store.
 - iii. Locations with existing infrastructure or that are readily serviceable.
- b. Modify the future land use map and future land use category descriptions so that they reflect the community vision, depict

¹¹ 2010, United States Environmental Protection Agency, Office of Sustainable Communities; Nelson, Kevin AICP, et al.

preferred development areas, and clearly describe the desired mix of uses and community design principles for each area.

14. Policy: Incorporate Fiscal Impact Analysis in Development Reviews

- a. Fiscal Impact Analysis allows a community to understand the long-term public and private costs and benefits of infrastructure construction and maintenance.
- b. Requiring Fiscal Impact Analysis for certain developments (e.g., proposed subdivisions or annexations with 10 or more lots) would help the City of Horace understand the full range of costs and benefits of a proposed development, improve transparency, manage the pace of development, and allow City leaders to think carefully about development that doesn't pencil out for the City. This will also help give the community a clearer sense of who pays for infrastructure costs – developers, existing residents, and/or future taxpayers. Special assessments can still be used, but City leaders and residents should be aware of their long-term obligations and liabilities.
- c. Developers that can demonstrate benefits in line with the community's vision are more likely to have resident and stakeholder support.
- d. In lieu of Fiscal Impact Analysis, the City could also examine its fiscal sustainability holistically by studying citywide revenues and service costs as part of a Comprehensive Plan update.

15. Policy: Reform Rural Planned Unit Developments

- a. PUDs are attractive to developers due to flexibility. However, increased flexibility can also lead to inconsistent standards and development review. PUDs don't always reflect community planning goals and are hard to modify once they are approved.
- b. PUDs can have adverse impacts on rural areas and strain government resources.

- c. Consider PUD ordinances related to the community's vision for maintaining a small-town atmosphere.

16. Policy: Right-Size Rural Roads

- a. Rural roadways help define rural character and often determine the type of development adjacent to the roadways.
- b. Incompatible traffic uses of rural roads (trucks or pedestrians) can make roadway design challenging.
- c. Preserve existing rural neighborhood roads (shared streets with narrow lanes, no sidewalks, and stormwater ditches)
- d. Develop design standards for rural neighborhood roads that can be used in lower density neighborhoods.

17. Policy: Encourage Appropriate Densities on the Periphery

- a. Rural communities sometimes allow land development patterns that are not dense enough to provide cost-effective services and infrastructure, but are too dense to maintain a rural feel. The rural tax base is inadequate to provide urban services to these developments.
- b. Setting densities for peripheral areas gives land owners and developers more predictability.
- c. Establish community service areas in the Comprehensive Plan to limit service provision to peripheral areas.

18. Policy: Protect Agricultural and Sensitive Natural Areas

- a. Protecting working lands and farms contributes to the economy while preserving rural character.
- b. Protecting natural resources, particularly waterways, drainage-ways, and open space, helps with stormwater management.
- c. In Horace, the mature tree canopy is also a distinctive feature that sets the community apart from encroaching urban areas. Existing forest cover should be maintained or

integrated into future development as much as possible.

- d. Identify and map important cultural and natural resources that protect the rural nature of the community.

IMPLEMENTATION PLAN



This plan is intended to be a working document that can be updated as needed or modified as necessary to reflect the current conditions of the day. A description of funding sources is also provided.

Implementation tasks are organized into the following three (3) categories:

POLICY	COLLABORATION	INFRASTRUCTURE
Policies and ordinances are organized together. A policy is a course of action, principle, procedure, or strategy that is typically adopted by City leadership to guide official decision-making. Policies are official stances or guidance of the City Horace, including planning activities or providing formal guidance but not necessarily legally enforced. An ordinance is an official regulation enacted by Horace's City Council. Once adopted, ordinances become legally enforceable laws of the city.	While many of the tasks require interdepartmental collaboration, these tasks require collaboration with entities outside of City Departments.	Infrastructure projects are government-authorized construction, design, or demonstration activities that take place on or in support of city-owned properties. Projects can involve the development and implementation of technology tools or physical improvements such as new construction, renovation, or demolition of physical assets of the municipality.

IMPLEMENTATION KEY

Each task follows the Implementation Key; identifying the implementation goal, timeline, estimated cost/level of effort, and guiding principles that each recommendation meets. Due to the fast pace of growth in the City of Horace, many tasks are recommended for implementation in the short term to address issues and take advantage of opportunities afforded by this growth.

Goal

Each goal has a unique identifier coded to reflect the type; **Downtown (D) or Transportation (T)** and category of implementation; Policy (P), Collaboration (C), or Infrastructure (I). For example a Downtown Policy implementation goal would be coded as: **DPX.X**. The 'X.X' portion provides unique identification and/or reference to the description of the implementation from the [Realizing the Plan Chapter](#).

Description

Concise description of the implementation goal. Includes reference to the [Realizing the Plan Chapter](#) where a more detailed description is provided, as applicable.

Timeframe/Priority

Timeframes are also listed to indicate priority of implementation goal and are organized as follows:

Ongoing	A regular part of departmental City tasks. Typically high priority.
Short Term	1-5 years to study, recommend, and implement. High priority; things to do right away. (2026-2030)
Medium Term	6-10 years to study, recommend, and implement. Things to do right after short term priorities. (2031-2035)
Long Term	11+ years to study, recommend, and implement. Things to do as time goes on and the vision of Plan takes shape. (2036+)

Guiding Principles

Derived as critical, consistent themes across public engagement, reflecting public sentiment for the future of Horace. Guiding principles underly all the Downtown Neighborhood Plan’s goals and implementation strategies. They describe the qualities that are most important to community members. Some implementation steps align closer to certain guiding principles over others

-  Small Town Rural Identify
-  Accessible by Walking, Biking, and Driving
-  Utility Improvements and Maintenance
-  High Quality of Life
-  Activities for Everyone

Cost


















Indication of the cost or level of effort for implementation.
















- \$ Cost of tasks are incorporated into regular City and departmental budgets.
- \$\$ Costs of tasks may require some outside expertise or additional investments.
- \$\$\$ Cost of tasks will require significant investment in outside expertise or work

POLICY

Policy recommendations include changes to internal policies, ordinances or approaches to the actions or obligations of the City. These tasks are identified as priorities by City leadership or elected officials and are directed to staff and/or legal counsel to research and develop. Policy and Ordinance changes are generally recommended by City staff for review and approved by the City Council on an as-needed basis triggered by updates from each department's annual policy review or in response to changing trends, conditions, or emerging needs.





Downtown Policy
































Downtown Policy		Timeframe	Guiding Principles	Cost
Goal	Description			
DP1	Allow a diversity of building types.	Short		\$
DP1.a	Restrict building heights to two stories.	Short		\$
DP1.b	Allow a variety of building materials.	Short		\$
DP1.c	Enact maximum sign sizes and quantities.	Ongoing		\$
DP1.d	Avoid large blank walls along downtown sidewalk thoroughfares.	Medium		\$
DP1.e	Require pedestrian-scale elements on downtown sidewalk thoroughfares.	Short		\$
DP1.f	Prohibit incompatible downtown development.	Ongoing		\$
DP2	Focus on the relationship of buildings to the street.	Ongoing		\$
DP2.a	Require zero lot line development on downtown thoroughfares.	Short		\$
DP2.b	Require primary building entrances to face downtown thoroughfares	Short		\$
DP2.c	Encourage on-street parking over parking lots.	Short		\$
DP2.d	If parking lots are necessary, require them to be located behind buildings and do not allow placement between downtown buildings or at prominent corners.	Short		\$
DP2.e	Eliminate off-street parking requirements. Allow shared parking arrangements.	Short		\$
DP2.f	Discourage frontage roads.	Short		\$
DP2.g	Time streetscape improvements with downtown development. Provide space for boulevard trees, bike racks, street lights, planter boxes, etc.	Ongoing		\$
DP7	Consider more focused CR 17 commercial development that residents want to see north of downtown.	Short		\$\$\$
DP7.a	Revise the <i>Horace 2045 Comprehensive Plan</i> Future Land Use Plan/Map to reflect community sentiment.	Short		\$\$\$












Downtown Policy		Timeframe	Guiding Principles	Cost
Goal	Description			
DP7.b	Focus commercial and mixed-use land uses to key corridors, nodes, and/or intersections.	Short		\$\$
DP7.c	Revise zoning and land development requirements to achieve more neighborhood-oriented commercial development and neighborhood integration.	Medium		\$
DP11	Adopt comprehensive land use strategies that preserve rural small town character.	Short		\$\$
DP12	Adopt form-based zoning standards or overlay zones to promote small town character in appropriate locations.	Medium		\$\$\$
DP12.a	Review zoning code and ordinances for adherence to small town character for the Old Town and 17/76 zoning districts.	Short		\$
DP12.a.i	Review building height minimum & maximum.	Short		\$
DP12.a.ii	Review and expand building materials to include greater diversity/flexibility.	Short		\$
DP12.a.iii	Update parking requirements.	Short		\$
DP12.a.iv	Add minimum planting area for boulevard trees.	Short		\$
DP12.b	Develop form-based overlay zone in Old Town zoning district, particularly along Main St., Lakeview Dr., and Wall Ave.	Short		\$\$\$
DP12.c	Develop form-based overlay zone in 17/76 zoning district.	Short		\$\$\$
DP13	Determine areas for growth and preservation.	Ongoing		\$
DP13.a	Identify priority areas for development and market development in those areas.	Short		\$
DP13.a.i	Downtown core development, particularly along Main St.	Ongoing		\$
DP13.a.ii	Prioritize certain uses or types of development such as a grocery store, hardware store, and other essential goods and services.	Ongoing		\$
DP13.a.iii	Prioritize existing infrastructure or that are readily serviceable.	Ongoing		\$
DP13.b	Modify Future Land Use Map and future land use category descriptions to reflect the community vision.	Short		\$
DP14	Incorporate fiscal impact analysis in development reviews.	Short		\$\$
DP14.a	Understand long-term public and private costs of infrastructure construction and maintenance.	Short		\$\$

Downtown Policy		Timeframe	Guiding Principles	Cost
Goal	Description			
DP14.b	Require for development and annexations. Prepare to make decisions about proposals that do and do not pencil out for the City's long-term obligations and liabilities.	Short		\$
DP14.c	Encourage developers to demonstrate benefits in line with Horace's vision for the city.	Ongoing		\$
DP14.d	In lieu of a fiscal impact analysis requirement, the City could examine fiscal sustainability holistically.	Medium		\$\$
DP15	Reform rural planned unit developments (PUDs).	Medium	 	\$
DP15.a	Update requirements to balance development review efforts and reflection of community goals.	Medium	 	\$
DP15.b	Identify PUD adverse impacts.	Short	 	\$
DP15.c	Consider PUD ordinances related to the community's vision and desirability for small town character.	Medium	 	\$
DP17	Encourage appropriate Development density on the periphery of city limits.	Medium	  	\$\$
DP17.a	Establish development density requirements that provide cost-effective public services and infrastructure.	Short	  	\$\$
DP17.b	Provide more predictability and less land speculation across Horace's jurisdiction (consider also Extraterritorial jurisdiction).	Short	  	\$\$
DP17.c	Establish community service areas in the Comprehensive Plan to focus more cost effective development.	Medium	  	\$\$
DP18	Protect agricultural & sensitive natural areas.	Ongoing	  	\$
DP18.a	Protect working agricultural lands and farms to contribute to local economy and preserve Horace's character.	Ongoing	  	\$
DP18.b	Protect natural resources.	Ongoing	  	\$
DP18.c	Protect existing forest cover and mature trees. Integrate existing forests/trees into future development as much as possible.	Ongoing	  	\$
DP18.d	Identify and map important cultural and natural resources that protect the character of the city.	Ongoing	  	\$

Transportation Policy

Transportation Policy		Timeframe	Guiding Principles	Cost
Goal	Description			
TP1.1	Adopt a Complete Streets Policy	Short		\$
TP1.2	Incorporate Complete Streets Policy into Ordinances as necessary	Ongoing		\$
TP1.3	Establish Street Design Guidelines for existing and future streets	Short		\$\$
TP1.4	Update Subdivision Ordinance to reflect Street Design Guidelines, existing, and low-impact development types including rural street cross sections	Ongoing		\$\$
TP1.5	Update and refine list of Multimodal Strategies, as necessary	Ongoing		\$\$
TP1.6	Amend Horace Comprehensive & Transportation Plan to reflect Multimodal Transportation Strategy Guidelines and Street Types	Short		\$\$
TP1.7	Conduct a Safe Routes to School Study.	Medium		\$\$\$
TP16	Right-size rural roads.	Short		\$
TP16.a	Identify type of development appropriate for rural road context (no curb & gutter, ditch/swale stormwater).	Short		\$
TP16.b	Identify incompatible land uses that can make rural roads inappropriate for use (e.g. heavy trucks & pedestrians).	Short		\$
TP16.c	Preserve existing rural neighborhood roads.	Ongoing		\$
TP16.d	Develop design standards for new or reconstructed rural neighborhood streets.	Short		\$\$
R.2.a.i	Time R.2 multimodal improvements with future Wall Ave. improvements and phasing. Prioritize multimodal improvements in phasing.	Ongoing		\$\$\$
R.5.c	With R.5 Drain 27 multimodal improvements, plan to construct grade separations of Drain 27 trail at arterial and collector streets.	Ongoing		\$\$\$
R.5.d	With R.5 improvements, plan to construct raised crosswalks and/or curb extensions for Drain 27 trail at local streets.	Ongoing		\$\$
R.5.e	With R.5 improvements, plan to construct bike and pedestrian crossings of Drain 27 at least every 1/2-mile.	Ongoing		\$\$\$

Transportation Policy		Timeframe	Guiding Principles	Cost
Goal	Description			
R.7.d	With R.7 Front St. multimodal improvements, plan to construct conventional crosswalks and/or curb extensions at future street crossings/intersections as development continues.	Ongoing	 	\$\$
R.6.a	With R.6 Southwest Alignment multimodal improvements, plan to construct an off-street trail and/or shared use path from residential neighborhoods south and west of 100 th Ave. S. and CR 17	Ongoing	  	\$\$\$
R.6.b	Align R.6 Southwest Alignment improvements away from major section-line roads such as 100 th Ave. S. and CR 17.	Ongoing	  	\$
R.6.c	With R.6 Southwest Alignment improvements, plan for two primary connections to downtown/Wall Ave. S. and to Meadowlark Park.	Ongoing	  	\$\$\$
R.6.e	With R.6 improvements, plan for bike and pedestrian crossing of Sheyenne River to establish Meadowlark Park connection.	Ongoing	  	\$\$\$
R.6.f	With R.6 improvements, plan for grade separations at arterial streets.	Ongoing	  	\$\$\$
R.6.g	With R.6 improvements, plan for crosswalk striping, curb extensions, and RRBs as applicable at collector streets.	Ongoing	  	\$\$
R.6.h	With R.6 improvements, plan to construct conventional crosswalks at local streets.	Ongoing	  	\$
R.3.c	With R.3 Main St./CR 17 multimodal improvements, plan to time construction with development south of Park Dr.	Ongoing	 	\$\$\$
R.9.c	With R.9 76 th Ave. S. multimodal improvements, west of CR 17, plan to construct shared use path on at least one side of 76 th Ave. S. East of CR 17, plan to construct shared use path on both sides of 76 th Ave S.	Ongoing	  	\$\$\$
R.9.d	With R.9 improvements, on 76 th Ave. S., plan to install protected crossings (HAWK signal or construct a median refuge island with RRFBs) every 1/2-mile or at collector streets, whichever distance is less.	Ongoing	  	\$\$\$
R.8.a.ii	With R.8 64 th Ave. S. multimodal improvements, consider north side shared use path on 64 th Ave. S. from CR 17 to 57 th St. S. depending upon future 64 th Ave. S. and I-29 interchange and coordination with City of Fargo's network.	Ongoing	  	\$\$\$

Transportation Policy		Timeframe	Guiding Principles	Cost
Goal	Description			
R.8.c	With R.8 64 th Ave. S. multimodal improvements, west of CR 17, plan to construct shared use path on at least one side of 64 th Ave. S. East of CR 17, plan to construct shared use path on both sides of 64 th Ave S.	Ongoing		\$\$\$
R.8.d	With R.8 64 th Ave. S. multimodal improvements, on 64 th Ave. S., west of CR 17, plan to install protected crossings (HAWK signal or construct a median refuge island with RRFBs) every 1/2-mile or at collector streets, whichever distance is less.	Ongoing		\$\$\$
R.8.e	With R.8 improvements, on 64 th Ave. S., east of CR 17, plan to construct grade separations or at the very least protected crossings (HAWK signal or construct median refuge island with RRFBs) every 1/2-mile or at collector streets, whichever distance is less.	Ongoing		\$\$\$
I.1.c	At Main St. & Wall Ave. intersection, plan to construct a Wall Ave. eastbound left-turn lane by 2031.	Short		\$\$
I.1.d	Evaluate traffic signal control with pedestrian-activated lead intervals, multimodal priority, and permissive detection at Main St. & Wall Ave. intersection.	Medium		\$\$
I.1.e	Prioritize right-sizing infrastructure at Main St. & Wall Ave. intersection; one of the most critical intersections for multimodal connection and downtown character.	Ongoing		\$
I.1.f	Prioritize Main St. & Wall Ave. intersection as a safe route to school.	Ongoing		\$
I.2.c	Main St. & Ironwood Dr. turn lane improvements should only be considered with closure of thru-traffic on Main St.'s west side frontage road: Plan to construct an Ironwood Dr. eastbound turn-lane by 2029.	Short		\$
I.2.d	Evaluate all-way stop at Main St. & Ironwood Dr. intersection. Future traffic signal may be warranted.	Short		\$\$
I.2.e	Evaluate vehicle speed reduction in front of Horace Elementary school; speed limit is posted 25mph but data shows 85% of vehicles are traveling 37mph or faster.	Short		\$
I.2.f	Prioritize Main St. & Ironwood Dr. intersection as a safe route to school.	Ongoing		\$
I.3.b	Study traffic impacts associated with Lakeview Development on the east side of CR 17 to right-size traffic control and multimodal improvements at the I.3 intersection of CR 17 & Lakeview Dr.	Medium		\$\$

Transportation Policy		Timeframe	Guiding Principles	Cost
Goal	Description			
I.4.b	Plan to convert to 4-way stop with timing of downtown development at Main St. & Cener Ave. intersection.	Ongoing		\$\$
I.5.b	Time Main St. & Park Dr. improvements with routes R.1 and R.3 improvements, as applicable.	Ongoing		\$
I.9.c	Consider timing Wall Ave. & Front St. intersection improvements with future Wall Ave. improvements, R.2, and R.7 projects. Prioritize multimodal improvements in phasing.	Short		\$
I.9.d	Prioritize Wall Ave. & Front St. intersection as a safe route to school.	Ongoing		\$
I.10.b	Consider timing Wall Ave. & Nelson Dr./Future Lakeview Dr. intersection improvements with Wall Ave. improvements and R.2 project.	Ongoing		\$
I.10.c	Emphasize priority of the Wall Ave. & Nelson Dr./Future Lakeview Dr. intersection as a critical downtown intersection connecting Main St. to Lakeview development.	Ongoing		\$
I.6.b	Consider timing CR 17 & Liberty Ln. intersection improvements with R.3 project and future development along CR 17.	Ongoing		\$
I.8.c	Consider timing Wall Ave. & 63 rd St. S. intersection improvements with future Wall Ave. improvements and R.2 improvements. Prioritize multimodal improvements in project phasing.	Medium		\$
I.8.d	Prioritize Wall Ave. & 63 rd St. S. intersection as a safe route to school.	Ongoing		\$
I.11.b	Prioritize CR 17 & Chestnut Dr. or 81 st Ave S. intersection as a safe route to school, as applicable.	Ongoing		
I.12.b	Prioritize 63 rd St. S. & 79 th Ave. S. intersection as a safe route to school.	Ongoing		

COLLABORATION







Collaboration recommendations include efforts that will require significant interdepartmental collaboration, efforts that require collaboration with Federal, State, or Regional organizations or partners, and collaboration and engagement with community members and community organizations. It is incumbent on Horace city leadership to cultivate a collaborative environment; this includes shared decision-making, trust, and mutual respect within City departments, with partner organizations, and with the community at large.

Downtown Collaboration

Downtown Collaboration		Timeframe	Guiding Principles	Cost
Goal	Description			
DC1	Develop a strategy to regularly engage with the community about critical challenges and opportunities. The strategy should fit the culture of Horace and ability of the City to commit to engagement.	Ongoing	 	\$\$
DC1.a	Monthly "coffee with Community Development"	Ongoing	 	\$
DC1.b	Ad-hoc "think tank" committee	Ongoing	 	\$
DC1.c	Regular community surveys	Ongoing	 	\$\$
DC1.d	Meet with local developers about the future vision for downtown.	Ongoing	 	\$
DC1.e	Engage on a recurring basis with key downtown stakeholders including but not limited to Southern Valley Fire & Rescue, Senior Center, and downtown businesses and property owners.	Ongoing	 	\$
DC3	Develop incrementally – the vision cannot, and should not be built over night. Time public investment with private investment and utilize resources.	Ongoing	     	\$\$
DC3.a	Start a formal Horace Economic Development Organization.	Short	  	\$\$
DC3.a.i	Provide organizational support, grant support, or other strategic incentives to draw entrepreneurs downtown.	Ongoing		\$\$\$
DC3.a.ii	Advocate for Horace's needs with the Greater Fargo-Moorhead Economic Development Corporation (GFMEDC) and regional municipalities.	Ongoing	 	\$
DC3.a.iii	Retain existing businesses and attract new businesses.	Ongoing	 	\$\$
DC3.a.iv	Market Horace's assets.	Ongoing	  	\$
DC3.a.v	Actively develop and support new entrepreneurs.	Ongoing	  	\$\$
DC3.a.vi	Build a collaborative network of businesses, community leaders, and government entities to develop and implement long-term economic development strategies.	Short	  	\$

Downtown Collaboration		Timeframe	Guiding Principles	Cost
Goal	Description			
DC3.a.vii	Collaborate with other regional economic development organizations to represent Horace's small town economic development needs and goals.	Ongoing	  	\$
DC3.b	Support community-building opportunities and placemaking activities, such as Bean Days.	Ongoing	  	\$\$\$
DC3.c	Use tax incentives sparingly.	Ongoing	     	\$\$
DC4	Take advantage of available resources to enhance downtown.	Ongoing	     	\$
DC4.a	Engage with the North Dakota Department of Commerce on grants and resources.	Ongoing	     	\$
DC4.a.i	Become a <i>North Dakota Main Street Community</i> .	Short	  	\$
DC4.a.ii	Utilize North Dakota's Community Development & Rural Prosperity program.	Ongoing	     	\$
DC4.a.iii	Utilize Innovate ND grants.	Ongoing	     	\$
DC4.a.iv	Utilize Tourism & Community Enhancement grants.	Ongoing	  	\$
DC4.b	Utilize other resources, such as www.Mainstreet.org & www.pps.org	Ongoing	     	\$
DC6	Address special tax assessments through updated policy and practice.	Ongoing	     	\$\$
DC6.a	Engage residents and property-owners regularly about special tax assessments.	Ongoing	     	\$\$
DC8	Work with the owner of the grain elevators to retain property as the heart of downtown Horace and as a catalytic opportunity to bring more vibrancy and economic development to town.	Short	     	\$
DC8.a	Consider designation as a blighted property.	Short	     	\$
DC8.b	Utilize resources and grants for blighted properties.	Short		







Transportation Collaboration

Transportation Collaboration		Timeframe	Guiding Principles	Cost
Goal	Description			
TC1	Collaborate with Metro COG on future federal transportation funding solicitations for project implementation. Advocate for the unique needs of Horace in the context of the regional transportation network.	Ongoing		\$\$\$
TC1.a	Utilize STBGP-U, TA, and/or other federal transportation funding programs through Metro COG.	Ongoing		\$\$\$
TC1.b	Continue participation in Metro COG's Bicycle & Pedestrian Subcommittee.	Ongoing		
TC2	Collaborate with NDDOT to understand and pursue future state and federal funding solicitations for project implementation. Advocate for the unique needs of Horace in the context of the state.	Ongoing		\$\$\$
TC3	Collaboration with other communities across the state to support and refine legislative policy impacting statewide funding for transportation infrastructure.	Ongoing		\$\$\$
TC3.a	Utilize ND's Flexible Transportation Fund and follow recurring legislation related to the fund's distribution across the state.	Ongoing		\$\$\$

INFRASTRUCTURE





Infrastructure projects can be improvements to the built environment of the community or could be technological improvements that help the municipality operate more efficiently or effectively. These are tasks that are above and beyond the usual work of City departments and typical involve the evaluation and selection of outside consultants. They require careful budget planning. Projects may be part of a Capital Improvement Plan (CIP). CIP projects are generally based on collaboration between departments to ensure that necessary steps are in place to properly execute a given project. Infrastructure projects may also be part of annual departmental budgets.

Downtown Infrastructure Projects

Downtown Infrastructure		Timeframe	Guiding Principles	Cost
Goal	Description			
DI5	Use small projects to bring vibrancy downtown.	Short		\$\$
DI5.a	Paint crosswalks at downtown intersections (Main St./Wall Ave. and Main St./Center Ave.) and/or crosswalk at Elementary School.	Short		\$
DI5.b	Host a town cleanup day.	Short		\$
DI5.c	Paint a mural.	Short		\$\$
DI5.d	Plant some flowers.	Short		\$
DI5.e	Explore pop-up marketplace.	Short		\$\$

Transportation Infrastructure Projects














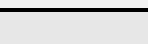





a.

Transportation Projects		Timeframe	Guiding Principles	Cost
Goal	Description			
TI9.a.i	Reconstruct Main St. from Center Ave. to Wall Ave. as a 2-lane with on-street parking, wide sidewalks, and space for incremental streetscape investment.	Medium		\$\$\$
TI9.a.ii	Reconstruct Wall Ave. from Main St. to Nelson Dr. as a 2-lane with on-street parking, wide sidewalks, and space for incremental streetscape investment.	Short		\$\$\$
TI9.a.iii	Strengthen and integrate multimodal connection to the Horace Senior Center for people walking and biking.	Medium		\$\$
TI10	Implement downtown street, sidewalk, and intersection priorities from the multimodal transportation toolboxes	Ongoing		\$\$\$

Transportation Projects		Timeframe	Guiding Principles	Cost
Goal	Description			
R.4	Shared use path, both sides of Main St./CR 17 from Wall Ave. to 52 nd Ave. S.	Short		\$\$\$
R.4.a	Upgrade multimodal infrastructure at downtown intersections.	Short		\$\$\$
R.4.a.i	Main St. & Ironwood Dr. intersection (I.2) strategies.	Short		\$\$\$
R.4.a.ii	Main St. & Wall Ave. intersection (I.1) strategies.	Short		\$\$\$
R.4.a.iii	CR 17 & Chestnut Dr. or 81 st Ave. S. intersection (I.11) strategies.	Long		\$\$\$
R.4.a.iv	Install HAWK signal or median refuge island with RRFBs at CR 17 & 81 st Ave. S.	Medium		\$\$\$
R.4.a.v	Construct grade separation or install HAWK signal at CR 17 & 68 th Ave. S. intersection.	Short		\$\$\$
R.4.b	Construct shared use path to fill gaps in existing network. Prioritize locations closest to downtown.	Short		\$\$\$
R.4.b.i	East side of Main St. from 81 st Ave S. to 76 th Ave. S.	Short		\$\$\$
R.4.b.ii	East side of Main St. from Wall Ave. to Ironwood Dr. (consider timing with I.1 Main St. & Wall Ave. intersection improvements).	Short		\$\$\$
R.4.b.iii	East side of CR 17 from 73 rd Ave. S. to 52 nd Ave. S.	Medium		\$\$\$
R.4.b.iv	West Side of CR 17 from 76 th Ave. S. to 52 nd Ave S.	Medium		\$\$\$
R.5	Off-street trail, at least one side of Drain 27 from south of 100 th Ave. S. to City of Fargo trail north of 64 th Ave. S.	Short		\$\$\$
R.5.a	Construct off-street trail to fill gaps in existing network. Prioritize locations closest to downtown and existing development.	Short		\$\$\$
R.5.a.i	East side of Drain 27 from south of 100 th Ave. S. to Wall Ave. S.	Short		\$\$\$
R.5.a.ii	East side of Drain 27 from south of 76 th Ave. S. to City of Fargo trail north of 64 th Ave. S.	Medium		\$\$\$
R.5.b	Upgrade multimodal infrastructure at existing intersections and plan for future network crossing of Drain 27.	Long		\$\$\$
R.5.b.i	Grade separation at Wall Ave. S.	Short		\$\$\$
R.5.b.ii	Grade separation at 76 th Ave S.	Medium		\$\$\$
R.5.b.iii	Grade Separation at 64 th Ave. S.	Long		\$\$\$
R.5.b.iv	Grade separation at 100 th Ave. S.	Long		\$\$\$
R.1	Off-street trail and yield street on Park Dr. alignment from Boxelder Cir. To Drain 27.	Short		\$\$\$
R.1.a	Acquire right-of-way (ROW) for an off-street trail.	Short		\$\$\$


Transportation Projects		Timeframe	Guiding Principles	Cost
Goal	Description			
R.1.a.i	Both sides of Sheyenne River, west of Independence Park.	Short	   	\$\$\$
R.1.a.ii	Park Dr. alignment from Main St. to Nelson Dr.	Short	   	\$\$\$
R.1.b	Install bike and ped bridge over Sheyenne River just west of Independence Park.	Long	   	\$\$\$
R.1.c	Construct off-street trail.	Long	   	\$\$\$
R.1.c.i	From Boxelder Cir. to Sheyenne Dr.	Long	   	\$\$\$
R.1.c.ii	From Nelson Dr. to Wild Goose Ln.	Medium	   	\$\$\$
R.1.d	Install yield street striping and signage.	Short	   	\$
R.1.d.i	From Sheyenne Dr. to Main St.	Short	   	\$
R.1.d.ii	From Nelson Dr. to Wild Goose Ln.	Short	   	\$
R.1.e	Upgrade multimodal infrastructure at existing intersections.	Long	   	\$\$\$
R.1.e.i	Install conventional crosswalk or curb extensions at Southwood Dr. and Sheyenne Dr.	Long	   	\$\$
R.1.e.ii	Main St. & Park Dr. intersection (I.5) strategies.	Medium	   	\$\$\$
R.2	Sidewalk (north side) and shared use path (south side) on Wall Ave. from Main St. to 57th St. S.	Medium	   	\$\$\$
R.2.a	Construct sidewalk/wide sidewalk (north side) and shared use path (south side). Prioritize areas nearest to downtown	Medium	   	\$\$\$
R.2.b	Upgrade multimodal infrastructure at existing intersections. Prioritize locations closest to downtown.	Medium	   	\$\$\$
R.2.b.i	Main St. & Wall Ave. intersection (I.1) strategies.	Short	   	\$\$\$
R.2.b.ii	Install mid-block crossing to Freed Park with RRFBs and curb extensions or raised crosswalk.	Short	   	\$\$\$
R.2.b.iii	Wall Ave. & Front St. intersection (I.9) strategies.	Medium	   	\$\$\$
R.2.b.iv	Wall Ave. & 63 rd St. intersection (I.8) strategies.	Long	   	\$\$\$
R.2.b.v	Wall Ave. & Nelson Dr./Future Lakeview Dr. intersection (I.10) strategies.	Medium	   	\$\$
R.2.b.vi	Install curb extensions and/or raised crosswalk at Wall Ave. & 62 nd St. S. intersection.	Medium	   	\$\$
R.7	Sidewalk (east side) and shared use path (west side) on Front St. from Wall Ave. to 82nd Ave S.	Medium	   	\$\$\$
R.7.a	Construct shared use path to fill gaps in existing network.	Medium	   	\$\$\$
R.7.a.i	West side of Fron St. from Wall Ave. to 83 rd Ave. S.	Medium	   	\$\$\$
R.7.b	Construct a sidewalk to fill gaps in existing network.	Medium	   	\$\$
R.7.b.i	East side of Front St. from Wall Ave. to St. Anne Ave.	Medium	   	\$\$
R.7.c	Upgrade multimodal infrastructure at existing intersections and plan for future crossing of the street.	Medium	   	\$\$\$

Transportation Projects		Timeframe	Guiding Principles	Cost
Goal	Description			
R.7.c.i	Install mid-block crossing for existing off-street trail north of St. Anne Ave. with RRFBs and curb extensions or raised crosswalk.	Short	   	\$\$\$
R.7.c.ii	Wall Ave. & Front St. intersection (I.9) strategies.	Medium	   	\$\$\$
R.6	Off-street trail/shared use path from south of 100 th Ave. S. to downtown/Wall Ave.	Medium	   	\$\$\$
R.6.d	Acquire right-of-way (ROW) through existing land development procedures and acquisition for route away from 100 th Ave. S. and CR 17 alignment.	Ongoing	  	\$\$\$
R.3	Shared use path, both sides of Main St./CR 17 from Park Dr. to south of 100 th Ave. S.	Long	   	\$\$\$
R.3.a	Upgrade multimodal infrastructure at existing intersections. Prioritize locations closest to downtown.	Long	   	\$\$\$
R.3.a.i	Main St. & Park Dr. intersection (I.5) strategies.	Medium	   	\$\$\$
R.3.a.ii	CR 17 & Liberty Ln. intersection (I.6) strategies.	Long	   	\$\$\$
R.3.a.iii	Construct a grade separation or install a HAWK signal at CR 17 & Sparks Addition street, south of Liberty Ln.	Long	  	\$\$\$
R.3.a.iv	CR 17 & 100 th Ave. S. intersection (I.7) strategies.	Medium	  	\$\$\$
R.3.b	Construct a shared use path to fill gaps in existing network.	Medium	   	\$\$\$
R.3.b.i	Both sides of CR 17 from Park Dr. to south of 100 th Ave. S.	Medium	   	\$\$\$
R.9	Shared use path, at least one side (south side) of 76 th Ave. S. from Sheyenne Diversion Channel to 57 th St. S.	Long	   	\$\$\$
R.9.a	Construct a shared use path to fill gaps in existing network.	Long	   	\$\$\$
R.9.a.i	South side of 76 th Ave. S. from Sheyenne Diversion Channel to 57 th St. S.	Long	   	\$\$\$
R.9.a.ii	North side of 76 th Ave. S. from CR 17 to 57 th St. S.	Long	   	\$\$\$
R.9.a.iii	South side of 76 th Ave. S. from Cub Creek Pkwy. To 57 th St. S.	Long	   	\$\$\$
R.9.b	Upgrade multimodal infrastructure at existing and future intersections. Prioritize locations strategic to downtown connections and connections to Horace Middle and High Schools.	Long	   	\$\$\$
R.9.b.i	Install a HAWK signal or construct median refuge island with RRFBs at Brink Dr./Sunnyside Ct. & 76 th Ave. S. intersection.	Long	  	\$\$\$

Transportation Projects		Timeframe	Guiding Principles	Cost
Goal	Description			
R.9.b.ii	Install a HAWK signal or construct median refuge island with RRFBs at Cub Creek Pkwy. & Future 66 th St S. intersection (future grade separation recommended at Drain 27, just to the east).	Long		\$\$\$
R.9.b.iii	Install a HAWK signal or median refuge island with RRFBs at 78 th St. S. & 76 th Ave. S. intersection (timed with development).	Long		\$\$\$
R.8	Shared use path, at least one side of 64th Ave. S. from Sheyenne Diversion Channel to 57th St. S.	Long		\$\$\$
R.8.a	Construct a shared use path to fill gaps in existing network.	Long		\$\$\$
R.8.a.i	South side of 64 th Ave. S. from Sheyenne Diversion Channel to 57 th St. S.	Long		\$\$\$
R.8.b	Upgrade multimodal infrastructure at existing and future intersections. Prioritize locations strategic to downtown connections and connections to Horace Middle and High Schools.	Long		\$\$\$
R.8.b.i	Install HAWK signal or median refuge island with RRFBs at 64 th Ave. S. & future 9 th St. W. intersection.	Long		\$\$\$
R.8.b.ii	Construct grade separation or install HAWK signal at 64 th Ave. S. & future 66 th St. S. intersection.	Long		\$\$\$
I.1	Maintain existing control and add multimodal improvements at Main St. & Wall Ave. intersection.	Short		\$\$\$
I.1.a	Upgrade multimodal infrastructure at all existing intersection legs with sidewalk/shared use path.	Short		\$\$
I.1.a.i	Construct curb extensions (consider demonstration project).	Short		\$\$
I.1.a.ii	Install crosswalk striping and stop bars.	Short		\$
I.1.a.iii	Include ADA compliance.	Short		\$\$
I.1.a.iv	Install pedestrian scale lighting.	Short		\$\$
I.1.b	Construct full build out as multimodal connections are made or with Wall Ave. reconstruction project (all intersection legs).	Short		\$\$\$
I.1.b.i	Construct curb extensions (consider demonstration project).	Short		\$\$
I.1.b.ii	Install crosswalk striping and stop bars.	Short		\$
I.1.b.iii	Include ADA compliance.	Short		\$\$
I.1.b.iv	Install pedestrian scale lighting.	Short		\$\$

Transportation Projects		Timeframe	Guiding Principles	Cost
Goal	Description			
I.2	Maintain existing control and add multimodal improvements at Main St. & Ironwood Dr. intersection.	Short	   	\$\$\$
I.2.a	Upgrade multimodal infrastructure at north leg of intersection.	Short	   	\$\$\$
I.2.a.i	Install push-activated RRFBs and construct median refuge island.	Short	   	\$\$\$
I.2.a.ii	Upgrade/oversize existing signage and striping and install yield lines at crosswalk.	Short	   	\$
I.2.a.iii	Install pedestrian-scale lighting.	Short	   	\$\$
I.2.b	Demonstration Project: remove cut-thru traffic from Main St.'s west side frontage road. Add jersey barriers north of the Post Office to close from thru-traffic.	Short	   	\$\$
I.2.b.i	Construct a permanent closure of the frontage road north of the Post Office, which should include a turnaround adequate for emergency services and U.S. Postal Service vehicles.	Long	   	\$\$\$
I.3	Maintain existing configuration at CR 17 & Lakeview Dr.	Short	   	\$
I.3.a	Prioritize improvements south (I.2 at Ironwood Dr.) and/or north (at Chestnut Dr.) of this location.	Short	   	\$
I.4	Convert Main St. & Center Ave. intersection to a 4-way stop with multimodal improvements.	Short	   	\$\$\$
I.4.a	Upgrade multimodal infrastructure at all intersection legs.	Short	   	\$\$\$
I.4.a.i	Construct curb extensions	Short	   	\$\$
I.4.a.ii	Install crosswalk striping and stop bars.	Short	   	\$
I.4.a.iii	Update ADA compliance	Short	   	\$\$
I.4.a.iv	Add pedestrian scale lighting.	Short	   	\$\$
I.5	Convert Main St. & Park Dr. intersection to a 4-way stop with multimodal improvements.	Medium	   	\$\$\$
I.5.a	Upgrade multimodal infrastructure all intersection legs.	Medium	   	\$\$\$
I.5.a.i	Construct curb extensions.	Medium	   	\$\$
I.5.a.ii	Install crosswalk striping and stop bars.	Medium	   	\$
I.5.a.iii	Include ADA compliance.	Medium	   	\$\$
I.5.a.iv	Pedestrian scale lighting.	Medium	   	\$\$
I.7	Convert CR 17 & 100 th Ave. S. intersection to a roundabout with multimodal improvements.	Medium		\$\$\$
I.7.a	Include multimodal improvements on all intersection legs.	Medium		\$\$\$

Transportation Projects		Timeframe	Guiding Principles	Cost
Goal	Description			
I.7.a.i	Include median refuge islands.	Medium	   	\$\$\$
I.7.a.ii	Include crosswalk striping and yield lines (time with path connections).	Long	   	\$
I.7.a.iii	Include ADA compliance.	Medium	   	\$\$
I.7.a.iv	Include streetlights.	Medium	   	\$\$
I.9	Add multimodal improvements at Wall Ave. & Front St. intersection.	Medium	   	\$\$\$
I.9.a	Upgrade multimodal infrastructure at all intersection legs.	Medium	   	\$\$\$
I.9.a.i	Construct curb extensions.	Medium	   	\$\$
I.9.a.ii	Install crosswalk striping and stop bars.	Medium	   	\$
I.9.a.iii	Include ADA compliance.	Medium	   	\$\$
I.9.a.iv	Install streetlights.	Medium	   	\$\$
I.9.a.v	Install RRFBs and/or median refuge island at shared use path connection across Wall Ave.	Medium	   	\$\$\$
I.9.b	Install a temporary/demonstration pedestrian crossing improvement.	Short	   	\$
I.10	Convert Wall Ave. & Nelson Dr./Future Lakeview Dr. to a 4-way stop with multimodal improvements.	Long	   	\$\$\$
I.10.a	Include multimodal improvements on all intersection legs.	Long	   	\$\$\$
I.10.a.i	Include curb extensions.	Long	   	\$\$
I.10.a.ii	Include crosswalk striping and stop bars.	Long	   	\$
I.10.a.iii	Include ADA compliance.	Long	   	\$\$
I.10.a.iv	Pedestrian-scale lighting.	Long	   	\$\$
I.6	Add multimodal improvements at CR 17 & Liberty Ln. intersection.	Long	   	\$\$\$
I.6.a	Upgrade multimodal infrastructure at north leg of intersection (time with path connections).	Long	   	\$\$\$
I.6.a.i	Install median refuge island with RRFBs.	Long	   	\$\$\$
I.6.a.ii	Install crosswalk striping and yield lines.	Long	   	\$
I.6.a.iii	Include ADA compliance.	Long	   	\$\$
I.6.a.iv	Install streetlights.	Long	   	\$\$
I.8	Add multimodal improvements at Wall Ave. & 63 rd St. S. intersection.	Long	   	\$\$\$
I.8.a	Upgrade multimodal infrastructure at east and south legs of intersection.	Medium	   	\$\$
I.8.a.i	Install crosswalk striping and stop bars.	Short	   	\$

Transportation Projects		Timeframe	Guiding Principles	Cost
Goal	Description			
I.8.a.ii	Install push-activated RRFBs at shared use path connection across Wall Ave (east leg).	Short	   	\$\$
I.8.a.iii	Include ADA compliance.	Short	   	\$\$
I.8.b	Full build out of multimodal improvements (time with future Wall Ave. project).	Long	   	\$\$\$
I.8.b.i	Include curb extensions.	Long	   	\$\$
I.8.b.ii	Include median refuge island with RRFBs (east leg) and yield lines.	Long	   	\$\$\$
I.8.b.iii	Include crosswalk striping and stop bars.	Long	   	\$
I.8.b.iv	Include streetlights.	Long	   	\$\$
I.11	Add multimodal improvements at CR 17 & Chestnut Dr. intersection or CR 17 & 81 st St. S. intersection.	Long	   	\$\$\$
I.11.a	Upgrade multimodal infrastructure at north or south leg of intersection.	Long	   	\$\$\$
I.11.a.i	Install HAWK signal or grade separation.	Long	  	\$\$\$
I.11.a.ii	Install crosswalk striping and stop bars or yield lines as applicable.	Long	   	\$
I.11.a.iii	Include ADA compliance.	Long	   	\$\$
I.11.a.iv	Install streetlights.	Long	   	\$\$
I.12	Add multimodal improvements at 63 rd St. S. & 79 th Ave. S. intersection.	Long	   	\$\$\$
I.12.a	Upgrade multimodal infrastructure at east and north legs of intersection.	Long	   	\$\$\$
I.12.a.i	Install median refuge island with RRFBs (north leg).	Long	   	\$\$\$
I.12.a.ii	Install crosswalk striping, stop bars, and yield lines as applicable.	Long	   	\$
I.12.a.iii	Include ADA compliance	Long	   	\$\$
I.12.a.iv	Install streetlights	Long	   	\$\$

FUNDING THE PLAN

Funding Horace's future vision of downtown will take substantial public and private investment. The vision cannot come to fruition overnight, and strategic, active pursuit must be taken by the City to focus investment on critical components of the Plan. Below are critical funding resources that could be tapped into in order to achieve the vision.

Private Investment

The vision will not be achieved without private investment. Private landowners and business owners are critical to achieving the future vision for downtown. The strategies identified above will help guide the City of Horace to foster and steer private investment. A balance must be struck by the City to support private investment without deterring potential developers and small business owners. The City must cautiously weigh incentivization of development downtown, so as not to negatively impact the community's bottom-line and ability to pay for infrastructure and future operations and maintenance thereof.

City of Horace Investment

The City of Horace has stable means of paying for infrastructure including the following primary source:

- **Local Sales Tax:** Funding generated by commercial activity which occurs within City limits. The Downtown Neighborhood Plan is an essential tool to help Horace grow the commercial tax base and increase funding that can be used to pay for critical infrastructure, services, operations, and maintenance.
- **Property Tax (Mill Levy):** Funding generated through mill levy, or tax rate applied to the assessed value of a property in City limits. The Downtown Neighborhood Plan may help spur private investment along Main Street and other key areas of town. With more private investment downtown, it is likely assessed property values may increase in currently vacant or underused properties. New developments such as Lakeview and Sparks Additions, near downtown, will also see property assessments increase; therefore, increasing the City's key funding source that can be used to pay for critical infrastructure, services, operations, and maintenance.
- **Special Assessments:** Special assessments are tax assessments levied against properties which benefit from infrastructure projects and/or services. The City must rethink the way it approaches special assessments, which have fueled an incentivization for new housing developments in every corner of the community and the rapid growth Horace has been experiencing. The Downtown Neighborhood Plan provides an opportunity to combat its reliance on special assessments, by encouraging efficient development in core parts of the community with critical existing infrastructure.

Horace must target investment in established parts of the city, through implementation of the Plan. The city has been accommodating green field development for over a decade, which is proven to be much less fiscally efficient than investment in the community's core. The City, through the Plan's vision and implementation guidance herein, should consider setting targets for core or downtown budget line-item(s), whether a percentage of the budget or specific line-item for building downtown momentum, as applicable. By moving the needle just a little bit, the financial returns in the form of sales tax revenue, property tax revenue, and less reliance on special assessments will start to relieve the pressure that both the City and residents are feeling.

The city can also reduce reliance on special assessments by pursuing funding opportunities through the State of North Dakota and Federal Government, in the form of grants. Typically, grant opportunities are available for a wide range of projects, from plans and studies to preliminary engineering and construction. Horace should actively pursue the following resources:

Fargo-Moorhead Metropolitan Council of Governments

- **Surface Transportation Block Grant Program (STBG):** The Surface Transportation Grant Program is a federal transportation funding program administered locally by the Fargo-Moorhead Metropolitan Council of Governments (Metro COG). Funding from STBGP can be used to preserve and improve the conditions and performance on any Federal-aid highway, bridge and tunnel projects on any public road, pedestrian and bicycle infrastructure, and transit capital projects including intercity bus terminals. Projects eligible for STBG funding must be within the Metro COG Urbanized Area Boundary (UZA), be consistent with the Metropolitan Transportation Plan (MTP), and meet general eligibility required for use of federal aid dollars under Title 23 of the US Code 133 Surface Transportation Block Grant Program.
- **Transportation Alternatives (TA):** Transportation Alternatives are federally funded, community-based projects that expand travel choices and enhance the transportation experience by integrating modes and improving the cultural, historic and environmental aspects of transportation infrastructure. TA projects must be one of 10 eligible projects and must relate to surface transportation. Eligible projects include but are not limited to the following:
 - Bicycle & Pedestrian Projects:
 - Construction of on-street and off-street bicycle and pedestrian facilities
 - Construction of Safe Routes to School projects
 - Traffic calming and speed reduction improvements
 - Scenic and Environmental Projects:
 - Streetscape improvements and corridor landscaping
 - Vegetation management practices in transportation rights-of-way
 - Control and removal of outdoor advertising
 - Historic Projects:
 - Historic preservation and rehabilitation of operating historic transportation facilities

State of North Dakota

- **NDDOT**
 - Urban Grant Program (UGP) – The Urban Grant Program provides a funding mechanism focused on reinvesting and fortifying a community’s existing transportation assets which maximizes the public return on investment. The program focuses transportation investments inward toward the established community rather than outward expansion. Projects which directly support a community’s urban core or central business district, and which promote complete streets, walkability, and transit enhancements will be given preferential consideration.
 - Highway Safety Improvement Program (HSIP) – The HSIP program makes about \$6 million in federal funding available for counties and cities to address highway traffic safety issues. The HSIP program is based on the Strategic Highway Safety Plan. To be consistent with the SHSP, HSIP projects must relate to at least one of the six emphasis areas:
 - Lane departures

- Intersections
- Alcohol and/or drug related
- Unbelted vehicle occupants
- Speeding/aggressive driving
- Young drivers
- Flexible Transportation Fund Program (Flex Funds) – The Flex Fund was established by the 68th Legislative Assembly to complement the Federal Aid Highway program and further support an interconnected transportation system in North Dakota. The funds are used to improve transportation infrastructure on and off the state highway system, and to match local funds and federal discretionary grant funds. Non-oil producing counties, and cities in non-oil counties are eligible to apply. Priority will be given to projects that match federal or private funding, improve local corridors, and received preliminary engineering funding in the previous Flex Fund round. This program advances the mission of reducing maintenance and operation costs, efficiently moving people and goods, improving safety, connectivity, and longevity of the existing transportation network.
- **Department of Commerce**
 - Community Development Block Grant (CDBG) – The CDBG Program provides financial assistance to eligible units of local governments in the form of grants and loans for public facilities, housing rehabilitation and economic development projects. The primary beneficiaries of these projects must be very low- and low-income individuals. Examples of projects funded by CDBG in the respective emphasis areas are:
 - Housing – Housing rehabilitation of very low- and low-income homeowner units and rental units within a particular area.
 - Public facilities – Water and sewer projects, removal of architectural barriers, fire halls, ambulances, etc.
 - Economic development – Cities and counties receive funds to loan/grant businesses which create jobs for low-income persons.
 - Vibrancy Grant Program – The Main Street Initiative (MSI) Vibrancy Grant Program supports local efforts to increase community vibrancy by providing seed funding for a small project that will build momentum and inspire additional projects. Community vibrancy improves quality of life, helps attract and retain workforce, and enhances community pride. Eligible projects include public events, public art, the construction of a community gathering space, temporary bike lanes, decorative crosswalks, shared-use path lighting, etc.
 - Rural Slum and Blight Removal Grant – This program supports local governments and/or non-profit organizations in addressing dilapidated and blighted properties and/or facilities in rural areas (population less than 10,000). Eligible grant activities include the purchase and removal of blighted properties, the removal of hazards and the rehabilitation of structures from blighted properties, and up to \$10,000 per business or improvements if the business is located in a mixed-use property.
 - Partners in Planning (PiP) Grant Program – The Main Street Initiative (MSI) Partners in Planning (PiP) grant program supports non-profit organizations and/or local governments in completing a comprehensive plan or economic development/diversification strategic plan. This grant program is intended to empower communities to proactively plan their futures, seek smart, efficient infrastructure development and offer a healthy, vibrant environment for residents and visitors.

- Rural Facility Planning Grant – The purpose of a Rural Facility Planning Grant is to provide funding to support the planning and development of empty or soon-to-be empty facilities or buildings in rural areas (population less than 10,000). This grant is aimed at improving the quality of life, services, and opportunities for residents in rural communities. The grant supports communities in developing comprehensive plans that outline the potential uses of the facilities, with a focus on multi-use concepts, including housing, community spaces, workforce training facilities, and business incubators. Encourage collaboration with relevant organizations and agencies to leverage resources and expertise in addressing empty or soon-to-be empty facilities.
- Innovate ND – Innovate ND is a voucher-based reimbursement program designed to help North Dakota entrepreneurs advance their businesses by supporting customer and market research. Each participant may receive up to \$50,000 in reimbursable expenses, administered through the Entrepreneur Centers. The program is divided into two phases, each offering up to \$25,000 in reimbursements. Participants have six months per phase, with a maximum of 12 months to complete the full program. Program goals include the following:
 - Grow the entrepreneurial ecosystem in North Dakota and enhance economic development and economic diversity in the state.
 - Educate and assist participants on the lean business model canvas, value proposition, and customer identification.
 - Assist participants to validate their product and market by tracking metrics such as investment, sales, or customer interviews.
 - Support the development of new, innovative businesses with clear potential for Primary Sector Certification.
- **Parks and Recreation**
 - Recreational Trails Program (RTP) – The Recreational Trails Program is an 80/20 matching grant program that provides funding for both motorized and non-motorized recreational trail projects. Eligible projects include the construction of new recreational trails, restoration of existing trails, development and rehabilitation of trailside and trailhead facilities and trail linkages, purchase and lease of recreational trail construction and maintenance equipment, land acquisition/easements, trail accessibility assessment.

U.S. Government

Due to changing Administration priorities, some federal grant programs initiated under the Infrastructure Investment and Jobs Act (IIJA) have been paused or are being reevaluated. Future funding cycles for many of the below grant programs will be dependent on the multi-year surface transportation reauthorization legislation which is currently being worked on by Congress and will be voted on in 2026. The grants described below are those that have the best chances of being reauthorized.

- **Safe Streets and Roads for All (SS4A) Grant Program:** The Infrastructure Investment and Jobs Act (IIJA) established the Safe Streets and Roads for All (SS4A) competitive grant program with \$5 billion in appropriated funds over 5 years, 2022-2026. The SS4A program funds regional, local, and Tribal initiatives through grants to prevent roadway fatalities and serious injuries. The SS4A program supports the U.S. Department of Transportation's National Roadway Safety Strategy and its goal of zero roadway deaths using a Safe System Approach. The SS4A program provides funding for two main types of grants: Planning

and Demonstration Grants for Action Plans, including supplemental safety planning and/or safety demonstration activities, and implementation grants.

- **Better Utilizing Investments to Leverage Development (BUILD):** The BUILD Program funds capital investments in surface transportation that will have significant local or regional impact. The eligibility requirements of BUILD allow project sponsors to pursue multi-modal and multi-jurisdictional projects that are more difficult to fund through other grant programs. Eligible projects for BUILD Grants include, but are not limited to, the following:
 - Highway, bridge, or other road projects.
 - Projects to replace or rehabilitate a culvert or prevent stormwater runoff for the purpose of improving habitat for aquatic species while advancing the goals of the BUILD program.
- **Strengthening Mobility and Revolutionizing Transportation (SMART):** The SMART program was established to provide grants to eligible public sector agencies to conduct demonstration projects focused on advanced smart community technologies and systems in order to improve transportation efficiency and safety. Eligible activities include the installation of intelligent transportation systems (ITS), smart grid projects, and projects related to connected vehicles.

APPENDIX A: COMMUNITY SNAPSHOT

HORACE DOWNTOWN NEIGHBORHOOD PLAN











Final Draft:
04/23/2025



WHAT'S INCLUDED

The Community Snapshot serves as the foundation for discussions about the future of Horace. This section summarizes the existing and future demographic conditions that will influence the community's future transportation system.

Topics covered in this section

-  Background and History
-  Demographic Information
-  Existing Transportation System
-  Existing and Future Land Use
-  Recreation and Geography
-  Infrastructure Facilities and Services
-  Growth Trends and Forecasts
-  Cultural Resources

QUICK FACTS

- **Horace Population in 2023:** 6,655
- **Area:** 11.6 square miles.
- **Density:** 573.7 people per square mile.
- **Median household income:** \$146,184.
- **Number of households:** 1,951.
- **Average household size:** 2.8 people



Sources: North Dakota Data Center estimates, ACS 2018-2022 5-year tables

BACKGROUND AND HISTORY

Horace, North Dakota, is a vibrant and rapidly growing community situated just 16 miles southwest of downtown Fargo. With an estimated 2023 population of 5,643 residents, Horace blends a rural heritage with a commitment to preserving its small-town charm. Facing unprecedented growth pressure, the city aims to maintain its unique character while accommodating future development.

Established in 1871, Horace's roots trace back to a diverse group of settlers, including professionals such as doctors, teachers, and merchants. Named after Horace Greeley,

the influential publisher of the New York Tribune, the town reflects Greeley's vision of the American West as a land of opportunity. Greeley's famous exhortation to "Go West, young man" resonates with the spirit of the community, encouraging generations to seek prosperity and a sense of belonging in the community.

Figure 1. Horace Horizon Looking Northeast



Figure 2. Horace Past



Historically, Horace has evolved from a typical small North Dakota town – divided along ethnic lines in the 1930s with French Canadians on one side of Main Street and immigrants from Norway, Ireland, Sweden, and Germany on the other – to a vibrant community within the FM area. The city's development has been marked by significant milestones, including the establishment of essential services like the post office in 1875 and the construction of a Lutheran church in 1897, which highlight the community's longstanding commitment to social and cultural foundations.¹

As Horace continues to grow, residents and local officials recognize the importance of preserving the town's historical significance while planning for a sustainable future.

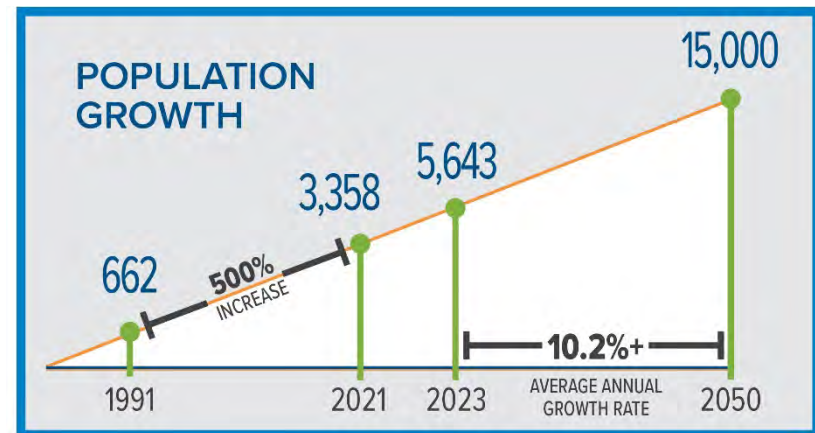
GROWTH

As steady growth has continued in the FM area, Horace has seen enormous development pressure. Growth has been steady, sparked by completion of the Sheyenne Flood Diversion, which provided the city permanent flood protection in 1992. Additionally, Horace is an appealing place for families and individuals looking for a quiet, small-town atmosphere with convenient access to urban amenities associated with the FM area.

Since 1990, the population of Horace has increased by over 500 percent. Over the past few years, Horace has been the fastest growing city in North Dakota, by percent

change. Between 2022 and 2023, the community saw a 32 percent population increase. As shown in **Figure 3** below, an average annual growth rate of approximately 10.2 percent is anticipated through the year 2050² however, the rate may fluctuate based on unforeseen circumstances such as market conditions and policy. If this bears out, Horace may become a community of over 15,000 residents within 25 years.

Figure 3. Horace Population Growth (Metro COG)



Recent Population Projections

At the beginning of 2025, the City of Horace conducted a 10-year *Population Outlook*. The recent study compared Metro COG demographic forecasts with updated assumptions based upon building permit data and available developable land. Two scenarios were

¹ City of Horace (n.d). Welcome to Horace. <https://www.cityofhorace.com/about.php>

² Metro COG. (2022). [2050 Baseline Demographic Forecast](#).

developed: (1) baseline growth and (2) school growth. In conclusion, the 10-year Population Outlook indicates that assumptions regarding Horace's growth are underestimating what is going on, and that Horace's 2033 population is estimated to be 14,067 people, just under

Metro COG's estimated 2050 population of approximately 15,000. See **Table 1** to see the comparison.

Table 1. Population Projections (City of Horace)

Population Projections					
	Baseline Growth (AE2S)	School Growth (AE2S)	Metro COG Most Likely	Metro COG High Growth	2022 Report Baseline Growth (AE2S)
2023	6655	6655	4718	4794	4988
2024	7344	7251	5436	5552	5862
2025	8132	8061	6263	6431	6657
2026	8962	8895	6628	6834	7482
2027	9731	9751	7015	7263	8313
2028	10526	10632	7424	7719	9130
2029	11325	11538	7858	8203	9954
2030	12112	12469	8316	8718	10778
2031	12668	13139	8524	8969	11600
2032	13382	13828	8737	9227	ND
2033	14067	14537	8956	9493	ND

DEMOGRAPHIC INFORMATION

Demographic information is **from the U.S. Census Bureau's American Community Survey (ACS) data**. The ACS socioeconomic data is collected annually. However, the data provided in this section reflects an estimate based on the ACS's 2018-2022 five-year data tables, which do not reflect the 2023 estimated population as gathered from North Dakota Data Center's special survey (pop. 5,643). Five-year ACS estimates are typically the most reliable socioeconomic data source; including the largest sample size and smallest margin of error however, the ACS tables do not yet reflect the most recent growth. Aside from population, other ACS demographics, especially proportionality of population or population makeup in Horace can shed light on demographic trends. Long-term, demographics in Horace will likely change, as is typical with rapid population growth or population change.

community is focused on fostering an inclusive environment that caters to all residents, promoting engagement and collaboration across different age groups and backgrounds.

TOTAL POPULATION

Table 2. Total Population

TOTAL POPULATION	3,464	100%
MALE	1,799	51.9%
FEMALE	1,665	48.1%

Census ACS data estimates Horace's population as 3,464 in 2022, with demographics reflecting a slightly higher proportion of males (51.9%) compared to females (48.1%).

Understanding the community's demographics is essential for local planning, as demographics influence everything from educational needs to community services. The



AGE COMPOSITION

Figure 4. Age

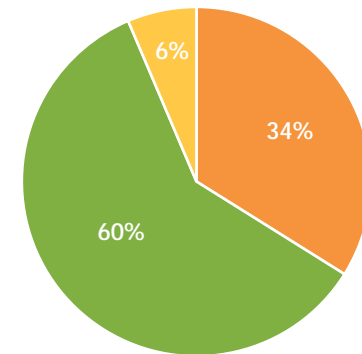
36.3

Median age

almost four years older than the median age
in the Fargo, ND-MN Metro Area: 32.7

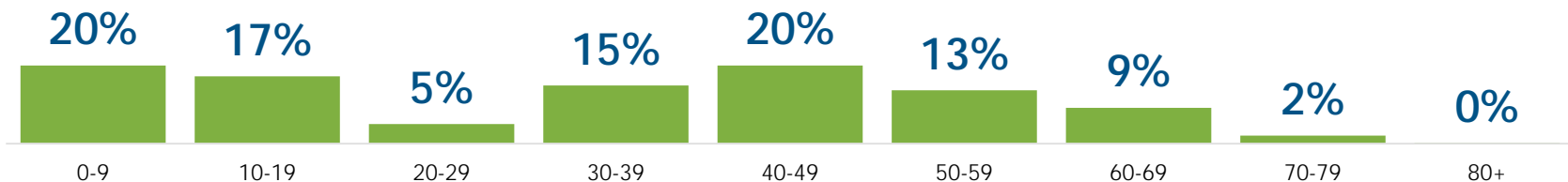
slightly older than the median age in North
Dakota: 35.4

Population by category



Under 18 18 to 64 65 and over

Population by age range



The age structure of Horace reflects a diverse population distribution with a notable concentration of younger individuals. With a total 2022 population of 3,464, the demographic breakdown reveals different composition trends.

About one-third of the population (1,264 individuals) is under 19 years old. The age category of 0 to 9 years is particularly prominent, with 678 individuals. The higher proportion of the youth population is consistent with the large share of families and larger household sizes.

The 20 to 29 age group comprises 168 individuals, while the broader category of 20 to 49 years sums up to 1,352 individuals. This suggests that Horace has a healthy proportion of young adults, contributing to the community's potential for growth. The population aged 60 years and over is relatively small, with 385 individuals.

SCHOOL ENROLLMENT

The higher proportion of youth population is consistent with the large share of families and larger household sizes. With growth, this has also put pressure on the public school system in Horace, part of the West Fargo Public School District (WFPSD). As a result, between 2019 and 2022, WFPSD constructed a new Middle School and High school in Horace. Horace's existing Elementary school was also expanded in 2022. It should also be noted that the enrollment area for schools in Horace also includes areas in south West Fargo.

As shown in the following figures³, the population living within each school's enrollment area align closely with projections of enrolled students. Horace and the WFPSD schools have been further catalysts for growth, making the community an even more attractive place to live.

HORACE ELEMENTARY SCHOOL

The enrollment projections for Horace Elementary School reflect a steady growth trend over the coming years. Starting with the 2023/2024 academic year, the enrollment was 450 students. This number is anticipated to increase to 546 students in the 2024/2025 school year, demonstrating a significant rise in elementary-age population.

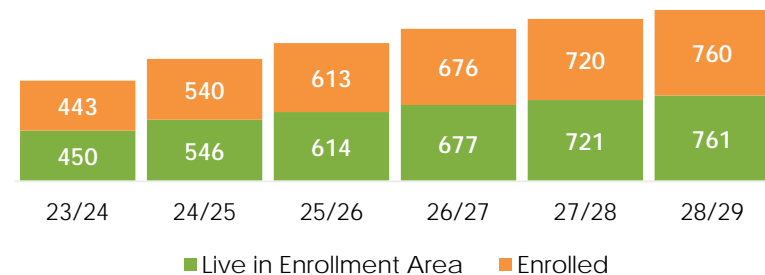
Enrollment is projected to continue growing, reaching 614 students in 2025/26, 677 in 2026/27, and eventually 721 students by 2027/28. The upward trend continues into the

2028/29 school year, with projections indicating an enrollment of 761 students as shown in **Figure 5**.

Current projections show that Horace Elementary School will be over operational and maximum capacity starting in the 2026/27 school year.

Figure 5. Enrollment Projections - Horace Elementary

Horace Elementary School Enrollment Projections



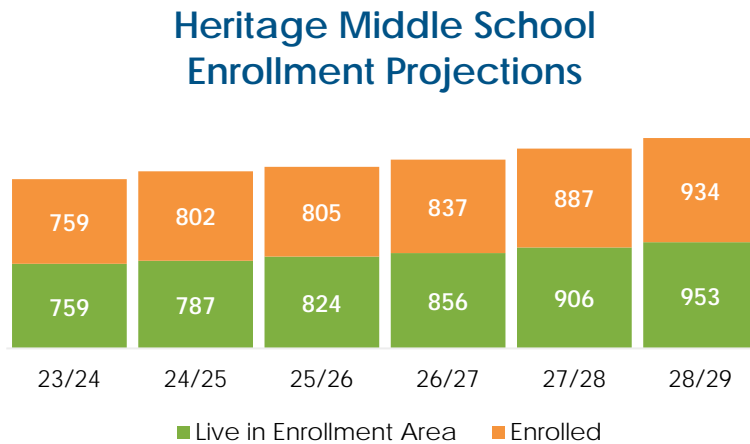
HERITAGE MIDDLE SCHOOL

Heritage Middle School is experiencing a steady increase in enrollment projections over the next several years as well. As of the 2023/24 school year, enrollment was 759 students. This number is expected to rise consistently, with projections of 787 students for the 2024/25 school year and further increases to 824 in 2025/26, 856 in 2026/27, 906 in 2027/28, and reaching 953 students by the 2028/29 school year as shown in **Figure 6**.

³ West Fargo Public School Schools. (2024). *Planning for the Future: West Fargo Public Schools, Enrollment Analysis*.

Current projections show that Heritage Middle School will be over operational and maximum capacity starting in the 2027/28 school year.

Figure 6. Enrollment Projections - Heritage Middle School



HORACE HIGH SCHOOL

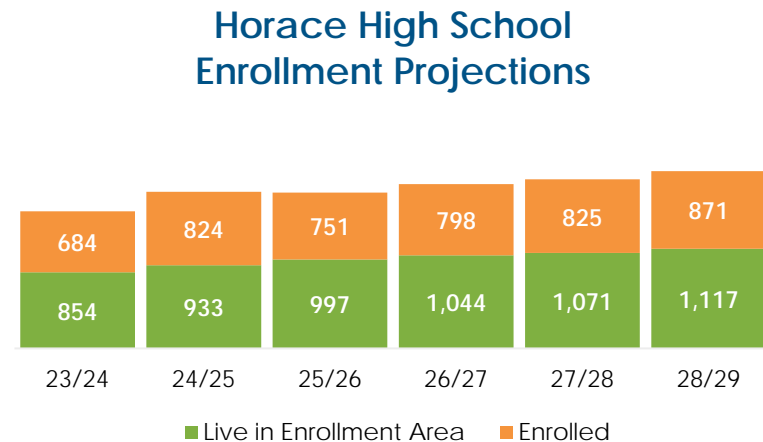
Horace High School enrollment projections show similar results to the other two schools. Starting with an existing enrollment of 854 students in the 2023/24 school year, the numbers are projected to rise significantly. For the 2024/25 school year, enrollment is expected to reach 933 students, followed by 997 in 2025/26.

Future projections indicate attendance will continue to increase, reaching 1,044 students in 2026/27 and climbing to 1,071 in 2027/28. By the 2028/29 school year, the projected enrollment is anticipated to be around 1,117 students as shown in **Figure 7**.

Current projections show that Horace High School will be over operational and maximum capacity starting in the 2027/28 school year.

RACE, ETHNICITY, AND SEX

Figure 7. Enrollment Projections - Horace High School



Approximately 3,347 individuals identify as White (89 percent). The other racial groups include approximately 147 Black or African American residents, 39 American Indian and Alaska Native individuals, and a small number of Asian and Native Hawaiian or Other Pacific Islander residents, 10 each. 99 residents identify as Some Other Race.

In terms of sex, the population is relatively balanced, with 1,799 males (about 52 percent) and 1,665 females (about

48 percent). The ratio stands at 108 males for every 100 females.

Figure 9. Race & Ethnicity

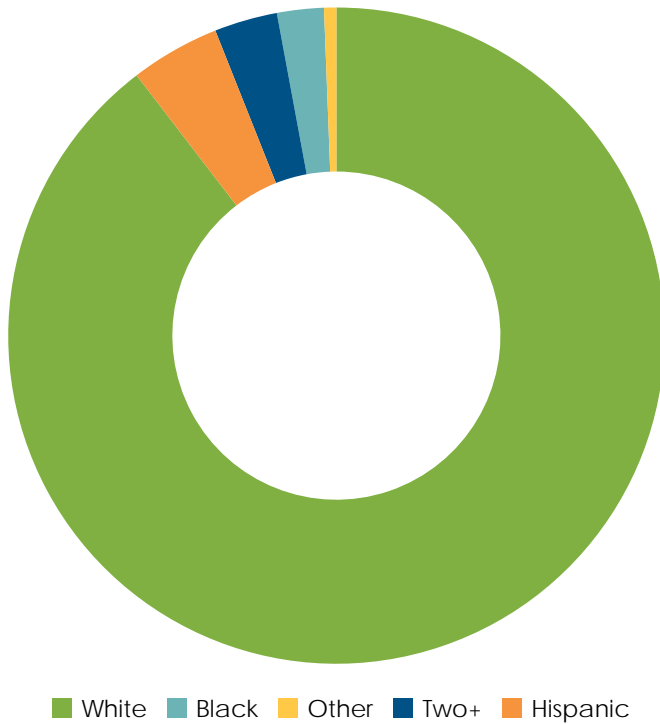
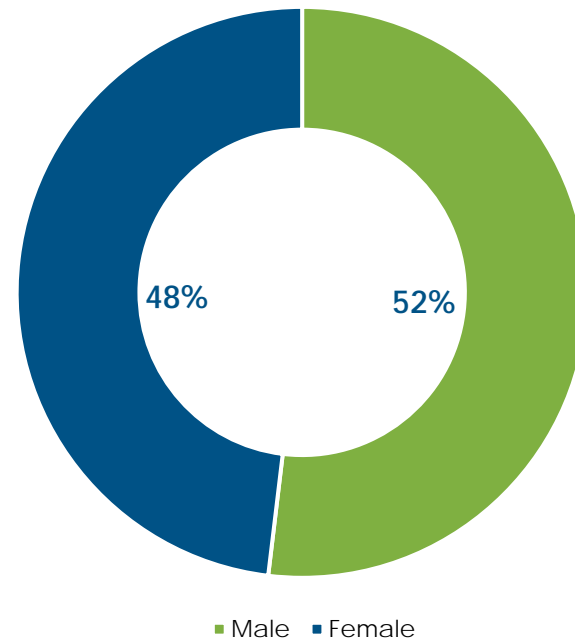


Figure 8. Sex



INCOME AND ECONOMICS

Figure 10. Income & Economics

\$67,931

Per capita income

more than 1.5 times the amount in the Fargo, ND-MN Metro Area: \$41,687

more than 1.5 times the amount in North Dakota: \$40,748

\$146,184

Median household income

double the amount in the Fargo, ND-MN Metro Area: \$73,641

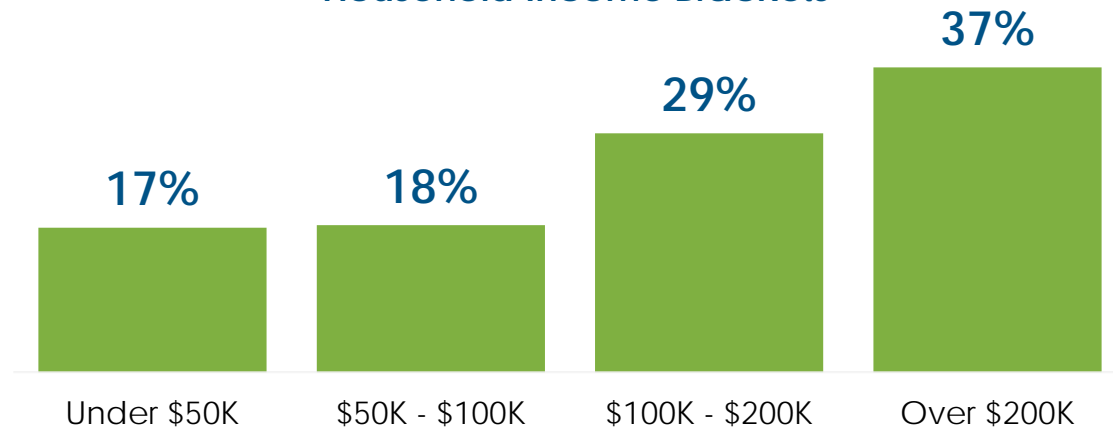
about double the amount in North Dakota: \$73,959

The annual income distribution of Horace indicates a diverse range of income brackets. A substantial 37 percent of households earn \$200,000 or more, with a notable concentration of higher-income earners in the area. Conversely, 17 percent of households earn less than \$50,000.

The mid-income brackets show varying representations with the \$100,000 to \$200,000 earners accounting for 29 percent. \$50,000 to \$100,000 earners comprise 18 percent. These percentages indicate a solid middle to upper-middle class demographic.

The median household income in Horace stands at \$146,184, while the average is \$188,124. The difference indicates that the average is skewed by a relatively small number of very high-income households.

Household Income Brackets



EDUCATION

Figure 11. Educational Attainment

98.4%

High school grad or higher

higher than the rate in the Fargo, ND-MN Metro Area: 95.3%

higher than the rate in North Dakota: 93.5%

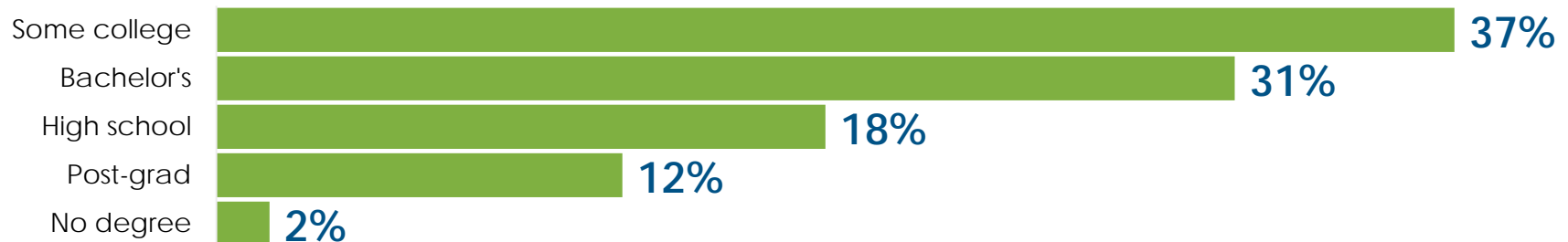
42.8%

Bachelor's degree or higher

higher than the rate in the Fargo, ND-MN Metro Area: 41.1%

about 1.4 times the rate in North Dakota: 31.4%

Population by highest level of education



The educational landscape of Horace is diverse and indicates a high educational attainment of residents. The largest segment of the population at 37 percent, have completed some college without earning a degree. Following closely, 31 percent hold a bachelor's degree.

High school graduates or those with equivalent credentials make up 18 percent. Meanwhile, those holding a post-graduate degree represent 12 percent, showcasing the presence of advanced education in the community. Finally, 2 percent do not hold any degrees.

HOUSEHOLDS & HOUSING UNITS

Figure 12. Occupied housing units

1,240

Number of households

the Fargo, ND-MN Metro Area:
105,017

North Dakota: 320,038

2.8

Persons per household

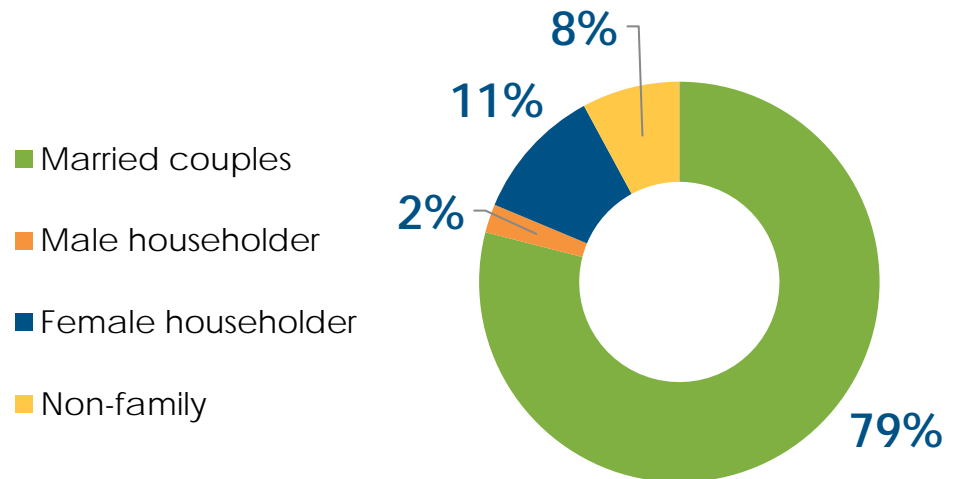
about 20 percent larger than the
figure in the Fargo, ND-MN Metro
Area: 2.3 242,571

about 20 percent larger than the
figure in North Dakota: 2.3

Married-couple households make up the largest portion, accounting for approximately 79 percent of the total. This includes 678 households with children under 18.

Single-person households are also notable, particularly among male (148) and female (150) householders, adding up to 15.7 percent of the population.

Population by household type

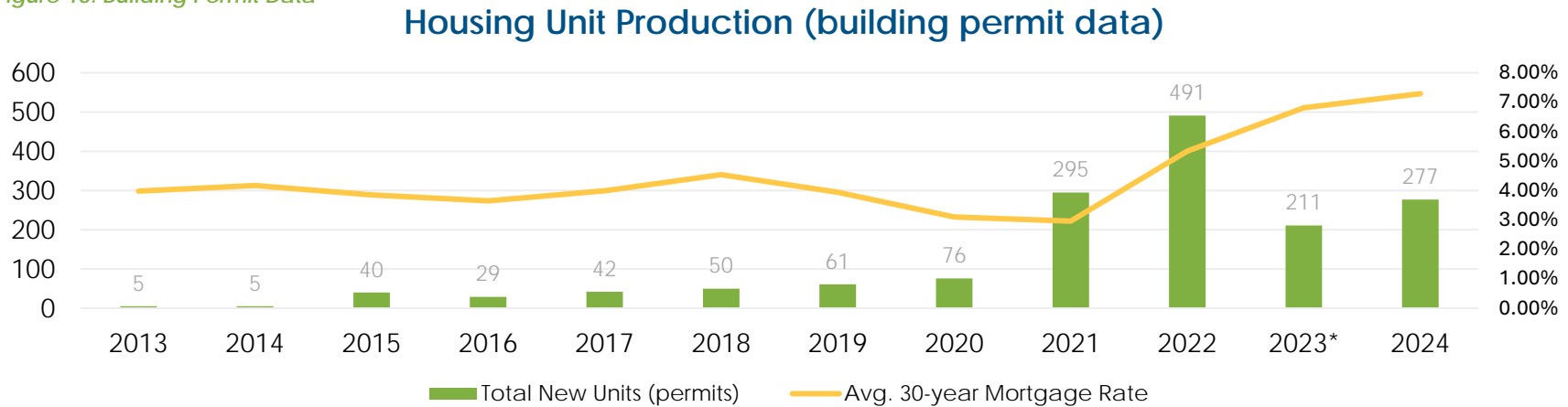


There are 1,008 family households, 55.0 percent of which are with at least one child under 18, and 163 households with one or more individuals aged 65 and over, making up 13.1 percent of the total households.

The median household size is 2.79, which is indicative of the large proportion of families in Horace.

Horace's household demographics are characterized by mostly married couples with children, a notable number of single-person households, and a diverse range of family structures. The presence of both younger and older populations suggests that Horace is a community with varying needs and dynamics. Horace's housing market is competitive, with a strong demand reflected in a low vacancy rate, rapid growth, and higher property values.

Figure 13. Building Permit Data



Housing unit production has kept pace with growth as shown in the chart above. Based on City building permit data from 2013 to 2024, there was a consistent uptick in housing production starting in 2015. Between 2020 and 2021, housing production exploded by nearly 400%. There were 491 housing units permitted for construction in 2022, the highest on record for the city however, it is important to note the figure includes 180- multi-family units.

Market conditions and other factors may affect the number of building permits received. As shown from 2022 to 2023, there is a drop of housing units however, the 211 permits in 2023 is still nearly 300% higher than permits issues in 2020. Even with changes in housing market conditions (e.g. increasing interest rates), housing unit permit trends and vacancy rates reflects a very strong residential housing market in Horace. The median sales price for

single-family homes was under \$400,000 in 2022 and exceeded this number in 2024.⁴

Given the durable market in Horace, it is difficult to quantify how local housing production may or may not be impacted by changing conditions in the future. Horace's production appears to be unphased by fluctuations in interest rates, and 2023 may simply reflect market uncertainty stemming from both federal (interest rates) and local policy. Regardless, over 200 permits in 2023 and 2024 reflect significant growth and population increase (over 10%).

⁴ U.S. News (2024). 2024-2029 Housing Market Predictions. <https://realestate.usnews.com/real-estate/housing-market-index/articles/housing-market-predictions-for-the-next-5-years>

Figure 14. Total Housing Units

1,294

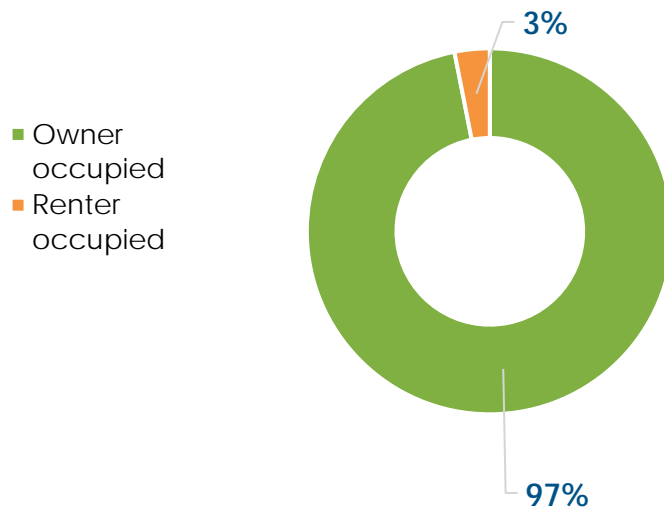
Number of housing units

the Fargo, ND-MN Metro Area:
112,931

North Dakota: 372,376

Based on the Census data, there are a total of 1,294 housing units in Horace. Out of the total units, 1,240 are occupied, indicating a high occupancy rate (95.8 percent) and low vacancy rate (4.2 percent). A high occupancy rate suggests a high demand for housing in the community and a booming housing market in Horace; people want to live in the community.

Ownership of occupied units



TRANSPORTATION SYSTEM

The multimodal transportation system plays a pivotal role connecting the community and providing access to employment, services, and amenities. Quality education, healthcare, housing, employment, and economic development are heavily dependent on a well-planned and maintained transportation system.

Horace is connected to multiple major locations in the FM area to the northeast. These connections are crucial as most Horace residents rely on the job market, medical facilities, entertainment, or other essential goods and services located nearby in the FM area.

EXISTING ROAD SYSTEM

Horace's existing road system is comprised of County Roads (CR) that connect the community to the broader region and local roads that provide access and connectivity within and between neighborhoods. Due to the historic small population of Horace, and like most 'small towns' across North Dakota, roads in older Horace neighborhoods have rural cross sections with stormwater drainage ditches and no sidewalks.

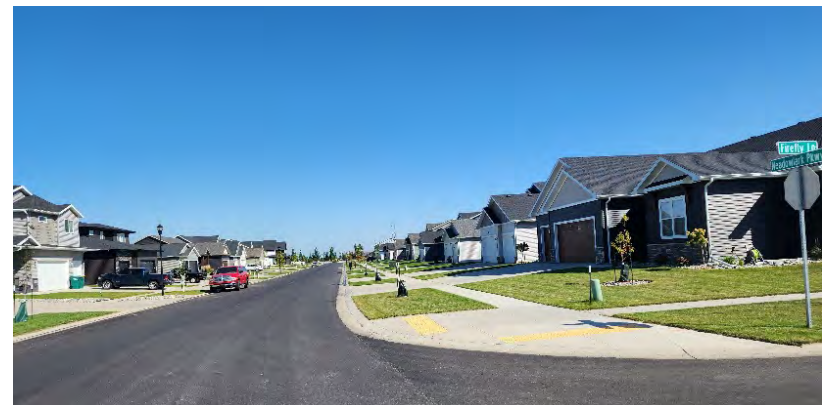
As rapid growth has occurred, newer neighborhood subdivisions and roads have been built with urbanized cross sections including underground stormwater (curb & gutter), sidewalks, and/or shared use paths.

The main road in and out of Horace is CR 17 (Main Street), passing north-south through the city center, eventually providing connection to Interstate 94 and US Highway 10-Business Route in the city of West Fargo to the north. As the

Figure 15. Typical Local Road Cross Section - Older Neighborhood



Typical Local Road Cross Section – Newer Neighborhood



only existing major north-south route, CR 17 (Main Street) provides critical connection to the east-west regional network including 52nd Avenue S., 76th Avenue S. (CR 6), 100th Avenue S. (CR 14), and 124th Avenue S. (CR 16). 52nd Avenue S., 100th Avenue S. (CR 14), and 124th Avenue S. (CR 16) connect to Interstate 29 approximately

two miles east of the city boundary. 76th Avenue S. (CR 6) provides a critical regional connection to 45th Street S. in Fargo and further connectivity to 64th Avenue S. (Fargo) and 52nd Avenue S. (Fargo).

Figure 16 displays categories of the street network in Horace:

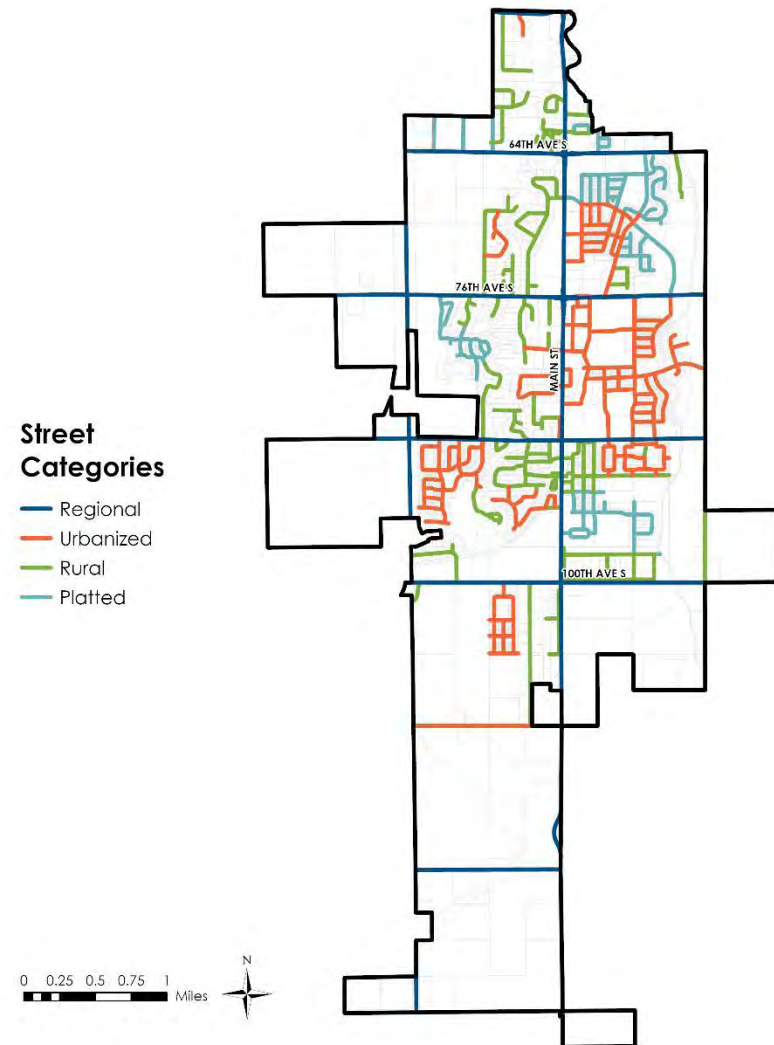
Regional: Typically section line streets. Some urbanized with curb and gutter however most are rural cross section roadways.

Urbanized: Typically local roads with curb and gutter, and sidewalks.

Rural: Typically local roads with drainage ditch, no curb nor gutter, and no sidewalks.

Platted: Typically local road right-of-way (ROW) which has been platted for new development. Horace's current standard is to urbanize street with sidewalks on at least one side.

Figure 16. Street Categories

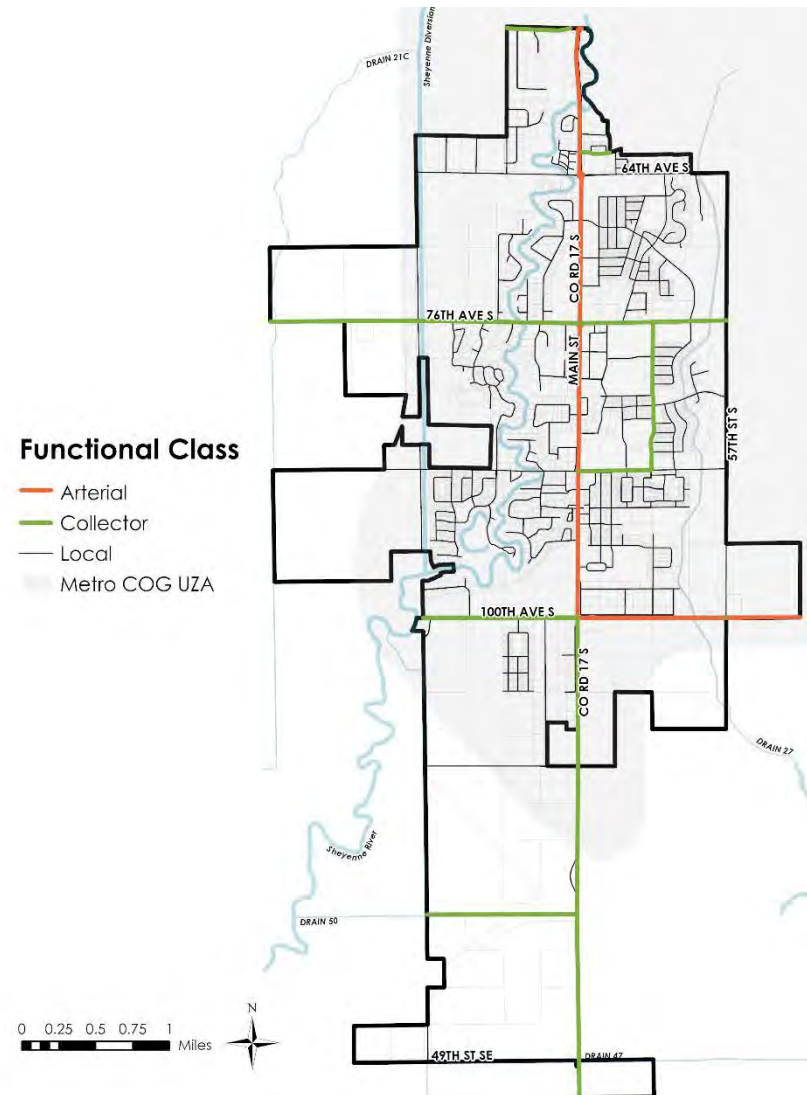


FUNCTIONAL CLASSIFICATION

All roadways have functional classification types such as interstate, arterial (major or minor), collector, and local roads. Functional classification depends on traffic volumes, posted speed, capacity, and design. In addition, such roads are classified by ownership, facility type, and connection to the overall transportation system. Most of the roads in Horace are classified as local however, CR 17 north of 100th Avenue S. and 100th Avenue S. (CR 14) east of CR 17 are classified as a minor arterials, and there are eight (8) collectors: (1) CR 17 south of 100th Avenue S. (CR 14), (2) 63rd Street S. between 76th Avenue S. and 88th Avenue S./Wall Avenue S., (3) 52nd Avenue S. west of CR 17, (4) Deer Creek Parkway S. east of CR 17, (5) 76th Avenue S. (CR 6), (6) 88th Avenue/Wall Avenue S. between CR 17 and 63rd Street S., (7) 100th Avenue S. (CR 14) west of CR 17, and (8) 124th Avenue S. (CR16) west of CR 17 as illustrated in **Figure 17**.

Overall, higher-functioning roadways, such as arterials, support longer trips, provide greater mobility, and have limited access, serving as connectors between communities and regions. In contrast, lower-classified roads, like collectors and local roads, accommodate shorter trips, offer lower mobility, have more access points, and link to higher-classification roadways. Maintaining this balance is crucial for an effective transportation network. The mobility vs. access balance is critical in Horace, as north-south mobility is currently almost solely provided by Main Street (CR 17).

Figure 17. Functional Classification



CRASH DATA

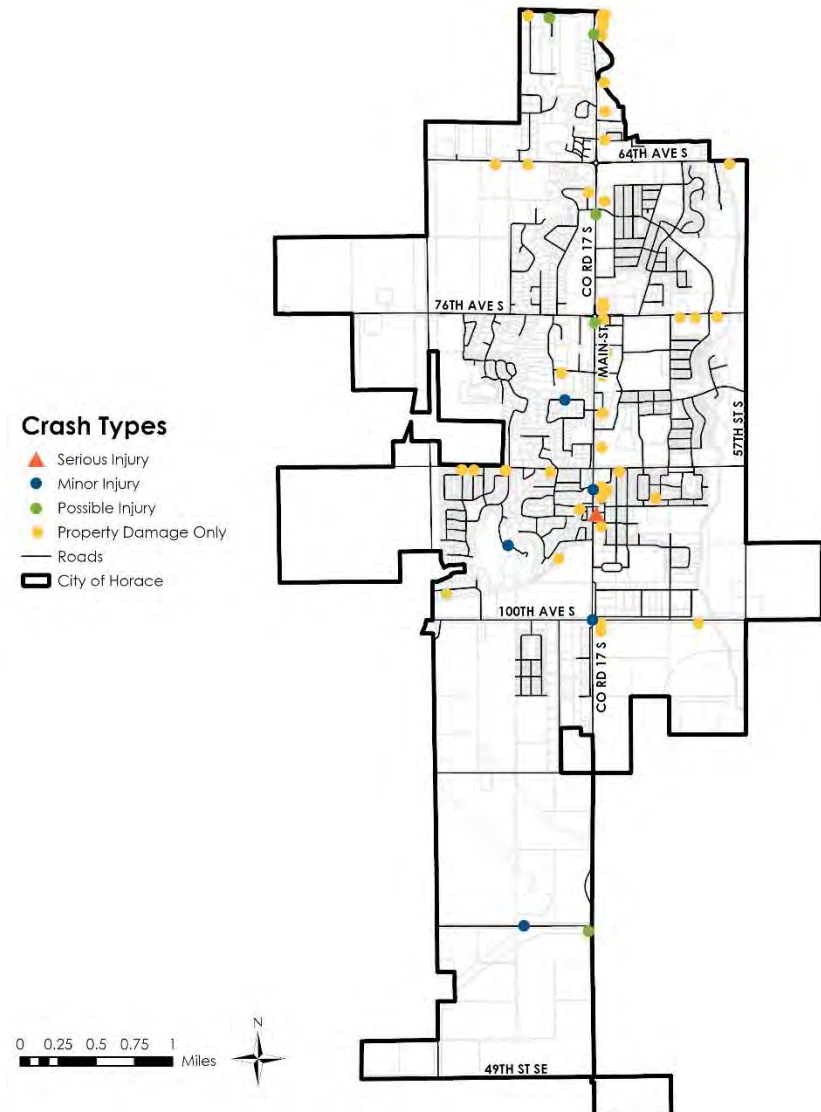
The North Dakota Department of Transportation (NDDOT) maintains crash data for the FM area. Between 2018 and 2022, there were 57 recorded crashes within Horace including one incapacitating or life-changing injury, five non-incapacitating injuries, five possible injuries, and 46 property damage only crashes, as illustrated in **Figure 18**.

The only incapacitating or life-changing injury crash included contributing factors including driving under the influence of alcohol and careless or reckless driving.

The intersection of Center Avenue and Main Street had four crashes over the five-year period, the most of any intersection. All were property damage crashes. No other intersection had more than 2 crashes. Main Street (CR 17), with the highest traffic volumes, had the highest number of crashes. Overall, there are no significant identifiable crash concerns for any intersection or corridor, and crashes are typical with associated vehicular volumes and turning-movement conflicts.

As growth continues in Horace, multimodal traffic volumes including bicycle and pedestrian traffic will increase, which raises the chances for more crashes in the future.

Figure 18. Crashes By Severity



TRAFFIC VOLUMES

In general, most traffic from Horace flows northeast and southwest to and from the FM area. **Figure 19** illustrates that overall Average Daily Traffic Volumes in 2021 were low (between 100 and 7,730 vehicles per day). Traffic volumes reflect 2021 data as maintained by the Fargo-Moorhead Metropolitan Council of Governments (Metro COG). The highest traffic volumes were along Main Street/CR 17 (minor arterial), specifically CR 17 near the 52nd Avenue S. intersection, with traffic volumes approaching 8,000 vehicles per day.

Figure 19. 2021 Traffic Volumes

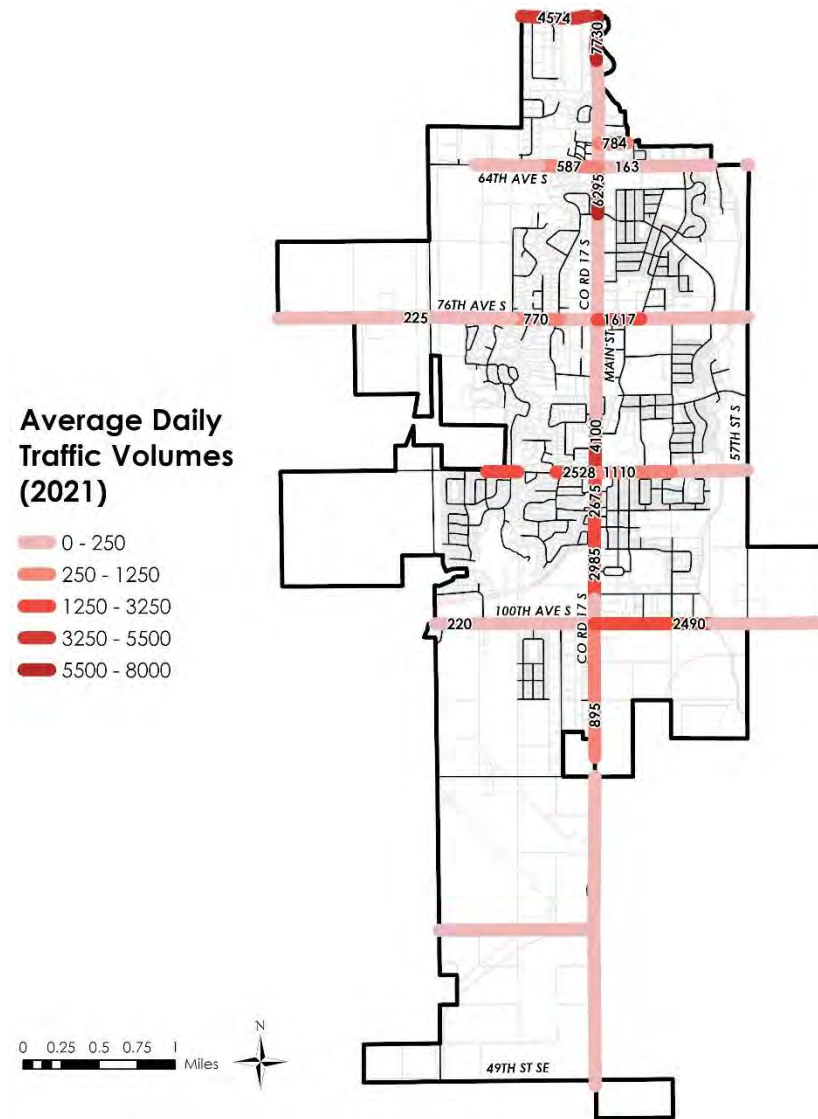
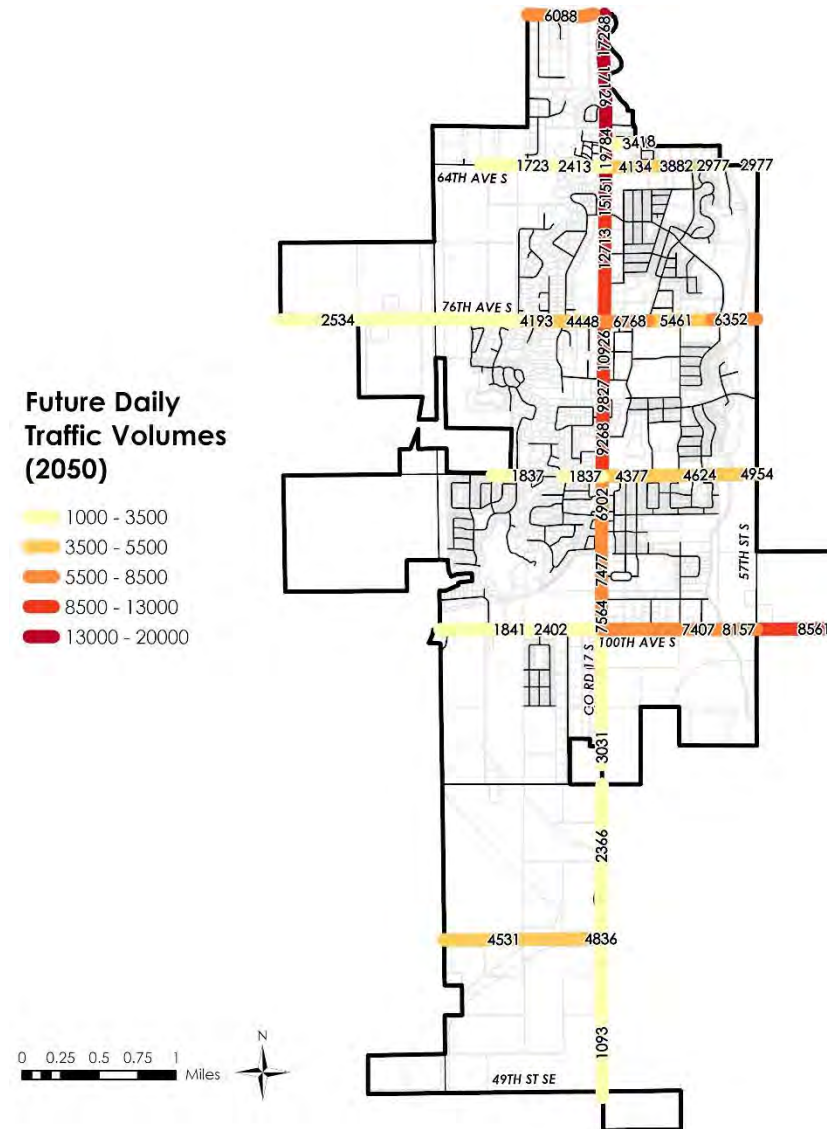


Figure 20 illustrates the Average Daily Traffic projections for 2050. Latest projections come from Metro COG's *Metro 2050 Metropolitan Transportation Plan*. Most road segments are expected to double or triple in volume, which matches Horace's 2050 population projections (see above).

Main Street/CR 17 is projected to carry between 7,000 vehicles per day near the intersection of 100th Avenue S. (CR 14) and 17,228 vehicles per day near the intersection of 52nd Avenue S. Other east-west collectors such as 64th Avenue S., 76th Avenue S., Wall Avenue S./88th Avenue S., and 100th Avenue S. (CR 14) show the greatest percent-increases; in particular, collector roads on the east side of Main Street/CR 17. In general, Horace's northeast and southwest traffic flows to and from the FM area remain the same as existing patterns however, with further development and transportation network expansion in Fargo's southwest growth area, more traffic will be pulled east before heading north.

Future daily traffic volumes are an educated guess, based upon socioeconomic factors and future transportation system investments as known today. A single project may impact future traffic volumes for example, a 64th Avenue S. and/or 76th Avenue S. interchange project at Interstate 29 would result in higher traffic volumes on those roadways. Conversely, such project(s) would result in less traffic volumes on other east-west collector roads.

Figure 20. Traffic Volume Forecast (2050)



BICYCLE AND PEDESTRIAN FACILITIES

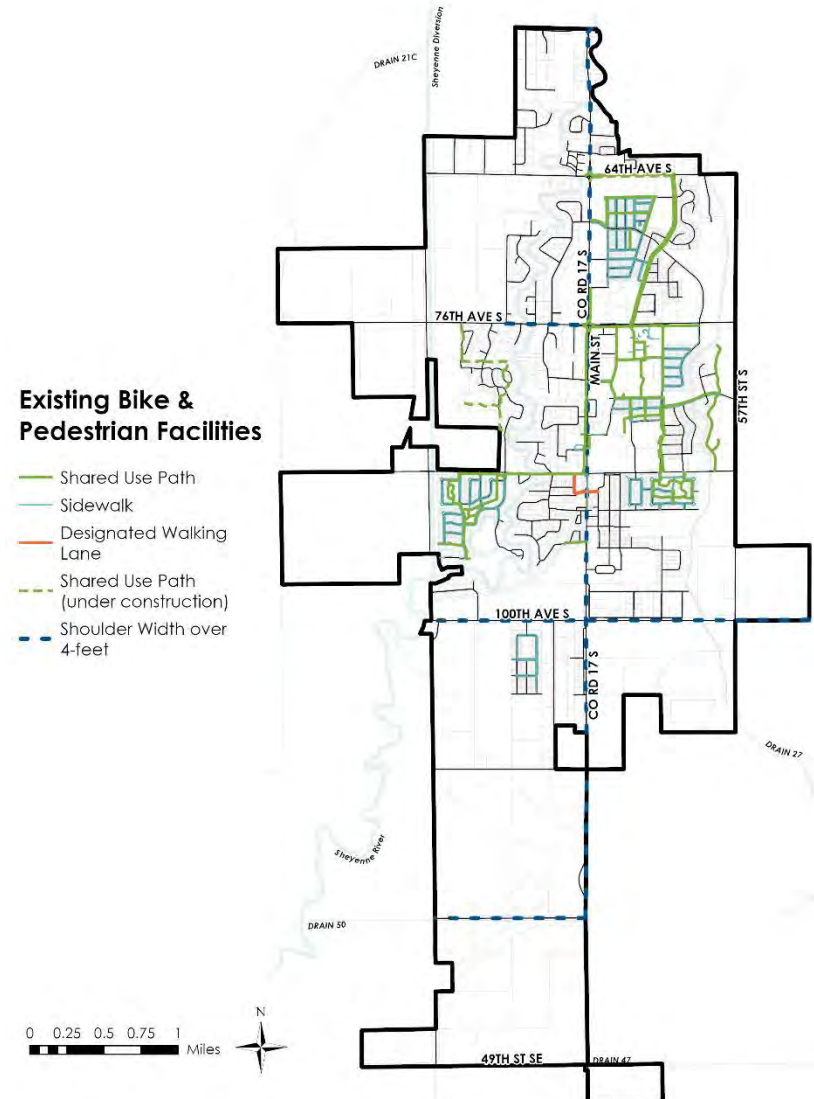
Existing Facilities

Recently, the City of Horace has prioritized improving multimodal infrastructure to accommodate people walking, biking, and rolling. Horace has made progress in building new sidewalks, shared use paths, and other multimodal improvements that help residents and visitors get town around outside of a motor vehicle.

As described above, most new neighborhood subdivisions in Horace include pedestrian or bicycle facilities. There are shared use paths and sidewalks scattered throughout new neighborhoods both east and west of Main Street/CR 17 as illustrated by **Figure 21**. Recently, the City has constructed shared use path facilities on both sides of Main Street/CR 17 north of Wall Avenue S./88th Avenue S. and along the south side of Wall Avenue S. west of Main Street/CR 17. Horace has successfully taken a phased approach to constructing bicycle and pedestrian facilities, piecemealing critical connections over time.

In 2023, the City took a unique approach to bicycle and pedestrian improvements in older neighborhoods without sidewalks or roads with urbanized cross sections: The Center Avenue/Thue Court designated walking lane project, also known as a yield street. The yield street provides demarcation of bicycle and pedestrian space on the roadway with dashed striping on both sides. When no pedestrians or bicyclists are present, vehicles can cross over the dashed lines and split the road, when pedestrians and bicyclists are present, cars must yield to them before crossing over the dashed lines (functioning as a single vehicular lane while preserving the space and safety of

Figure 21. Existing Bike & Pedestrian Facilities



pedestrians and bicyclists on the roadway). The yield street connects across two older neighborhoods east and west of Main Street.

Metro COG identifies existing bicycle facilities or paved road shoulders wider than four feet, along Main Street/CR 17, 100th Avenue S. (CR 14), 124th Avenue S. (CR 16), and west of CR 17 for one-half mile on 76th Avenue S. Typically, bicyclists and/or pedestrians who use these facilities are confident and comfortable riding or walking alongside vehicular traffic. Additionally, many residents in Horace utilize recreational modes of alternative transportation.

OTHER LIGHT MOTORIZED MODES

Weather permitting, many residents of Horace use other motorized modes of transportation to go about their daily lives or to get around town. These vehicles include but are not limited to:

- Snowmobiles
- Golf Carts
- Dirt Bikes
- Utility Task Vehicle (UTV)/All-Terrain Vehicle (ATVs)
- Electric Scooters or Similar
- Mobility Devices

In general, golf carts, electric scooters or similar devices, and mobility devices are the only other forms of transportation that utilize the shared use paths in Horace. Snowmobiles, dirt bikes, and UTVs/ATVs generally use stormwater ditches and roadways. All modes utilize stormwater ditches and roadways when necessary.

Light motorized modes are unique to Horace, as most other local jurisdictions in the FM area have stringent regulations and enforcement regarding operations in city limits or on public rights-of-way (i.e. roads). These types of modes are prominent in Horace and may likely be part of the reason people find the community such an attractive place to live.

Figure 22. Golf Cart in Driveway



Figure 23. Dirt Bikes at CR 17



Figure 24 shows short- and long-term bicycle and pedestrian facility improvements, as developed through various regional and local planning efforts. The City is prioritizing trail and sidewalk connections to improve mobility and safety for bicyclists, pedestrians, and other users, understanding the infrastructure will expand options for travel and recreation. Currently, the City's top priorities include bike & pedestrian infrastructure along major roads (minor arterials & collectors), strategic neighborhood routes, and long-term off-street trails such as the Drain 27 trail on the east side of the community.

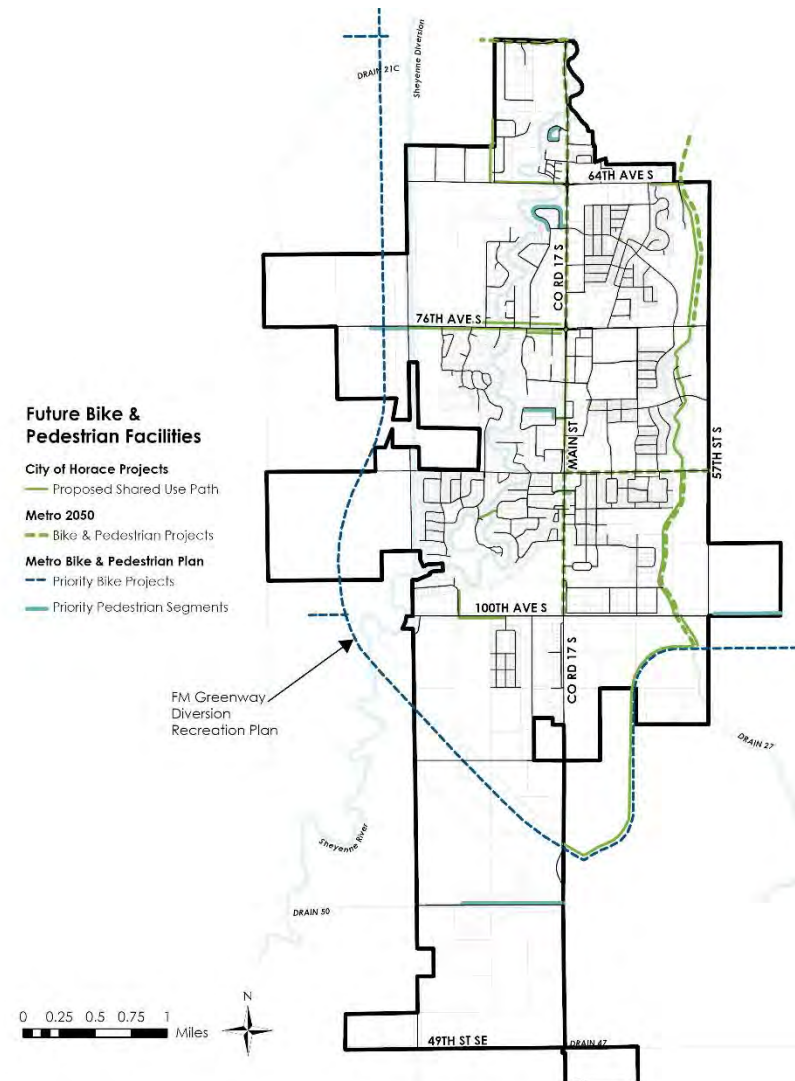
As listed by the City, current priority pedestrian segments include paved shoulders on 100th Avenue South, Riverdale Drive, and 48th Street Southeast.

Metro 2050, Metro COG's Metropolitan Transportation Plan, indicates broader bicycle and pedestrian projects, which include building shared use paths along Drain 27, facilities on either side of CR 17, facilities along Wall Avenue South, and a potential off-street facility connecting the FM Diversion to Horace. The potential, future off-street facility connecting the FM Diversion to Horace has been shown on the former Red River Valley and Western (RRVW) railroad spur. The alignment was identified and shown in the following plans:

- Horace Comprehensive Plan (2019) – Figure 12-3
- Metro COG Bike and Pedestrian Plan (2022) – page 35
- Metro COG Metropolitan Transportation Plan (2025) – page 36 (see project #85)

However, through Plan development and discussions with the Metro Flood Diversion Authority (MFDA). The City's

Figure 24. Future Bike & Pedestrian Facilities



opportunity to purchase the right-of-way (ROW) necessary for the off-street trail alignment on the RRVW railroad has slipped away, as adjacent private property owners have been approached with the option to purchase the former railroad ROW. The off-street path alignment will be reflected differently moving forward in the Plan so as not to interfere with private real estate transactions currently underway.

The City of Horace's Capital Improvement Plan (CIP) bike & pedestrian facilities or projects mostly include improving pedestrian safety and constructing 10-foot-wide shared use paths to increase connectivity across the community.

EXISTING & FUTURE LAND USE

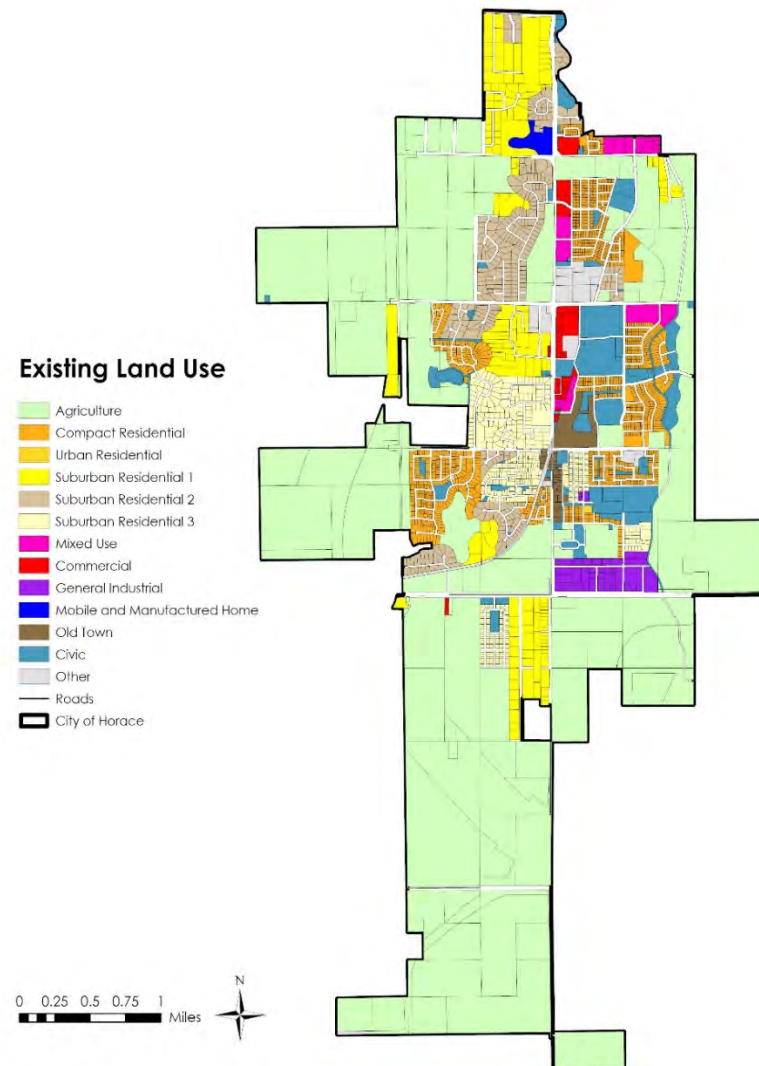
EXISTING LAND USE

Within Horace city limits, all properties are categorized based on existing land use, reflecting how each property is currently utilized. The existing land use pattern is shaped by city zoning.

Residential Uses

Residential land use constitutes approximately 15% of the city and is primarily depicted in shades of yellow and orange on the Existing Land Use Map (**Figure 25**). The residential land use of Horace largely consists of "Suburban Residential" areas, along with other developments of varying densities, including mobile home parks.

Figure 25. Existing Land Use



Commercial and Industrial Uses

Commercial and industrial land uses together comprise a smaller portion of Horace. The commercial land, marked in red, is primarily concentrated along CR 17 in the “Old Town” area and between 76th Avenue S. and Wall Avenue/88th Avenue S. The “Old Town” area extends from the intersection of Main Street/CR 17 and Wall Avenue/88th Avenue S., covering about a half mile north and south, and about one eighth mile east and west of Main Street/CR 17. Established industrial uses, marked in purple, are located along the north side of 100th Avenue and east of CR 17.

Agricultural Area

The most significant existing land use category in the city is agricultural land, which accounts for roughly 65% of the area. This agricultural area encircles the developed regions. This area is primed for development with the completion of the Red River Diversion.

Additional Uses

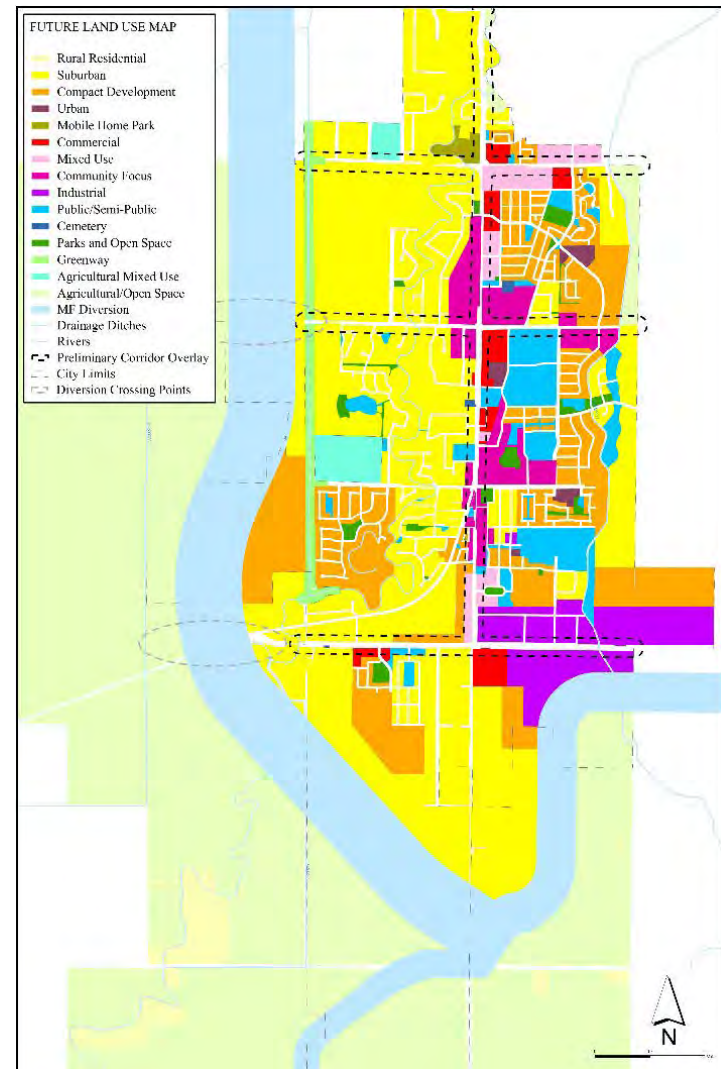
Additional uses are mostly located east of Main Street/CR 17, diversifying the land use as Horace continues to see development.

FUTURE LAND USE

The Future Land Use Map (**Figure 26**) for Horace outlines various land use categories, each reflecting the community's vision for development. The same map is utilized in the 2045 Horace Comprehensive Plan, adopted in 2020. The comprehensive plan and future land use map guide growth and provide the foundation for the city's official zoning map. In the comprehensive plan, the main considerations to establish future land use are

environmental conditions, historical growth patterns, and public service availability.

Figure 26. Future Land Use



Since adoption of the 2045 Horace Comprehensive Plan, the city has faced enormous growth and development pressure. City staff and policymakers have identified several areas of the comprehensive plan and future land use map that may not align with current community values. Strategic updates to the future land use map have occurred over time however, the City is considering a more thorough update to the comprehensive plan in the near term.

Land Use Categories:

- **Agriculture/Open Space:** Rural areas focused on agricultural uses, receiving minimal services. Density: 1 unit/acre.
- **Agriculture Mixed Use:** Traditional agricultural and compatible commercial uses, with limited services. Density: Up to 1 unit/acre.
- **Urban Residential:** Features higher-density housing, such as apartments, and small commercial spaces. Density: 5-14 units/acre.
- **Compact Residential:** Includes various housing types and small commercial uses, with a focus on open space and transitions to lower densities. Density: 3-5 units/acre.
- **Low Density Residential:** Predominantly single-family homes, maintaining a low-density character. Density: 1-3 units/acre.
- **Rural Residential:** Low-density, single-family homes in agricultural areas, with minimal public services. Density: Up to 1 unit/acre.
- **Mobile Home Park:** Existing mobile home parks within the city.
- **Mixed Use:** Combines residential and commercial uses in flexible designs, promoting walkability. Density: Up to 14 units/acre.
- **Public/Institutional:** Publicly owned lands for schools, utilities, and governmental facilities, subject to local review. Density: Varies.
- **Commercial:** Retail and office spaces located near major roads and existing commercial areas.
- **Community Focus Areas:** High-density residential and commercial uses with design guidelines for compatibility. Density: Up to 14 units/acre; FAR: 0.5-2.0.
- **Industrial:** Areas for light manufacturing and warehousing, including some commercial uses.
- **Diversion Channel:** Areas related to the F-M Diversion Project, maintaining existing uses with potential greenway concepts.
- **Greenways:** Buffer zones along rivers and drainageways, with specific zoning requirements.

- **Park and Open Space:** Designated for recreational use, with locations determined based on community needs.

DEVELOPMENT NEAR DOWNTOWN HORACE

Development is occurring across the city, including new single-family homes, multi-family housing, commercial businesses, institutional and civic uses, and industrial activities. Near the Horace Downtown Neighborhood Plan focus area, there are two nearby developments: (1) Lakeview Addition and (2) Sparks Addition.

Lakeview

As shown in **Figure 28**, the Lakeview Addition proposes a new commercial and mixed-use town center district. Centered around a large stormwater retention area and the St. Benedict's Catholic Church. Lakeview Drive is proposed to provide a pedestrian-focused commercial thoroughfare. Lakeview Addition also provides single-family residential on the north and southeast sides of the area, with limited high density residential proposed near the northwest corner of the intersection of Wall Avenue S. and Front Street.

Some of the property has been platted and started developing however, it is limited to the single-family neighborhood on the north and the St. Benedict's Catholic Church property.

Figure 27. Nearby Development Areas

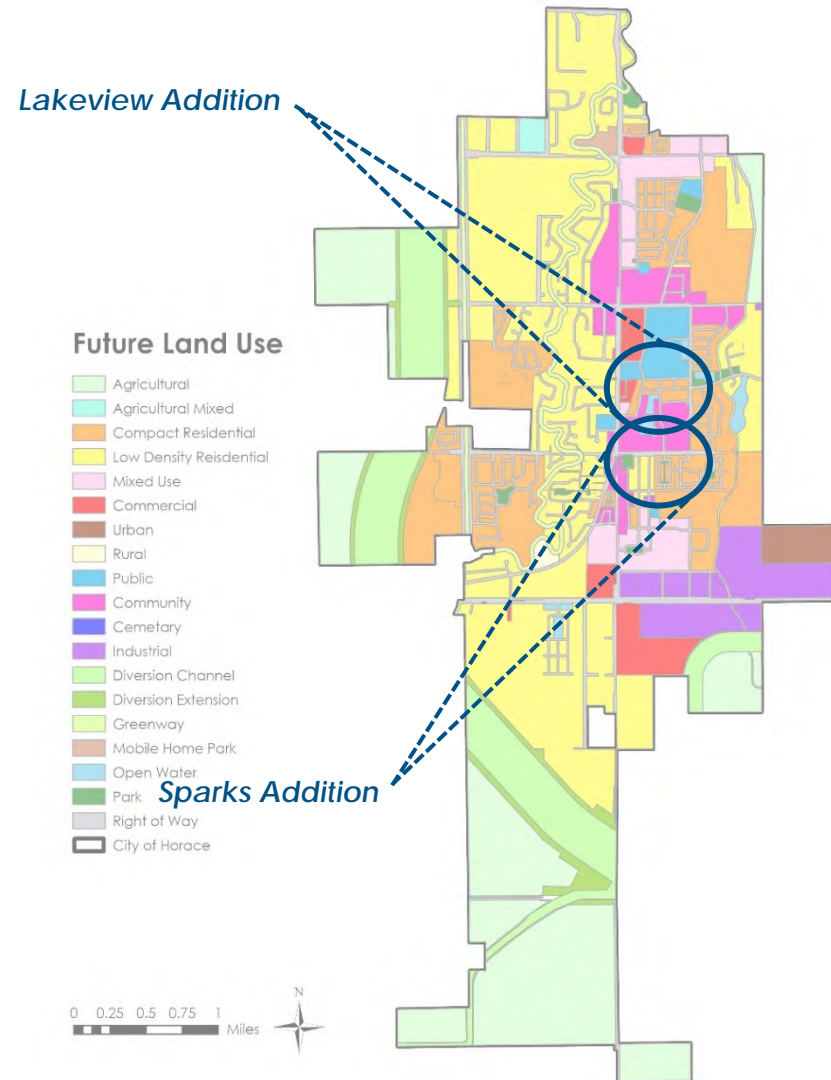


Figure 28. Lakeview Addition - Plan View



Figure 29. Sparks Addition – Aerial Rendering Looking Northeast



Sparks Addition

Sparks addition is proposed south of downtown, just north of the industrial area along 100th Avenue S. The developer proposes commercial, commercial mixed-use, and civic uses along the east side of CR 17 and centered around a large public open space (see **Figure 29**). Single-family residential and limited high density residential uses are also proposed.

Partial platting of the subdivision has occurred however, no infrastructure or development has been constructed.

RECREATION AND GEOGRAPHY

PARKS

The Horace Park District, established in 1972, plays a vital role in managing the city's parks and summer recreation programs. The existing park system includes three (3) large Community Parks and five (5) smaller Neighborhood Parks, providing diverse recreational opportunities for residents. See **Figure 30**.

Community Parks:

- **Freed Community Park (4.28 Acres):** Features a baseball diamond, playground equipment, skateboarding area, tennis court, volleyball, basketball hoops, horseshoe pit, and winter skating.
- **Horace Lions / Westwood Community Park (1.98 Acres):** Offers playground equipment and baseball diamond.
- **Meadowlark Community Park (5.89 Acres):** Includes a playground, lighted hockey rink, warming house, picnic shelters, junior soccer field, youth baseball/softball field, tennis court, basketball court, connected multi-use paths, and a gazebo.

Neighborhood Parks:

- **Adelman Neighborhood Park (0.37 Acres):** A small area with playground equipment and basketball hoop.

- **Arrowwood Neighborhood Park (0.30 Acres):** Features playground equipment and picnic table.
- **Independence Neighborhood Park (3.19 Acres):** Contains a horseshoe pit, playground equipment, and shelter.
- **Maple Grove Neighborhood Park (1.57 Acres):** Offers a sitting bench with a pond view.
- **Southdale Farms Neighborhood Park (0.39 Acres):** A small area with playground equipment.
- **Willow Court Neighborhood Park (0.13 Acres):** A small area with playground equipment.

NATURAL RESOURCES

The Sheyenne River is a vital resource that flows through Horace, adding to the city's unique character and ecological diversity however, like most waterways in the Red River Valley, the Sheyenne River is prone to flooding. Spring flood events are common in the Red River Valley cause by snowmelt. Because the valley is so flat, as rivers like the Sheyenne River rise with snowmelt, the water breaks the banks and flows significant distances. Floodwaters from overland flooding can stretch for miles, given the extremely flat topography.

Historically, most of Horace was in the 100-year flood plain but in 1992, the United States Army Corps of Engineers completed a large flood protection project, the Sheyenne River Diversion. The Sheyenne River Diversion provides permanent flood protection for the City of Horace and City of West Fargo to the north. The Sheyenne

River control structure operates by diverting rising water levels to the Sheyenne Diversion channel which flows north around Horace and West Fargo on the western municipal boundaries.

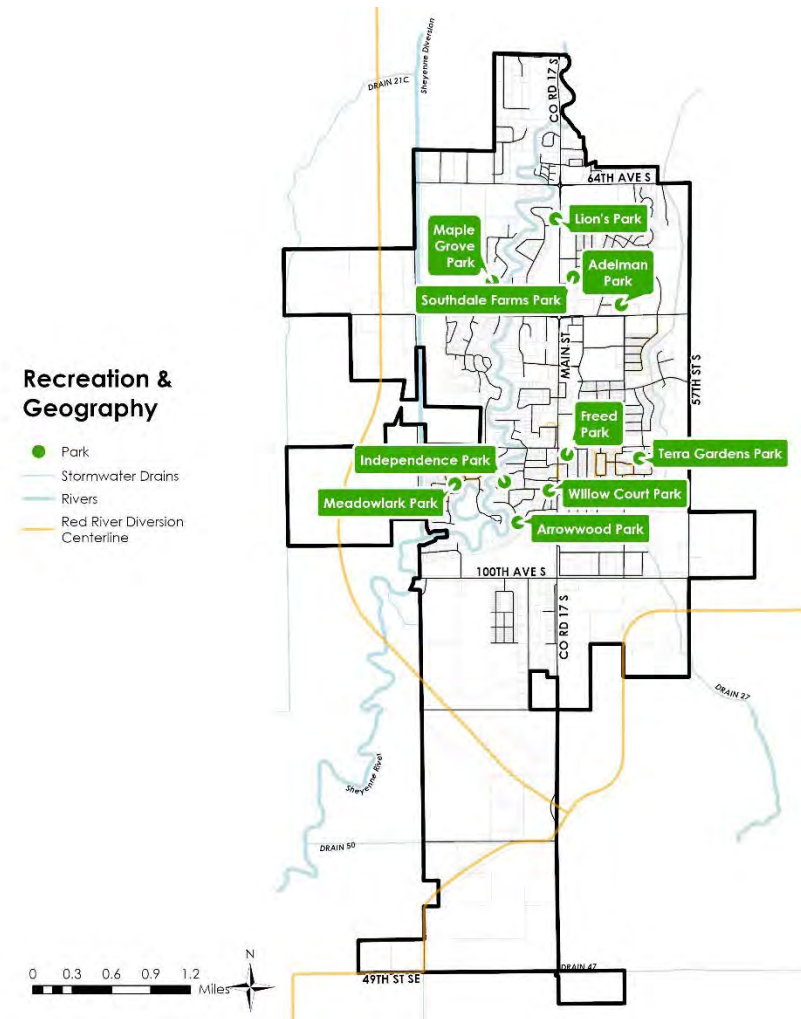
The Sheyenne River is considered a riparian greenway, and plays a key role in filtering pollutants, mitigating floodwaters, providing habitat for wildlife, and providing opportunities for recreation. Within the city, riparian areas also include 'lost river' formations including old oxbows where the Sheyenne River once flowed, but no longer do, due to change in course. The riparian greenway is evident in Horace by the dense vegetation and mature tree canopy. Horace's riparian areas also have higher land values than the surrounding plains and are seen as an amenity for private development. Most private development along the Sheyenne River in Horace occurred after completion of the Sheyenne River Diversion in 1992 however, there are a few naturally elevated areas along the river that were developed prior.

Public access to the Sheyenne River is limited due to private ownership however, flood control and stormwater infrastructure provide opportunities for public open space. At the local level, stormwater infrastructure such as retention ponds provide walking paths and open space. Regional infrastructure, such as the Sheyenne River Diversion and Drain 27, provide opportunities for regional trail connections to West Fargo, Fargo, and beyond. Furthermore, Horace is in proximity to the Red River Diversion channel, currently being constructed to protect the entire FM region from flooding.

As outlined in the Red River Diversion Recreation Plan, Horace could connect to 30+ miles of continuous trail,

super-structures such as the channel inlet structure or Sheyenne River aqueduct, interpretive sites, and/or other natural areas that may be associated with the multi-billion-dollar regional flood control project. See **Figure 30**.

Figure 30. Parks and Natural Resources



INFRASTRUCTURE FACILITIES & SERVICES

WATER SUPPLY

In the past, City of Horace's water service relied on two (2) wells feeding a treatment plant built in 2003, with storage in an underground reservoir (pre-1966) and a 75,000-gallon water tower (1981). Currently, less than 50% of homes receive city water, about 4% use private wells, and the remainder is served by Cass County Rural Water.

At the end of 2023, the City completed a project to hook up to Cass Rural Water to receive water from the City of Fargo to service Horace's water service area.

SANITARY SEWER

The sanitary sewer system consists of a wastewater lagoon with an expansion constructed in 1983, with wastewater collected via gravity mains to six (6) lift stations. Less than 10% of properties use individual or regional septic systems. In 2017, the city partnered with Fargo, allowing wastewater collection via gravity mains to an additional 10 lift stations and miles of force mains to be treated at City of Fargo's facilities, easing the lagoon's capacity issues and accommodating ongoing growth.

STORMWATER MANAGEMENT

Stormwater in Horace is managed through a combination of 15 lift stations, ditches, culverts, and ponds, designed to manage stormwater runoff, and mitigate flash flood events. The city continues to develop and enhance its stormwater infrastructure to support increasing

Figure 31. Utilities Near Downtown

Storm Utilities

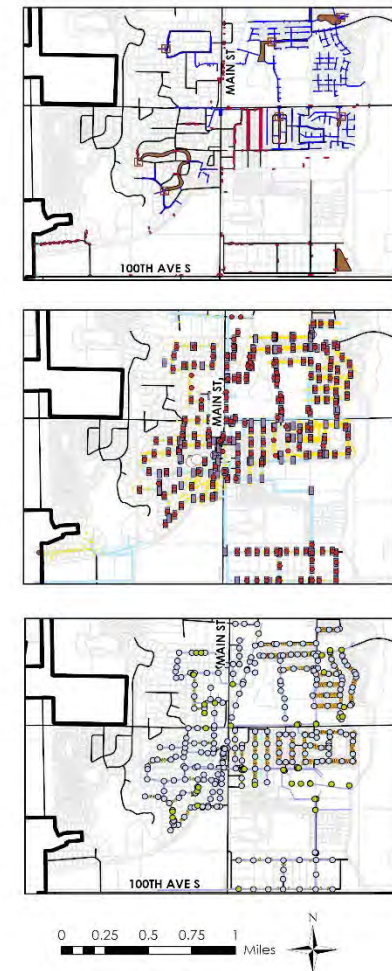
- Storm Lift Station
- Storm Culvert
- Stormwater Ponds

Water Utilities

- Water Well
- Water Tower
- Water Meter Pit
- Water Hydrant
- Water Gate Valve
- Water Main
- Water Services

Sanitary Utilities

- Sanitary Points
- Sanitary Manhole
- Sanitary Service
- Sanitary Main



urbanization and maintain water quality in local waterways.

COMMUNITY GROWTH TRENDS AND FORECASTS

Based on socioeconomic analysis performed in the City's *10-Year Population Outlook (2025)*, Horace is projected to grow to more than 14,000 residents by 2033. The projected population exceeds projections made in the City's comprehensive plan (2021) and Metro COG's latest socioeconomic forecast for the city (2022). As growth continues, development should be aligned with the community's needs and values. While growth creates opportunities to diversify the land use mix and establish new development patterns, the scale and intensity of development need to be consistent with the community's identity.

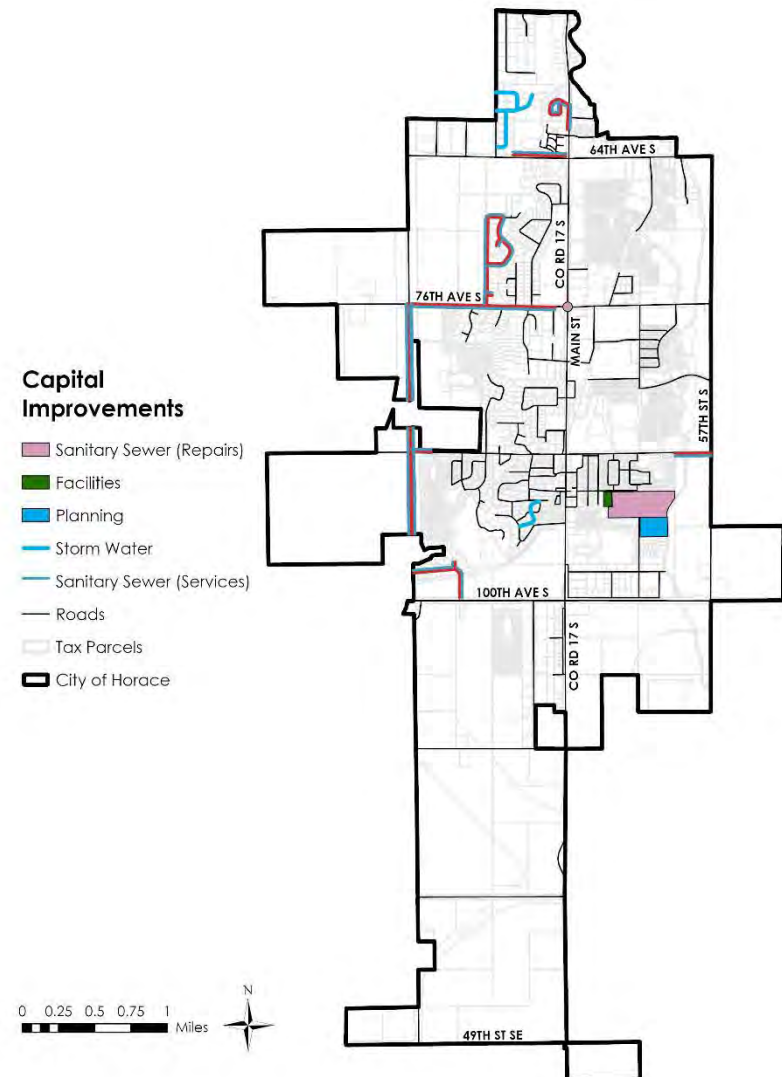
GROWTH OPPORTUNITIES & CHALLENGES

Rapid growth presents both challenges and opportunities for Horace. On one hand, growth can expand the tax base, increase land values, and enhance access to housing, employment, services, and amenities that people may otherwise drive to Fargo or West Fargo for. On the other hand, growth increases pressure on schools, services, and infrastructure, and on the agencies and departments that manage these services. If growth areas are poorly planned, development may be disorganized, detract from community character, and lead to resistance or resentment within the community.

To embrace opportunities, Horace must focus on strategic land use planning that fosters economic development while preserving existing neighborhoods and natural resources. In addition, the City should balance its needs

for infrastructure expansion with the need to maintain and improve existing facilities. **Figure 32** illustrates the city's 2023-2027 programmed capital improvements.

Figure 32. Capital Improvements



ADDRESSING DEMOGRAPHIC CHANGES

As demographics of Horace may shift with growth, the city should adapt strategies to meet the needs of a changing population. This involves not only providing affordable housing options but also fostering an inclusive community where all residents feel valued. The city should focus on initiatives that provide accessibility and promote health and wellness, ensuring that growth benefits everyone.

Figure 33 and Figure 34 illustrate future employment and household density as projected in for 2050 as developed from the *2050 Baseline Demographic Forecast* and allocated as part of the 2050 Metropolitan Transportation Plan update process.

Figure 33. Future Households

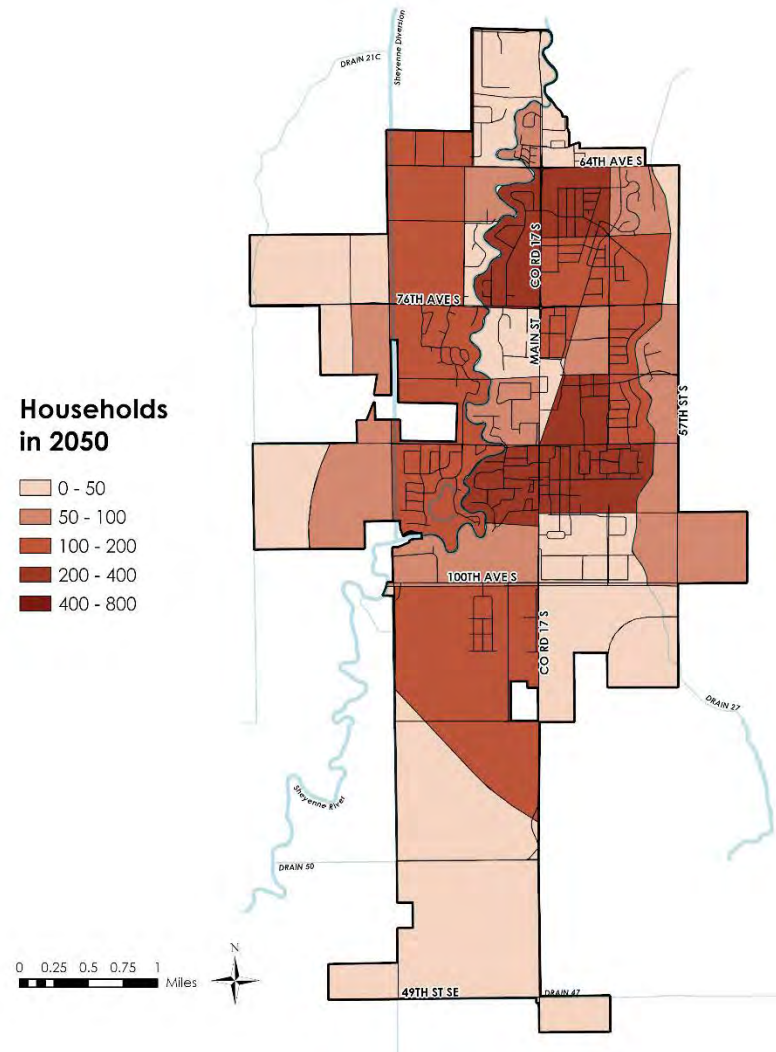
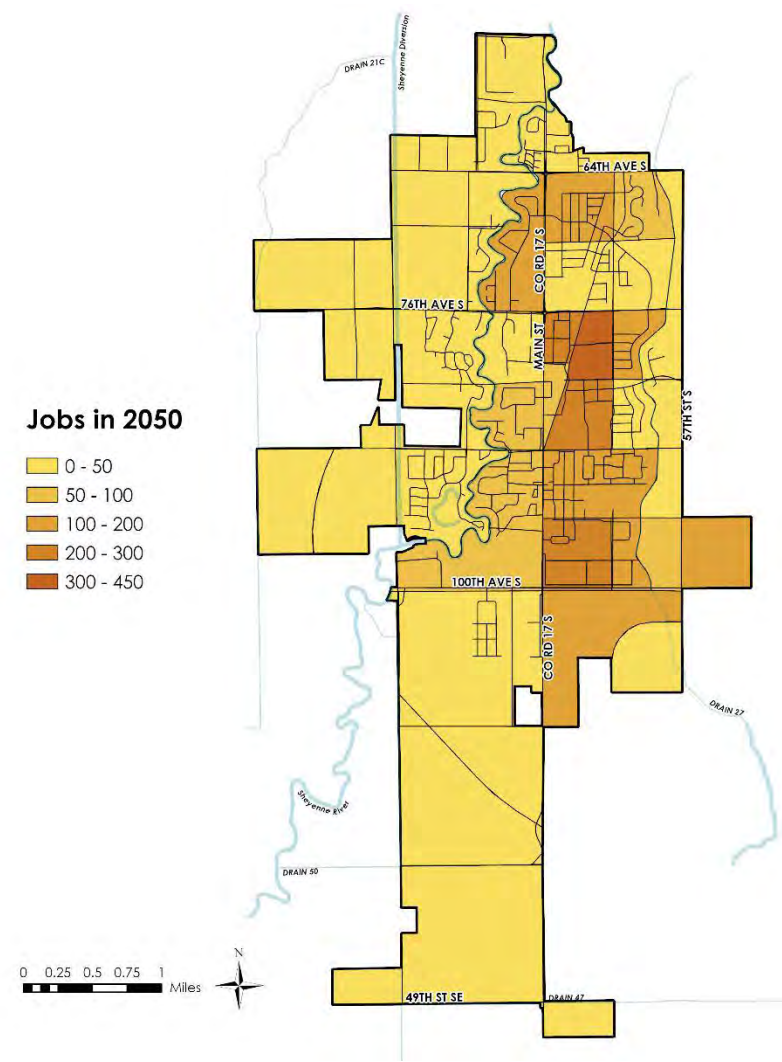


Figure 34. Future Households



APPENDIX B: PUBLIC ENGAGEMENT SUMMARY

HORACE DOWNTOWN NEIGHBORHOOD PLAN



SRC Meeting Summaries.....	3
Round 1 Engagement Summary.....	42
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Final Advertising.....	X

Horace Downtown Neighborhood Plan

Study Review Committee Kickoff Meeting

Notes

August 1, 2024, 10:00-11:30 a.m.

Horace City Hall Conference Room – 215 Park Dr

Attendees

- Joel Luing, City of Horace
- Ron Erickson, Horace Planning & Zoning
- Jace Hellman, City of Horace
- Chelsea Levorsen, Metro COG
- Michael Maddox, Metro COG
- Dave Sweeney, SRF
- Scott Harmstead, SRF
- Luke Champa, SRF
- Rachel Lee (Virtual), Citizen Representative
- Naomi Burkland (Virtual), City Council
- Tanya Olson (Virtual), Tallgrass

1) Introductions – 10 minutes

a. What words come to mind when you think of Downtown Horace?

Jace: Opportunity, history, blank slate

Ron: rural, past, opportunity

Joel: bar, water tower, elevator

Michael: elevator, cultural center

Dave: opportunity

Scott: trees

Luke: small town, sleepy, agricultural

Naomi: gathering space, opportunity, history

Rachel: small, potential, landmarks

Tanya: growth, community, flat

Themes

Elevator/water tower
/landmarks (5)

Opportunity/potential (5)

History/past (3)

Chelsea: elevator, wide street, big erv's

2) Overview of Scope & Deliverable – 15 minutes

a. Consultant Team

b. Project Balance

Michael: scope – how does Downtown service the surrounding neighborhoods? Many facilities in newer parts of Horace that need to be connected. And provide access to older areas.

Focus on connections to facilities near downtown.

3) Study Review Committee Roles & Responsibilities – 5 minutes

Sept SRC – Doodle poll right away as school will be in session (busier schedules)

4) Draft Public Engagement Plan – 25 minutes

Rachel: school teacher at the elementary school – ways to have school district promote? Aug. 20 back to school event. Elementary may be better one to connect with because families are a little more tuned in. Principal is resident of Horace.

Any opportunity to notify through school district?

Michael: engagement, anything going on that has messaging, where this project can be integrated into the messaging? (i.e. CR 17 will need a new name!)

Paul Hankel is the city communications lead (contact information is in the public engagement plan). Horace Happenings content due by the middle of the month.

Michael: need notification communication strategy. Need this ASAP. Must meet COG public engagement strategy.

Need PEP developed ASAP, need everything planned out!!!!

a. Which individuals, groups, or others will be critical in shaping/implementing this plan? (i.e. business community, long-time residents, new residents, etc.)

- Elementary School
- Larger landowners and businesses downtown
 - Somewhat connected landowners and businesses (group setting – OK)

- *Big Erv's*
- *Chelsea Johnson and Brandon Bixby*
- *Hopewell Auto*
- *Firehall*
- *Senior Center*
- *Some bigger developers interested in Downtown*
 - *Lakeview north of downtown*
 - *Sparks south of downtown*
 - *Dabbert*
 - *Dwyer*
 - *Sparks Family*
- *Al? longtime, well-connected resident*
- *Tom Samuelson*
- *Lion's Club*

Logo – 4a, keep water tower, consider removing or making it less significant? Make Downtown Neighborhood different color. Elevators are a heritage aspect. Middle ground between 4A and 4B. Keep sun, brings down water tower scale, outline of elevator. Incorporate greenery (blue and green)

5) Data Needs – 5 minutes

6) Downtown Boundary Exercise – 25 minutes

a. What do you foresee as the biggest challenges and opportunities of this project?

Opportunities: Lakeview area (2), opportunity to take people off CR 17, development and redevelopment, making the most of Lakeview and Sparks Additions – end caps of Downtown. Might be some local champions. Locals feel like they have a say.

Challenges: getting people to participate (2), CR 17 – how to build downtown around a busy road? Fight the negative opinion of city, combat funding opinion (special assessments), not being a burden to existing residents. Conveying costs and benefits, integrating infrastructure into preexisting areas. Horace factions (old, new, river). What is actual downtown area that truly feels like downtown?

7) Next Steps – 5 minutes

Action items:

- *SRC to help develop stakeholder list and key contacts with contact information, as applicable*
- *Luke to send Doodle poll for 2nd SRC scheduling in September*

- *Jace to identify and get detail on school district notification policies/events – August 20th back to school event.*
 - *Coordinate with Rachel and Naomi accordingly.*
- *Luke/Dave to contact Paul Hankel for Horace Bean Days plan and Horace Happenings*
- *Luke/Dave to provide notification Communication Strategy Table in Working Public Engagement Plan (reference Metro COG PPP requirements for more formal events/public comment period)*
- *Team to Update/Refine Logo (4A/4B)*

Next Steps

- *Website content*
- *Public Engagement notification, materials, and preparation and scheduling for phase 2 activities*
- *Stakeholder outreach/scheduling*
- *Data Request - receive data by 8/14/2024 (City of Horace/Metro COG)*

Luke Champa

Subject: Study Review Committee Meeting #2 - Horace Downtown Neighborhood Plan

Location: Horace City Office (215 Park Dr, Horace, ND 58047)

Start: Fri 9/6/2024 1:00 PM

End: Fri 9/6/2024 3:00 PM

Show Time As: Out of Office

Recurrence: (none)

Meeting Status: Meeting organizer

Organizer: Luke Champa

Required Attendees Michael Maddox; Chelsea Levorsen; Jace Hellman; David Sweeney; tanya tallgrasslandscapearchitecture.com; Joel Luing; Naomi Burkland; Ron Erickson; Paige Shockman; mrs.rachellee@yahoo.com; Jerad Daul; Arsen Martyshchuk

Optional Attendees: Alex Bossert

***Let's meet at City Hall, 215 Park Dr, Horace, ND 58047.**

This is the time that worked for the most people. I realize there are a couple who can't make this but we'll plan to include some key information beforehand so that you may participate "on your own."

Right now, the plan is to get together on-site and perform a walk/bike-audit or field review of sorts which will include:

- Walking outside with and without sidewalks (We'll plan to bring some safety vests)
- Answering some key questions or recording observations along the way

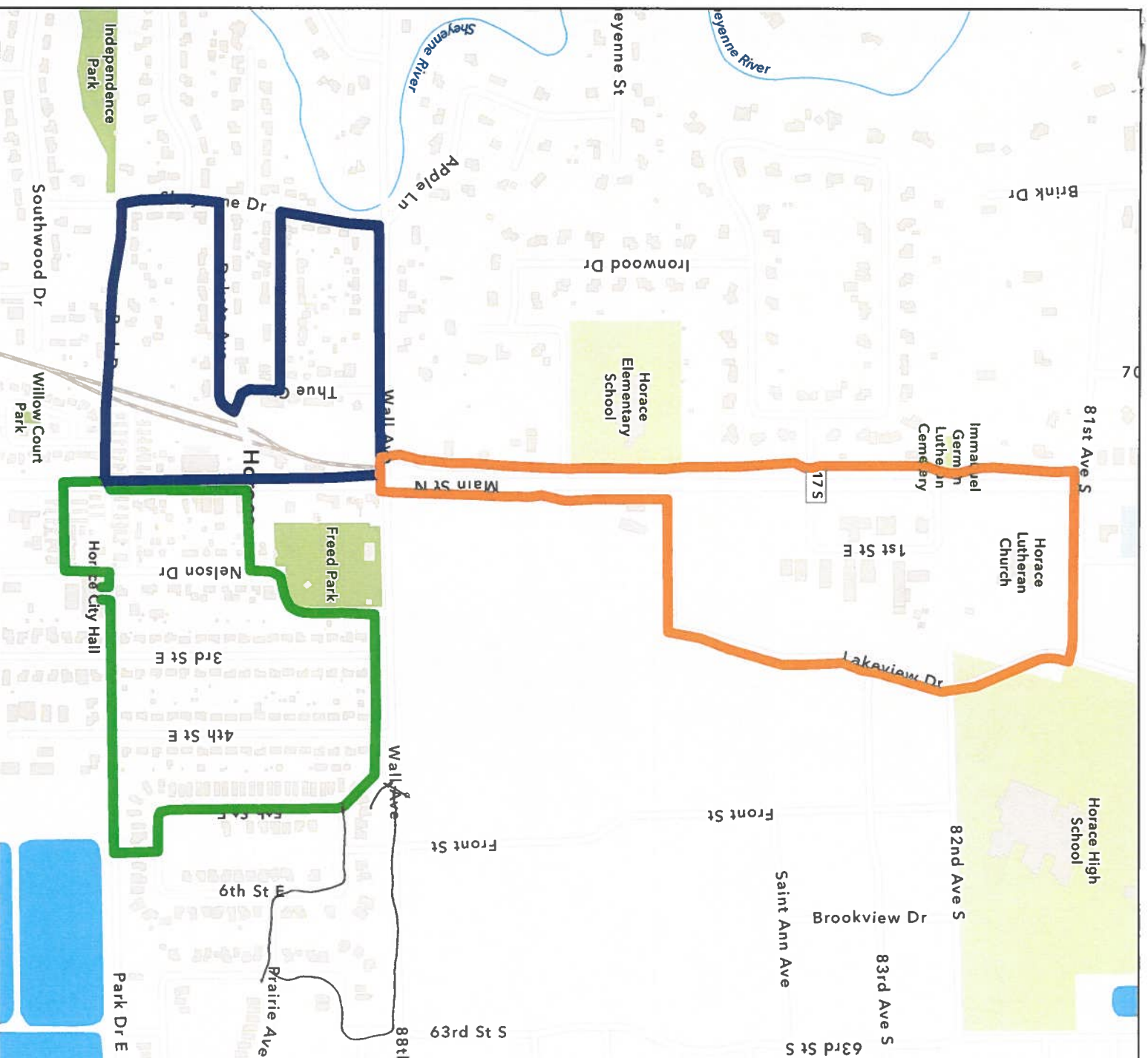
We'll keep tabs on the weather. If anyone has a challenge or concern with walking, please let me know (ljchampa@srfconsulting.com / 701-893-7407) and we'll make some adjustments.

Downtown Walk Audit

September 6, 2024

Routes

- A
- B
- C



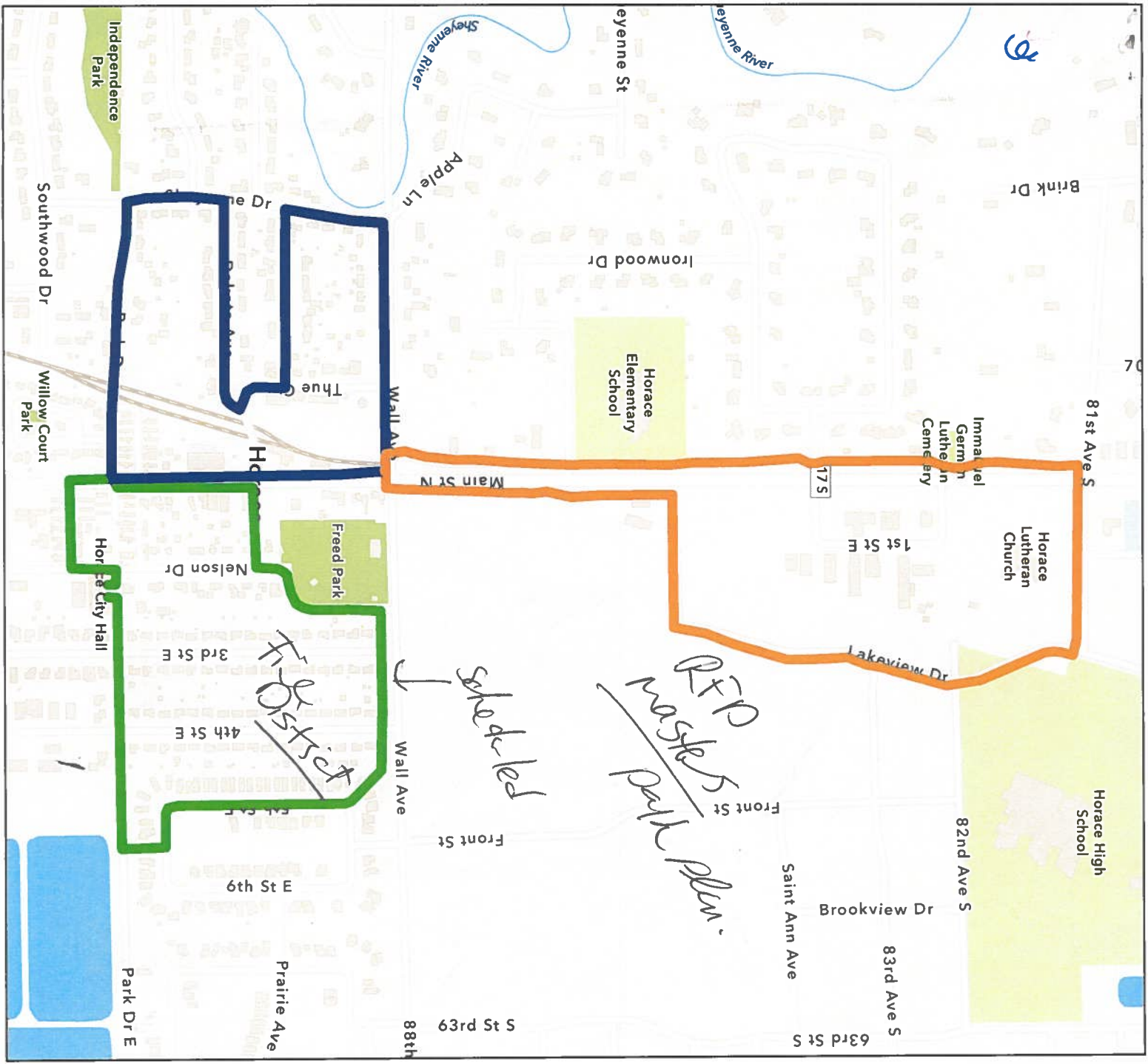
Downtown Walk Audit

September 6, 2024

Routes

- A
- B
- C

alternatives to
~~the~~ urban
 patchwork
 potential new
 collector streets
 address





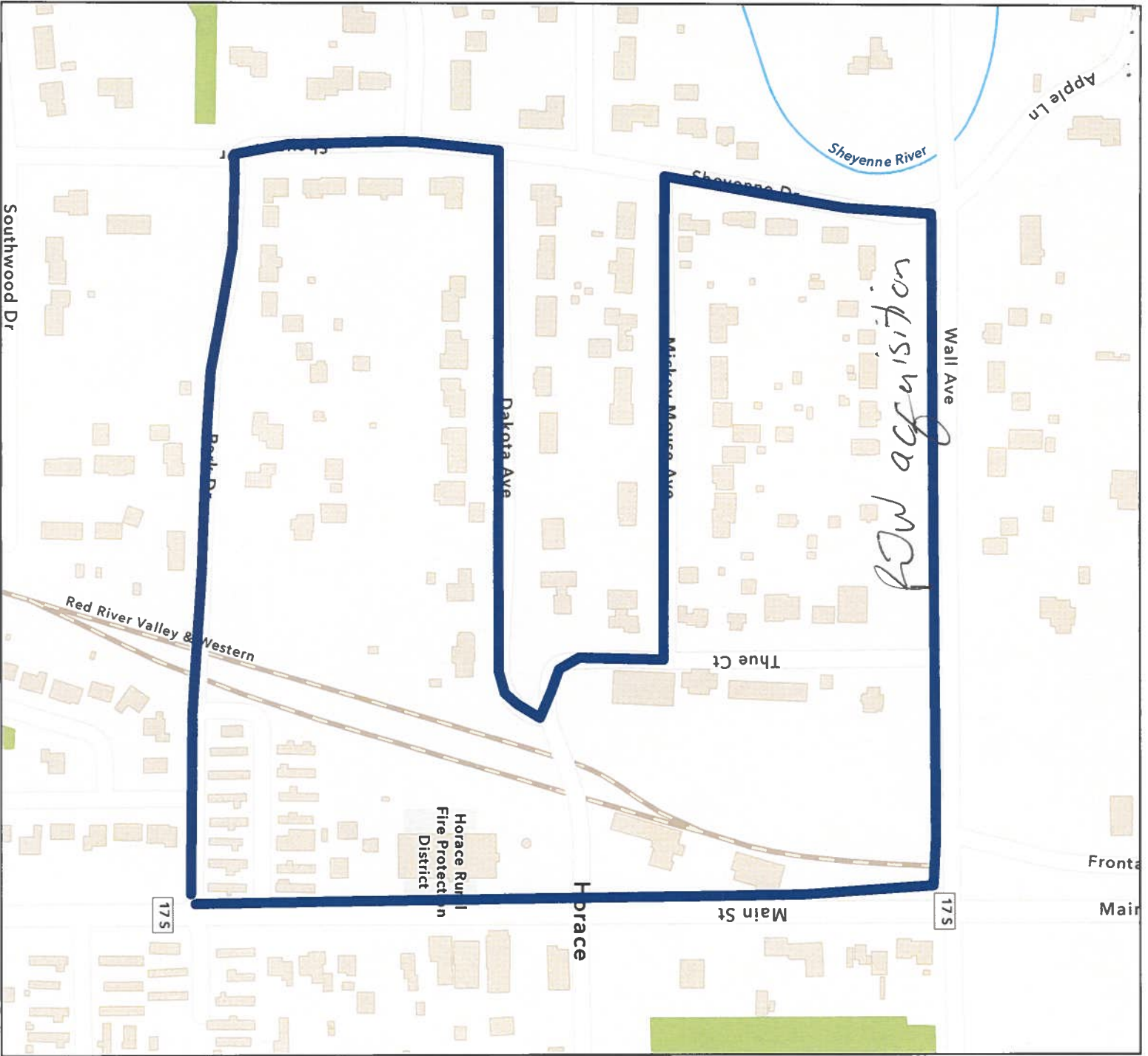
Do you see people walking or biking here?

Where are there opportunities to improve bike/ped facilities?

What are challenges to improving bike/ped facilities in these locations?

Where do you have concerns about transportation safety?

Other comments:



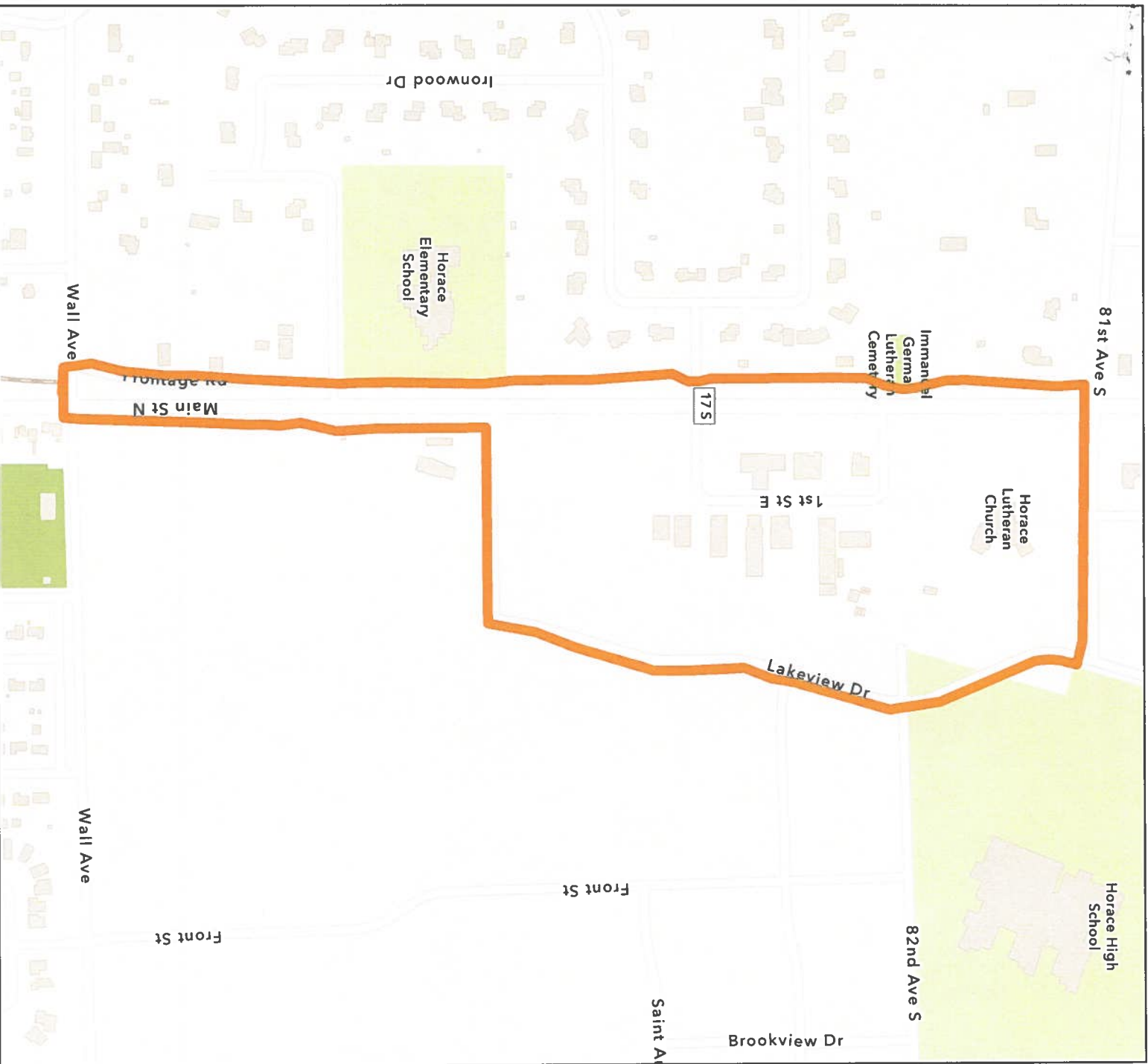
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Other comments:



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What are challenges to improving bike/ped facilities in these locations?

Where do you have concerns about transportation safety?

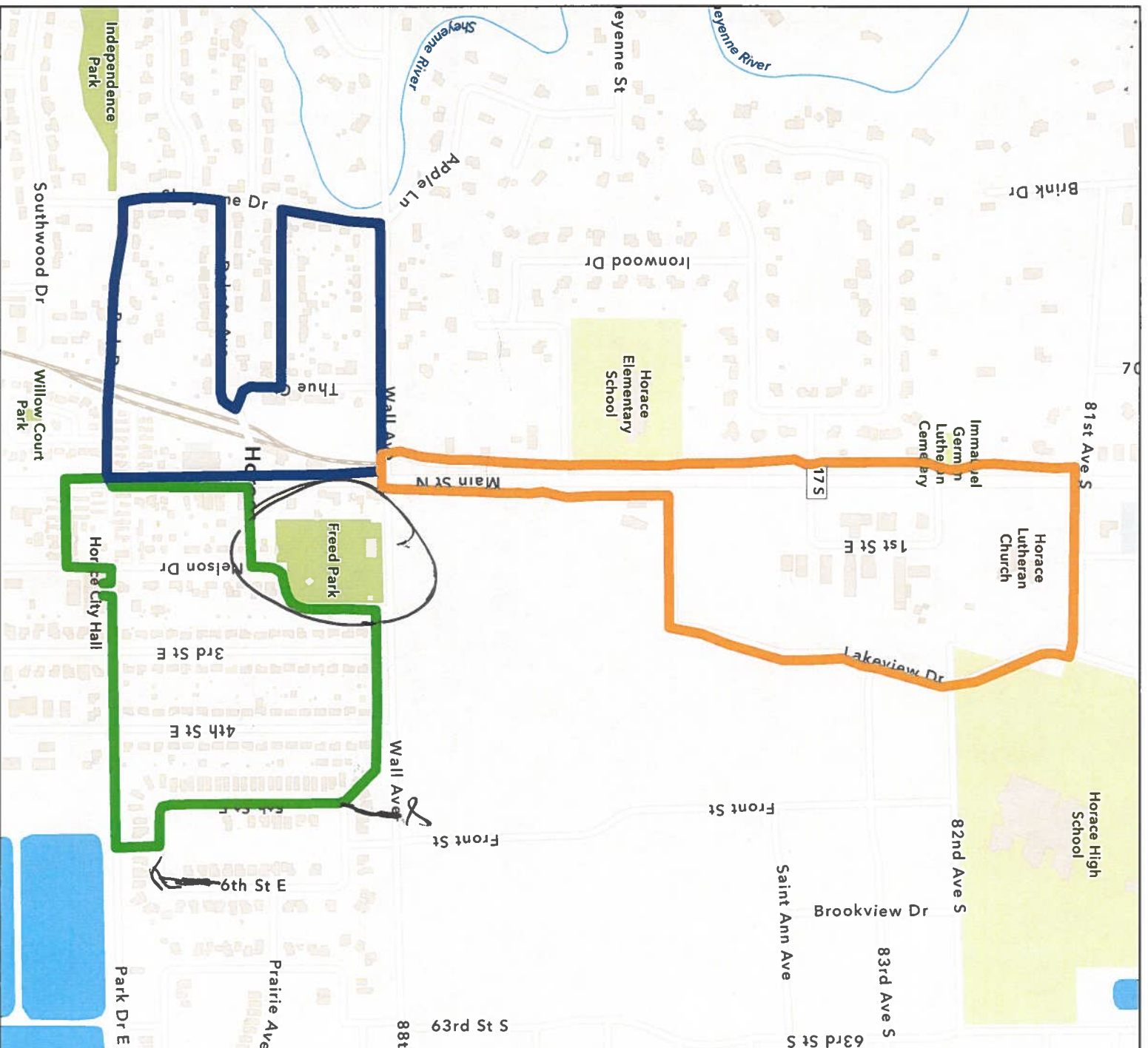
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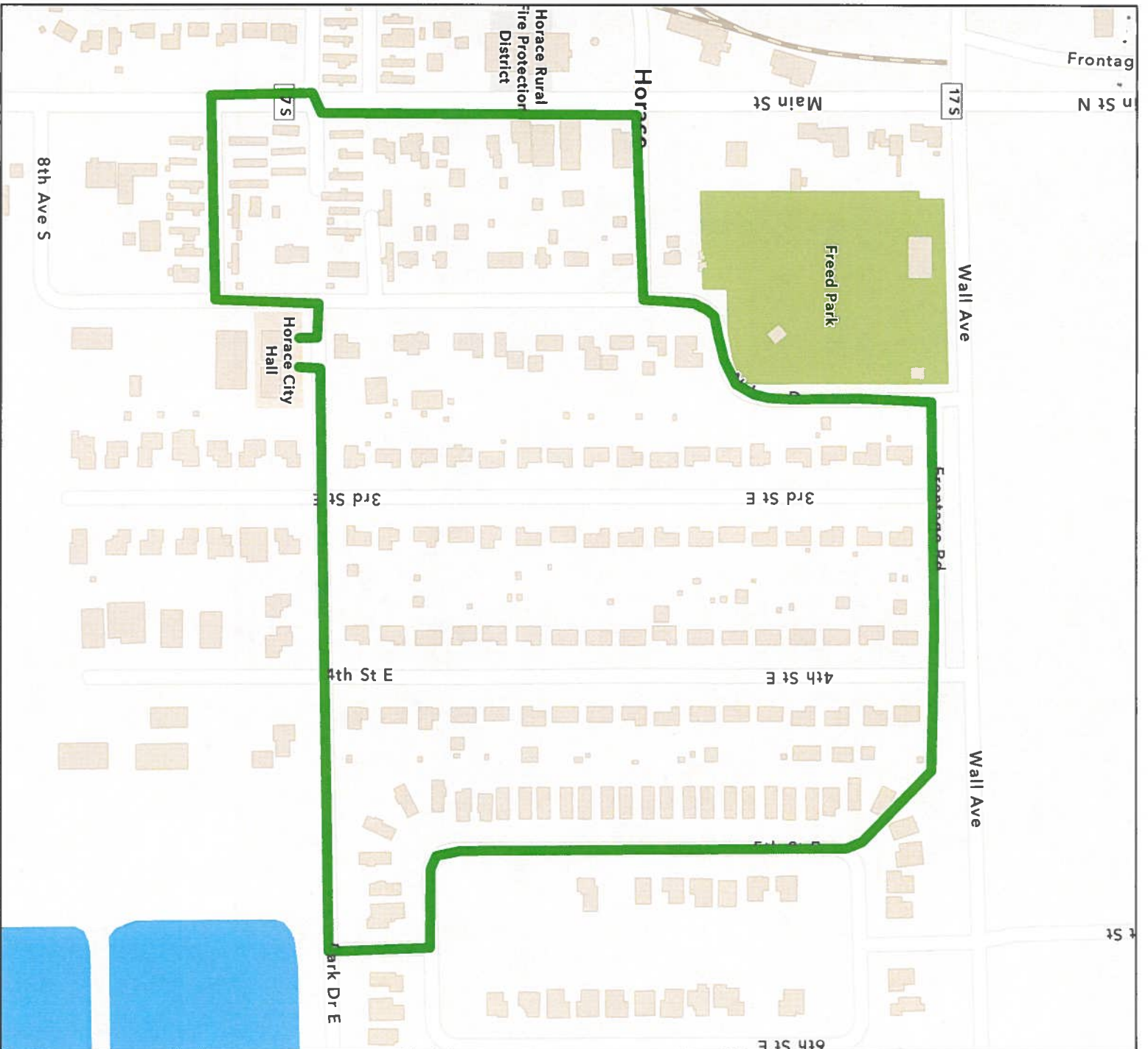
Downtown Walk Audit

September 6, 2024

Routes

- A
- B
- C





Do you see people walking or biking here?

Mixed - many sections

Where are there opportunities to improve bike/ped facilities?

Yes - when some streets are improved

What are challenges to improving bike/ped facilities in these locations?

Not in older areas that will need major

Where do you have concerns about transportation safety?

area where there are a lot of miles with residential

Other comments:

Attention for improvements

Do you see people walking or biking here?

*Multi-modal
Wall Ave*

Where are there opportunities to improve bike/ped facilities?

*In older areas
surrounding the city*

What are challenges to improving bike/ped facilities in these locations?

Where do you have concerns about transportation safety?

Other comments:

*Independence Park
is very sad - Mountain*

Southwood Dr

Apple Ln

Sheyenne River

Wall Ave

Cheyenne Dr

Michigan Avenue

Dakota Ave

Thue Ct

Red River Valley & Western

Horace Rural
Fire Protection
District

Horace

Main St

17 S

Front
Main



Do you see people walking or biking here?

Where are there opportunities to improve bike/ped facilities?

What are challenges to improving bike/ped facilities in these locations?

Where do you have concerns about transportation safety?

Other comments:

Downtown Walk Audit

September 6, 2024

IT - undisturbed neighborhood

Routes

Route 15
Circles 1-2

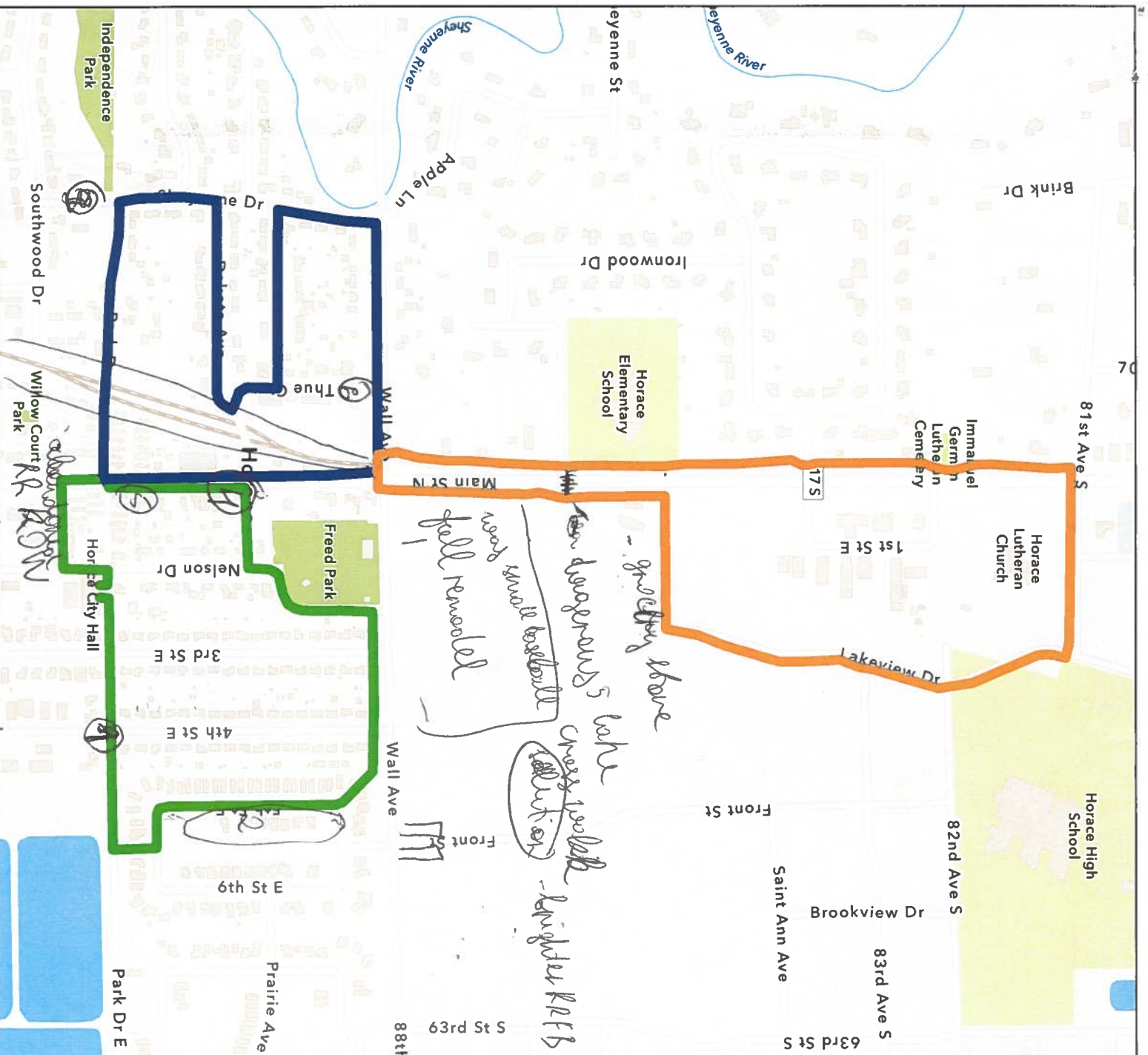
- A
- B
- C

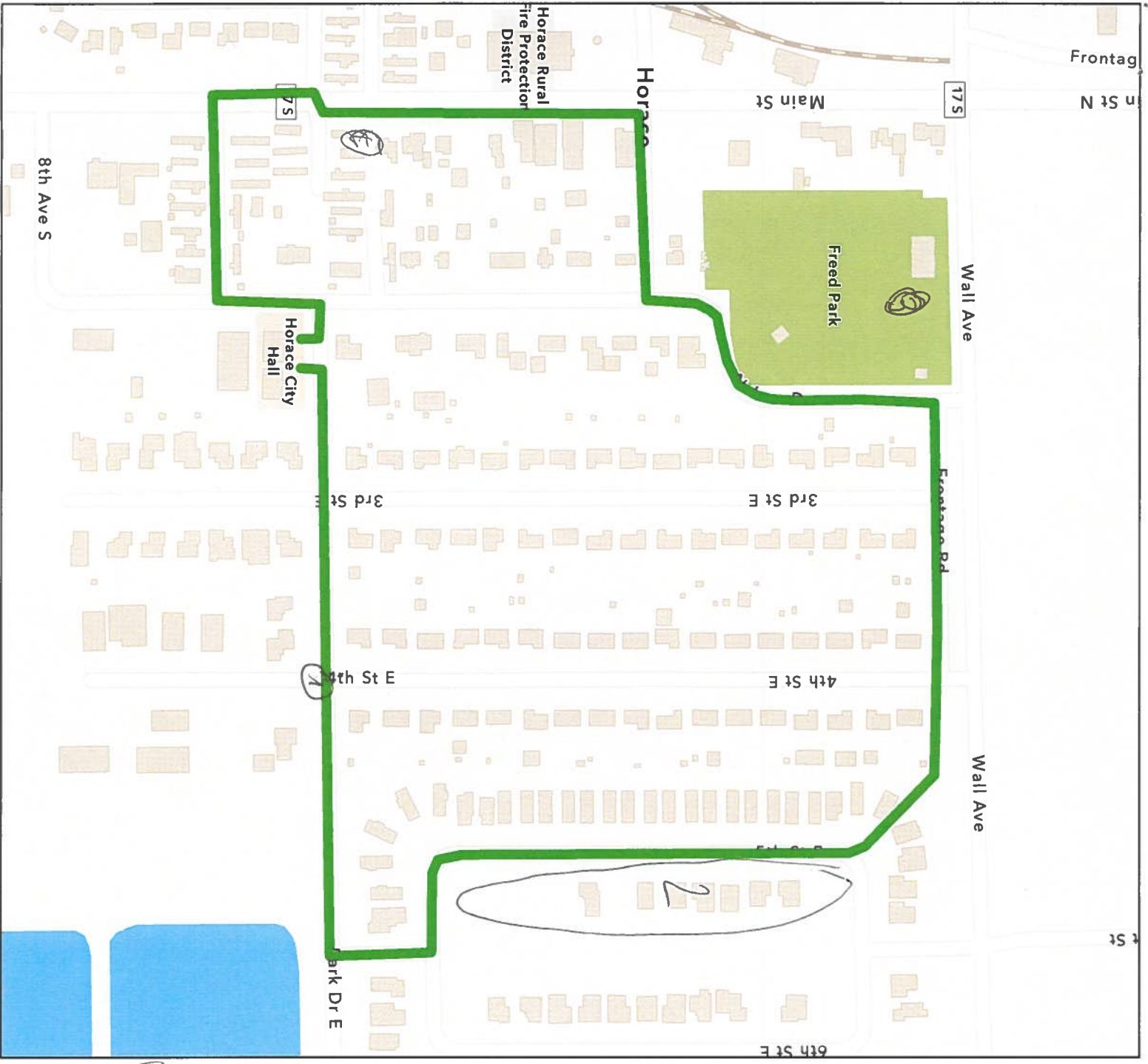
On-street parking off working hours with St people would travel the east side of the street - might be the square used food, curb and gutter - more pedestrian, sidewalks curb and gutter

① Main and Center - 2 lanes, with 2 lanes, 1 lane and 1 lane - more pedestrian



Downtown Neighborhood Plan





Do you see people walking or biking here?

1 - no jeds or bikers
2 - i-10
5 - 10 bikes

Where are there opportunities to improve bike/ped facilities?

1 - shared water
2 - sidewalks that don't curve away

What are challenges to improving bike/ped facilities in these locations?

1 - opposition

Where do you have concerns about transportation safety?

2 - people backing up their own cars

Other comments:

back baseball, baseball, pickleball, playground (about 1/2 mi)

Do you see people walking or biking here?

6 - no, but used regularly
7 - yes - 1, ride of the road

Where are there opportunities to improve bike/ped facilities?

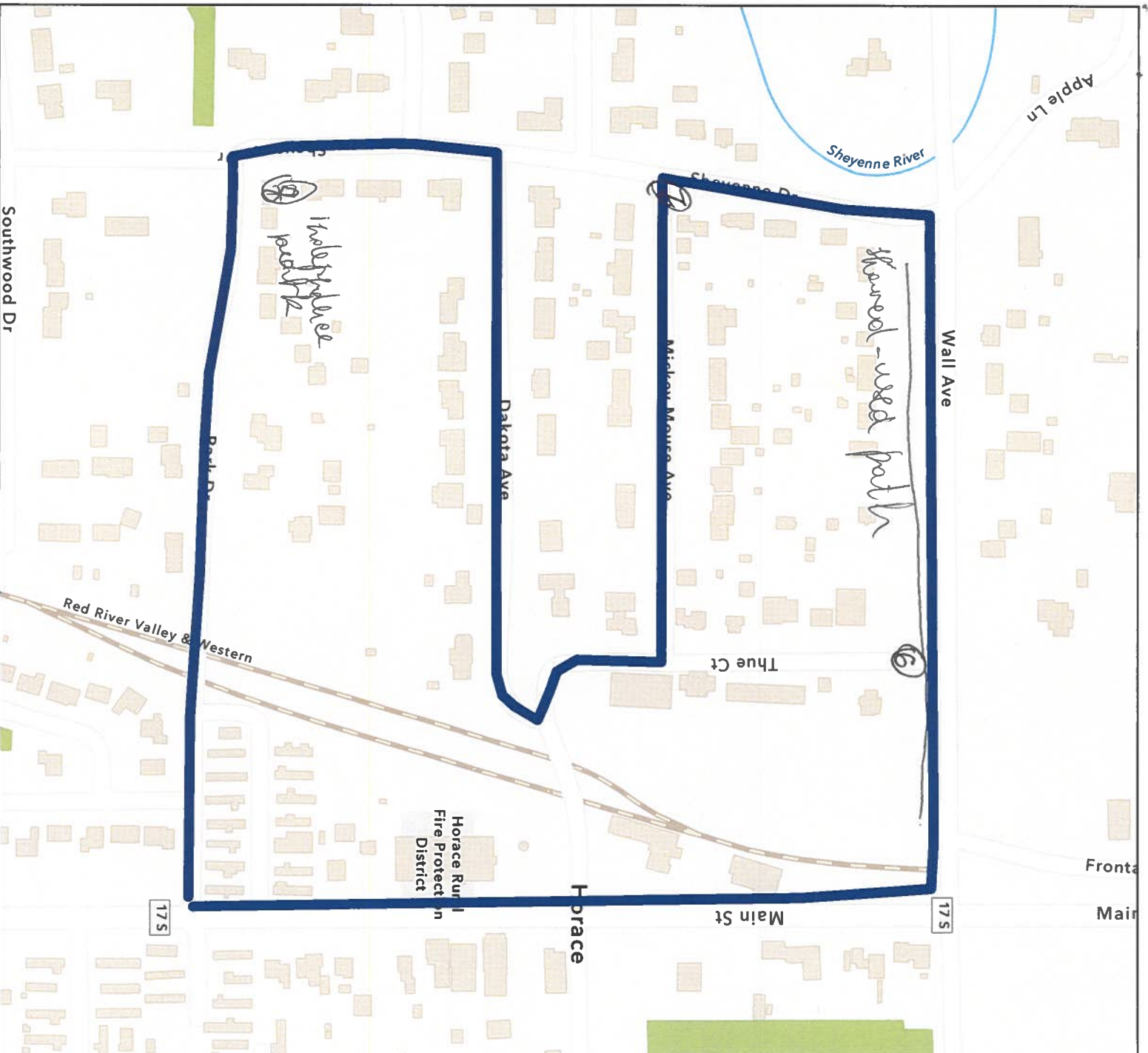
What are challenges to improving bike/ped facilities in these locations?

8 - more skating in the park

Where do you have concerns about transportation safety?

Other comments:

8 - the only area experiencing homelessness and people
stakeholder list - diversified authority





Do you see people walking or biking here?

Where are there opportunities to improve bike/ped facilities?

What are challenges to improving bike/ped facilities in these locations?

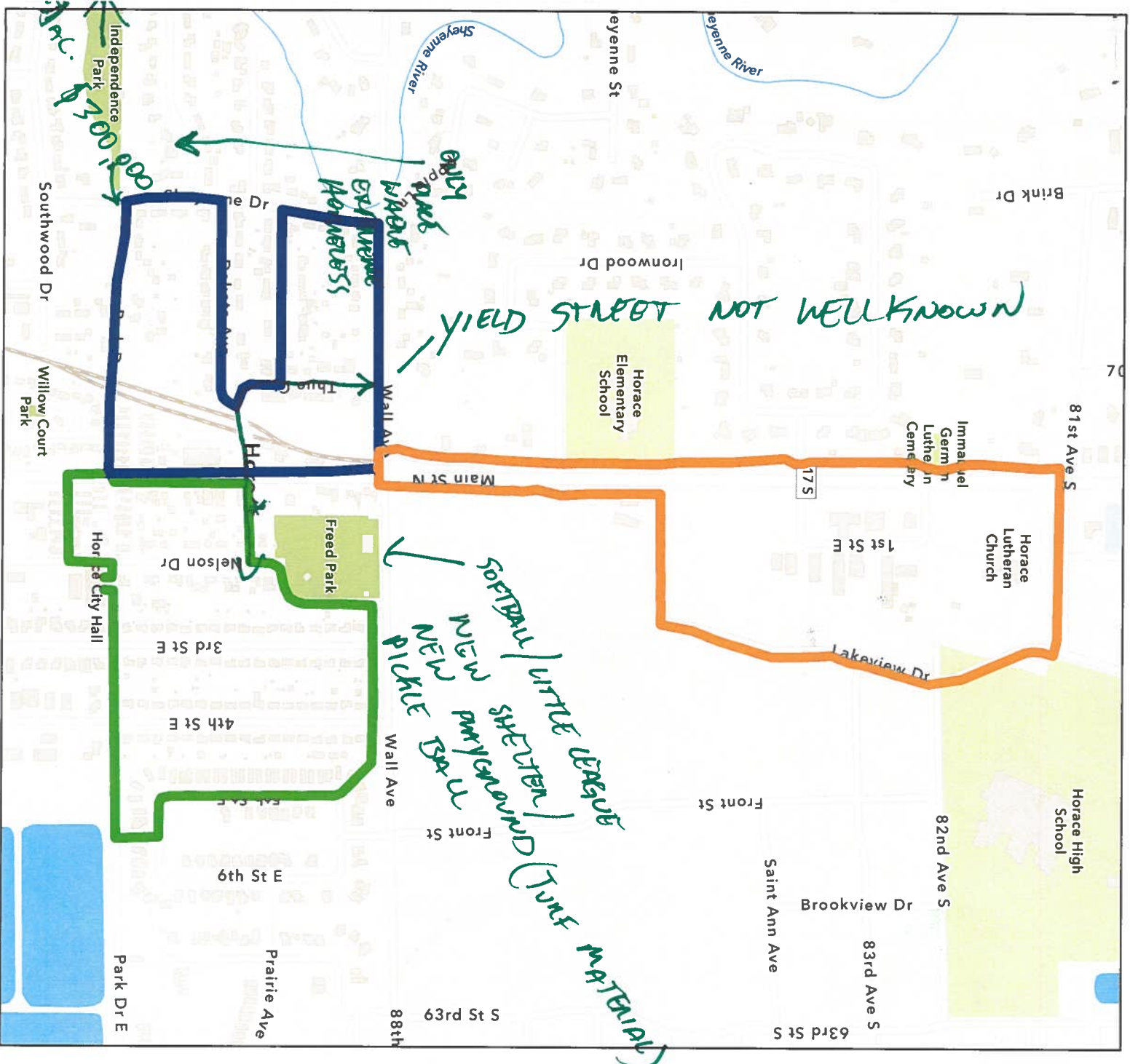
Where do you have concerns about transportation safety?

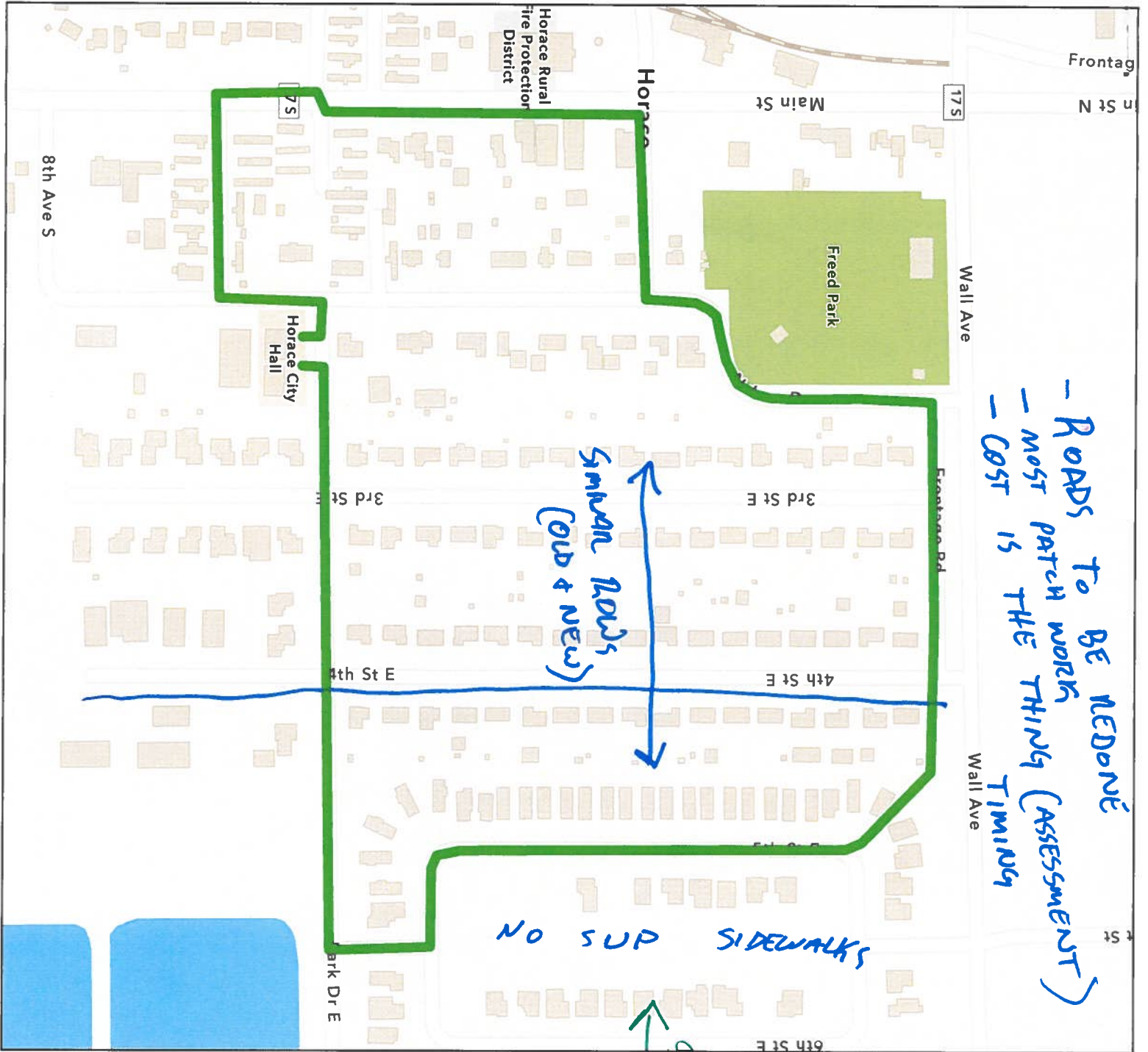
Other comments:

Downtown Walk Audit

Downtown Walk Audit

A B C





Do you see people walking or biking here?

Where are there opportunities to improve bike/ped facilities?

SUP system
What are challenges to improving bike/ped facilities in these locations?

Where do you have concerns about transportation safety?

Other comments:

- BUD Trees/Program
- 1 1/2" CAREN, SPACING
- THROUGH COVENANTS

Do you see people walking or biking here?

Where are there opportunities to improve bike/ped facilities?

What are challenges to improving bike/ped facilities in these locations?

Where do you have concerns about transportation safety?

Other comments:

DIVISION 40440179



Downtown Walk Audit

September 6, 2024

Routes

A

B

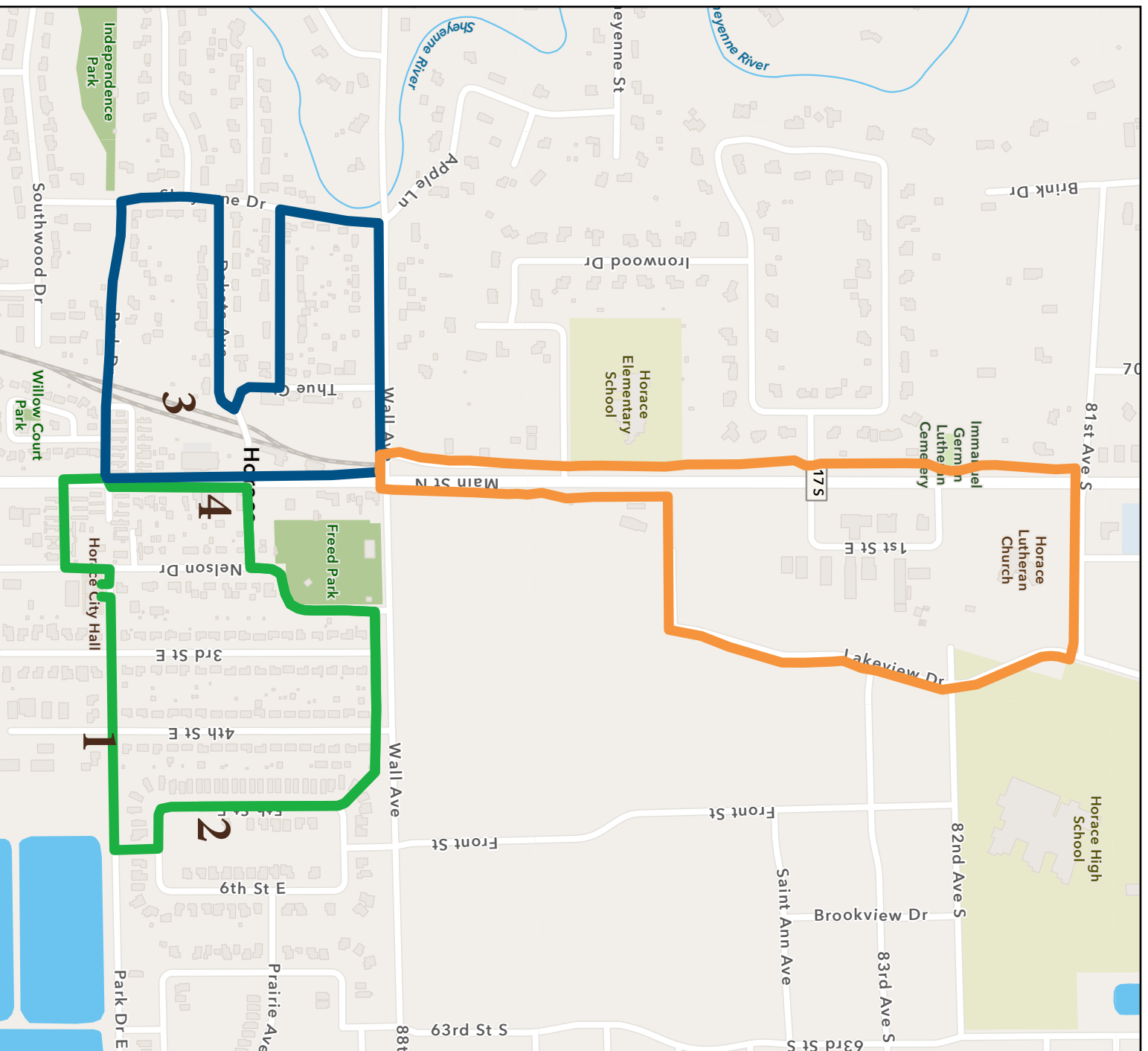
C

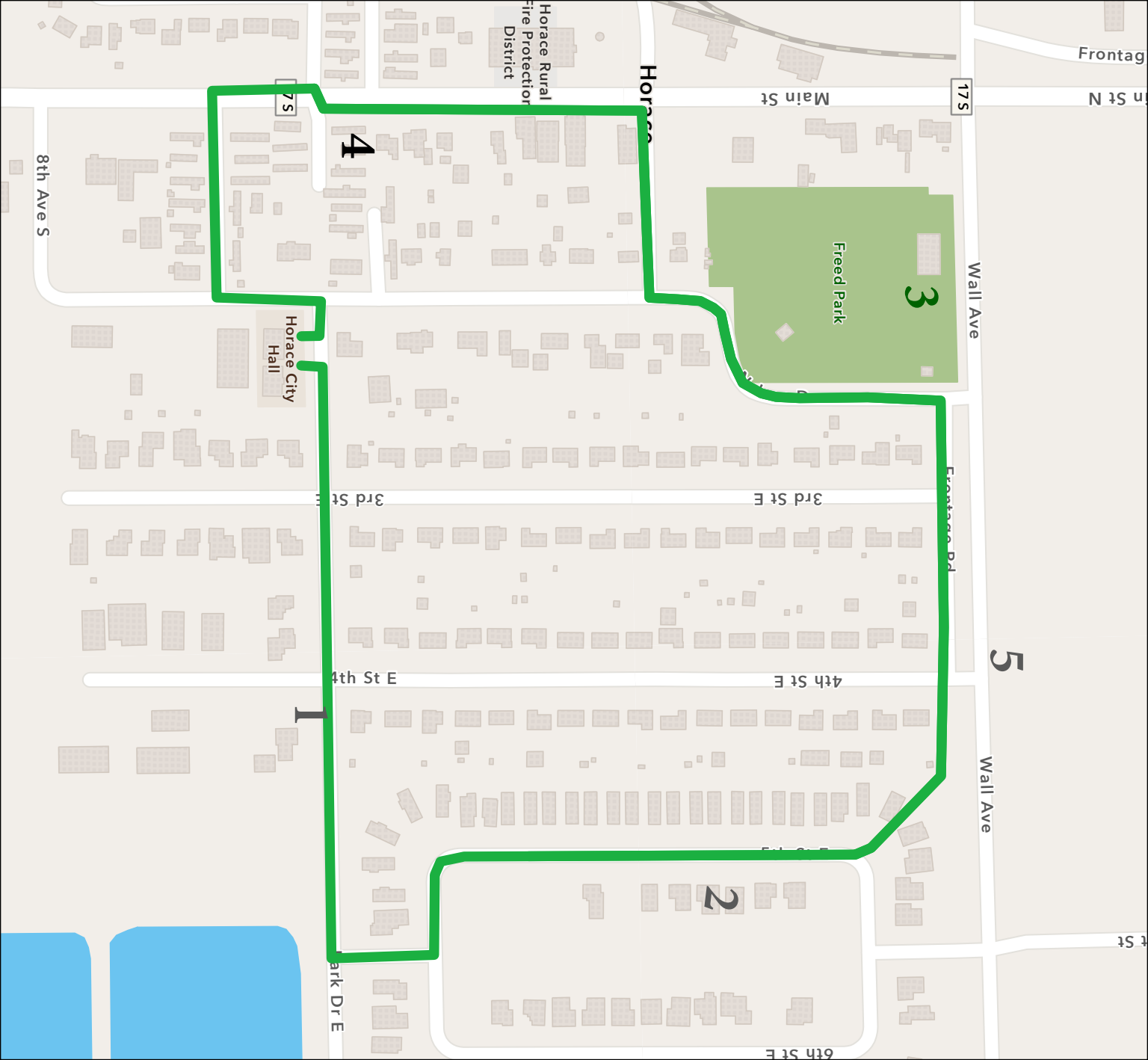
1 - On-street parking - off working hours. People would fight the cost of rebuilding 4th Street, they would want something cheaper.

2 - need for sidewalks, curb and gutter in newer subdivisions.

3 - Abandoned railroad right of way, connection to downtown.

4 - Main and Central - 2 lane, not signalized, shared-use road. Need: more signals.





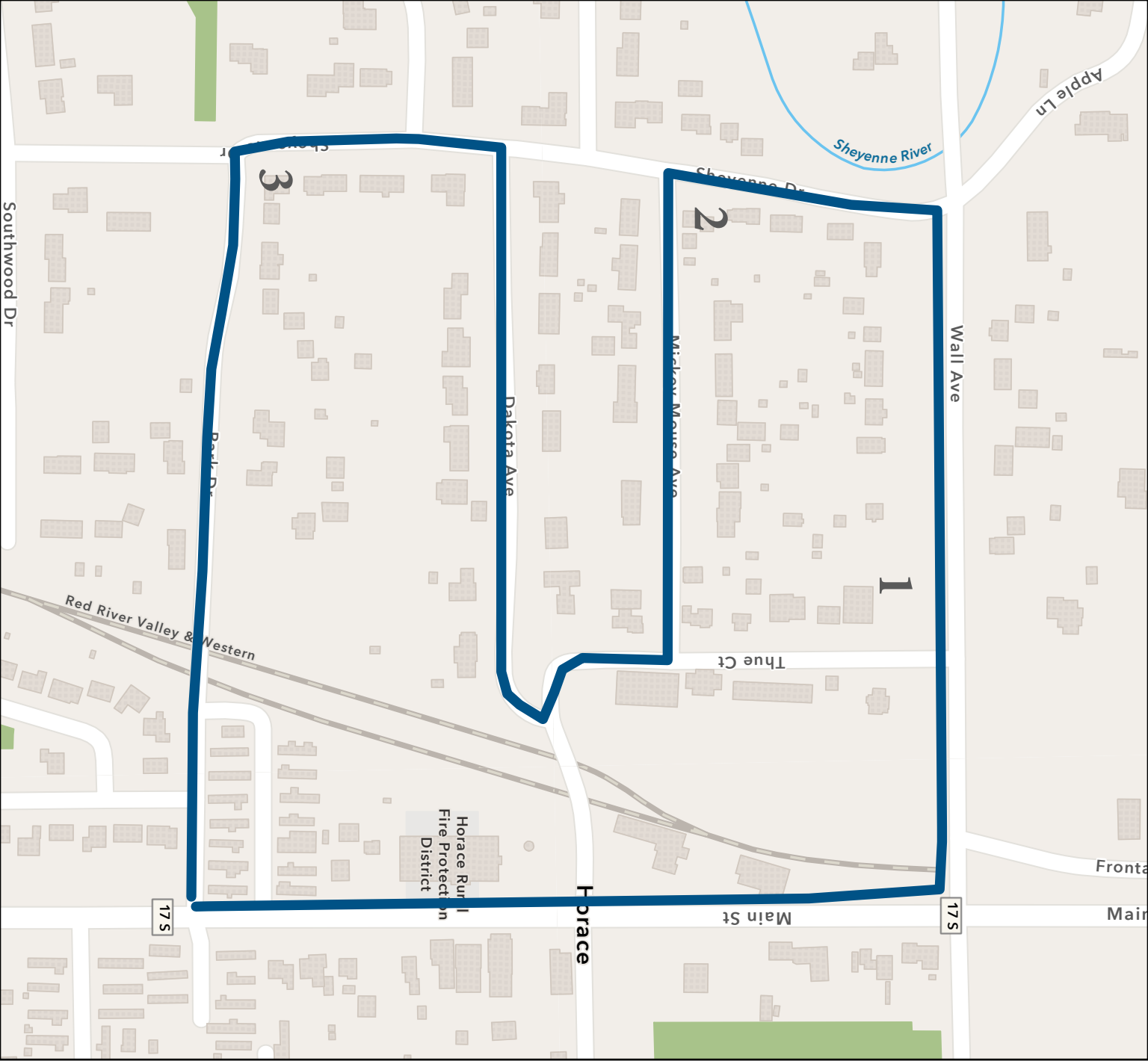
Do you see people walking or biking here?

Where are there opportunities to improve bike/ped facilities?

What are challenges to improving bike/ped facilities in these locations?

Where do you have concerns about transportation safety?

Other comments:



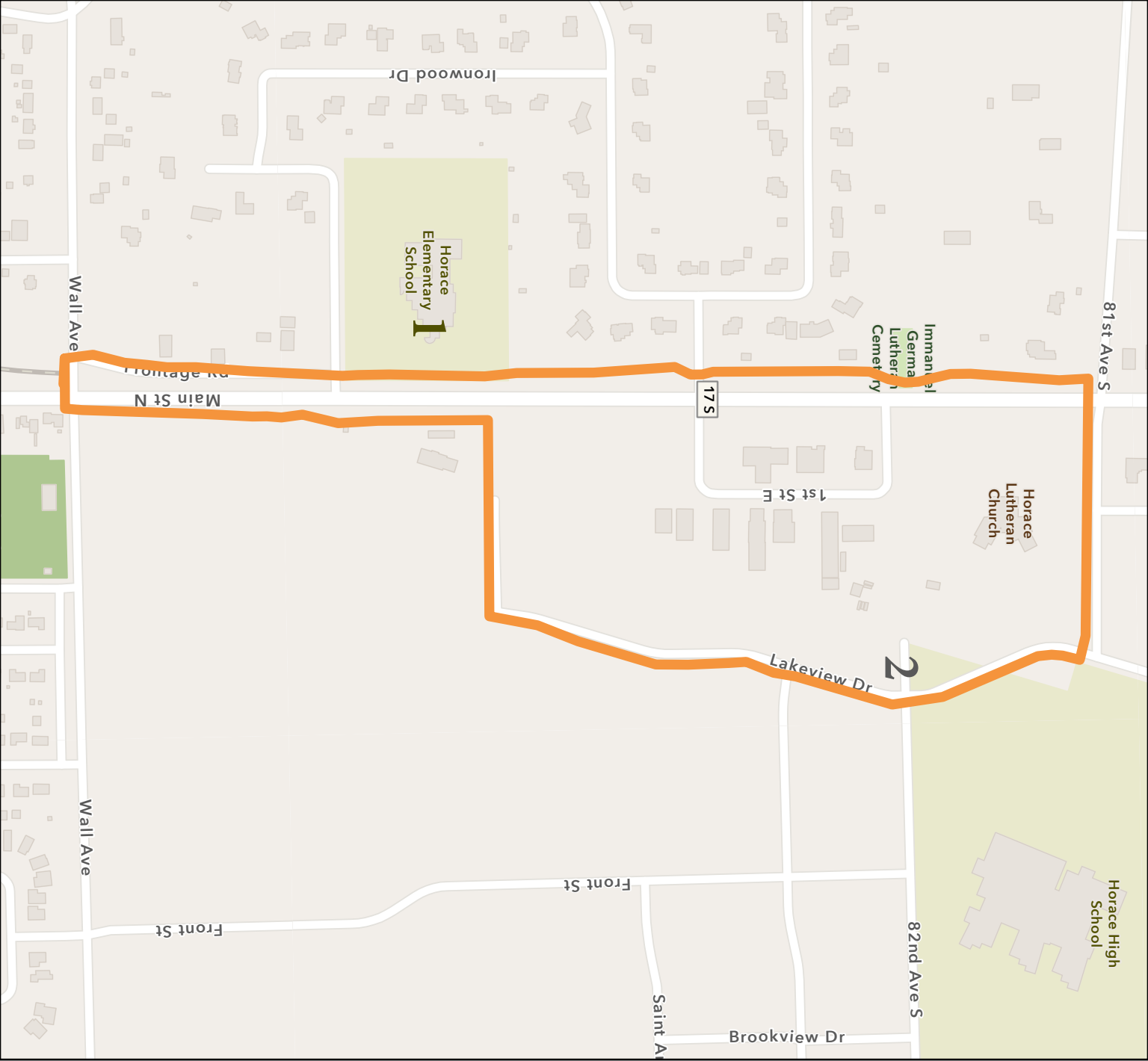
Do you see people walking or biking here?

Where are there opportunities to improve bike/ped facilities?

What are challenges to improving bike/ped facilities in these locations?

Where do you have concerns about transportation safety?

Other comments:



Do you see people walking or biking here?

Where are there opportunities to improve bike/ped facilities?

What are challenges to improving bike/ped facilities in these locations?

Where do you have concerns about transportation safety?

Other comments:

Horace Downtown Neighborhood Plan

Study Review Committee Kickoff Meeting

AGENDA

October, 2025, 10:00-11:30 a.m.

Horace City Hall Conference Room – 215 Park Dr

1) Bean Days Event Summary

- General discussion about the Bean Days engagement events and summary of feedback received.

2) Community Survey Summary

- General discussion about the online survey and summary of feedback received.

3) Community Snapshot

- General discussion about the existing conditions report.
- Update to Freed Park.
- Add hydrology layer to maps.
- Double check median home price
- Urbanized area boundary to maps
- Functional classification is new
 - 76th collector or arterial
 - 64th, once open all the way to Fargo may be collector or arterial as well.
 -
-

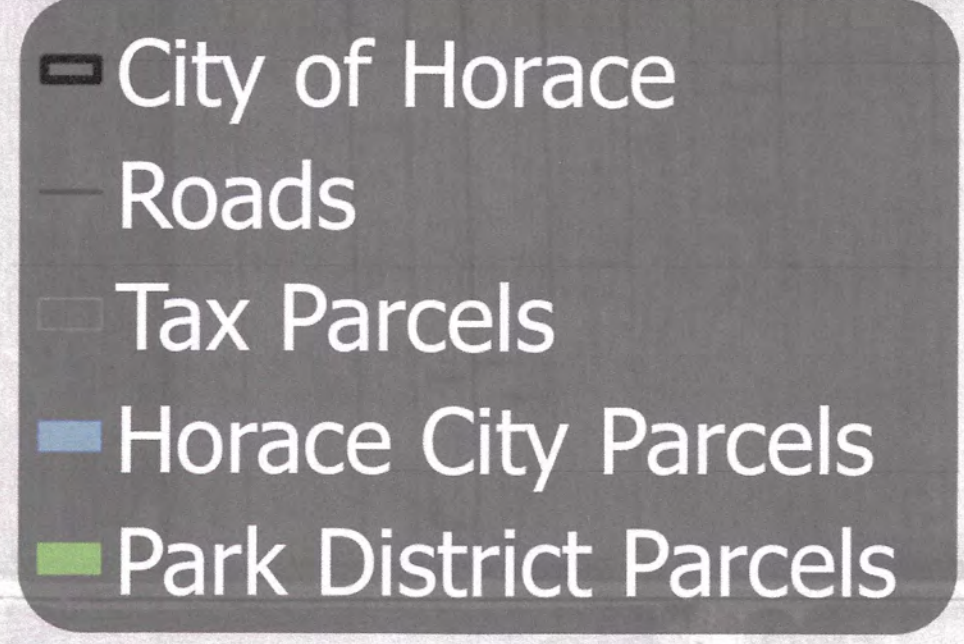
4) Workshop

- Traffic calming 76th to 100th Ave S.
- What willing to consider
 - Smaller moves /connections to the greater system
- 76th to Wall Ave. could be revolutionary
 - Consider narrower cross section

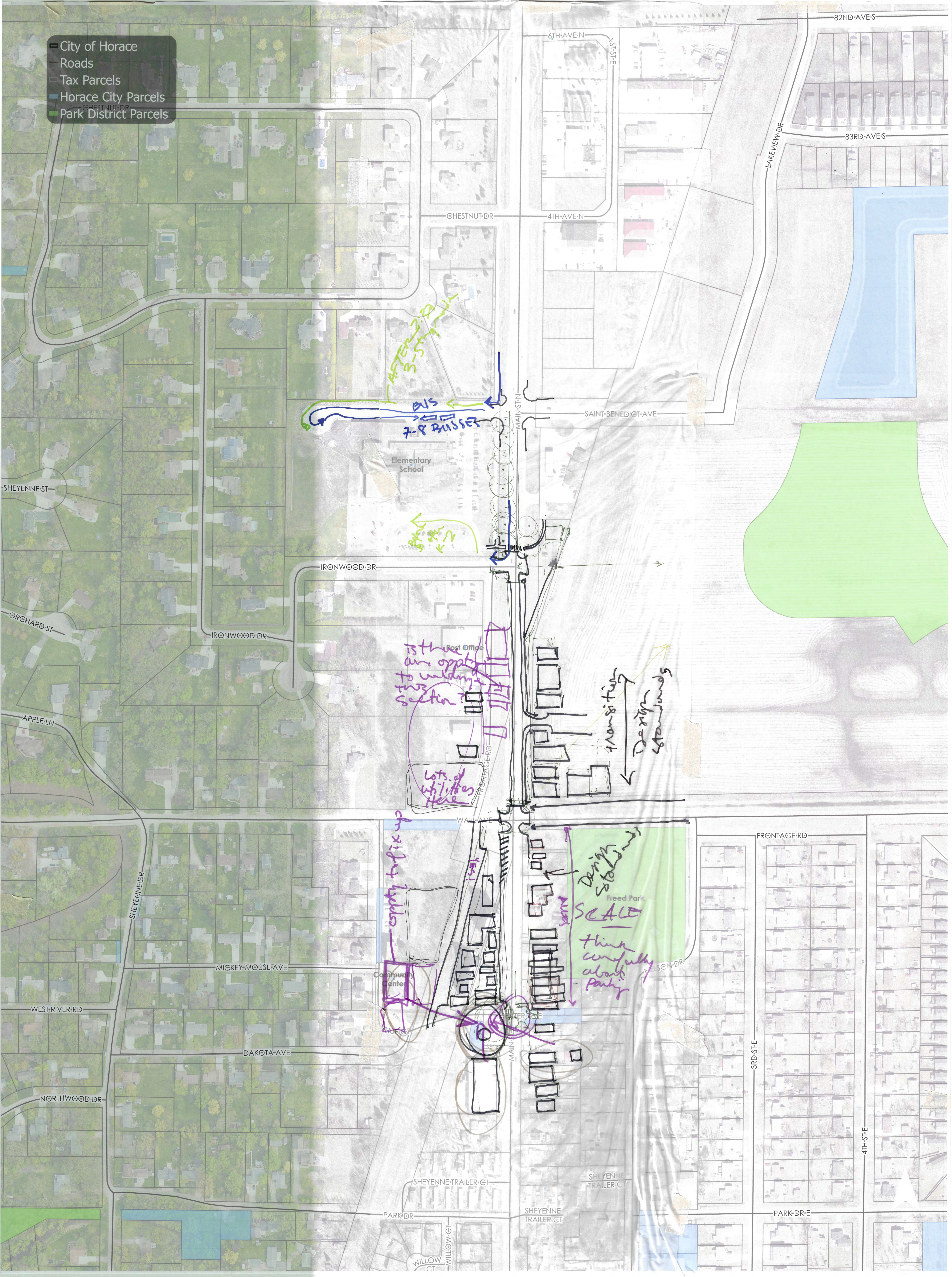
- Skinny streets, less of a barrier, slower traffic speeds
- Quick builds
- Abatement
 - Tax Differences
- County just redid CR 17
- Elementary school uses north and south sides for pickup and dropoff
 - Strict guidelines for both sides
- WFPSD does not bus students who live within 1-mile
 - Elementary has 7-8 buses
- Consider move of City Hall? Hard to access
 - New residents don't even have to visit City Hall
 - Online bills and services
 - Potential move to Sparks Addition and utilizing Horace Parks facility
- Casselton has a shared City Hall and Library
- Could help with civic engagement
- Energy Park South of 100th Ave S. will also provide some business development.
- Connection from Rivers Bend to Lost River in the works
- Yield Street throughout all rural cross sections
- Shockman: Sidewalk wanted on rural roads?
- Lee: part of the appeal of house, wanted yard space, don't want to lose by adding sidewalk
- City is up against a lot upset people
- Holding town halls for Independence Park and Terra Gardens park improvements
- Missing small town feel because of road/highway through town
- Main St. houses could be repurposed eventually if owners desired
- Size of development
 - Small scale, not big
- Community center/auto dealer – could some properties be refreshed?
- Biggest gripe – no historic buildings.
- Don't want to see super modern buildings, something that feels lived in, classic
- West Main Pizza
- Library
- Little grocery store
- Repurpose the elevators
- Brewery – city or city adjacent
- Business and small business
 - Entrepreneurs
 - May be hard in Horace
- Business association

- 1st step recommendation
 - Casselton and local banks and local support in that small town
- More information on Magnolia Development
 - Factors – economics for grain elevator repurposing
- Diversion connection for recreation plan is smart to do.
 - State is interested in state parks nearby looking at two location
 - More interest on north side of metro area
- Maddox: interested in path, understand actual rail bed challenges but could be a good connection conveyance.
- Wall Ave critical connection to Drain 27
- Trail Facilities
 - Include large concepts
- Provide guidance on grade separations
- Connections to City hall, Sparks addition
- Mid-mile crossing points of drains
- Roundabout at 100th Ave S.
- Multi-use gathering space downtown!
- Streetscape
- Utility lot nw corner of Main St. & Wall Ave.
- Small towns – scale and streetscape may be more important than use
- Freed Park parking/expansion
- Rail Corridor potential
- Renaissance Zone incentive
- Economic Development Committee
 - Need to do more groundwork
- Could Renaissance Zone needs be met through Downtown Neighborhood Plan?
- Do not propose removing anyone from their homes as part of the vision for this plan.

5) Next Steps – 5 minutes



- City of Horace
- Roads
- Tax Parcels
- Horace City Parcels
- Park District Parcels



Buses
7-8 BUSES
4-5-6-7-8
3-5-6-7-8
2-3-4-5-6-7-8
1-2-3-4-5-6-7-8

is there
an opp'ty
to und...
this
section?

Lots of
white lines
here

transition
Design
Standards

Design
Standards
SCALE
think
carefully
about
parking

Horace Downtown Neighborhood Plan

Study Review Meeting #4

AGENDA

December 6, 2024, 1:00-2:30 p.m.

Horace City Hall Conference Room – 215 Park Dr

1) Welcome & Introductions – 5 minutes

2) Community Snapshot – 5 minutes

- The updated community snapshot or existing conditions report was presented.

3) Stakeholder Meetings – 5 minutes

- The SRC was updated on stakeholder meetings and takeaways.

4) Concepts – 45 minutes

- **Concept 1**
- Shockman: Lakeview Addition park land? On current lakeview plans, considered?
 - Olson: considered through community connections
- Shockman: not a good grasp on maps, just curious.
- Olson: Visualizations will help public to understand concepts much better
- **Concept 2**
- Luing: Making turns, radius, etc. – mini roundabout not feasible for trucks need to double check.
- Maddox: enough room almost for a full-size – mini roundabout for sure
- Maddox: likes the trail concept update
- Champa: show parking
- Shockman: whats a village concept?
 - Olson: residential concepts – buildings to stay
- Sweeney: zoning can change, etc. overtime reinvestment etc. people may see opportunities to sell or utilize their home in a different way. Not forcing anyone out.
- Maddox: ingress/egress

- Alley E of Main St. is Park District Property? Could be redeveloped as two-way with parking on both sides.
- Park district owns/city has utilities – commercial traffic may pose a problem – would need to look at MOU/MOA
- Shockman: parking by playground
- Hellman: arterial roads can have parking?
- Would push back on parking north of Wall Ave.
- Maddox: mid-block crossing to Freed Park would be good
- Preliminary analysis of parkin – 4 to 5 spaces for 1000 sq. ft. maybe misunderstanding intensity.
- Push parking from off-street to on-street parking.
- Public gathering space
- Shockman: prefer concept A frontage over Concept B – day care/post office
- Hellman: challenges south redevelopment – needs attention

5) Transportation Strategies – 15 minutes

- There was general discussion about the transportation strategies.

6) Metrics for Concepts – 10 minutes

- There was general discussion about potential metrics to rate or prioritize concepts.

7) Next Steps – 5 minutes

Horace Downtown Neighborhood Plan

Study Review Committee Meeting #5

Notes

May 22, 2025, 2:00-3:30 p.m.

Horace Meeting House Conference Room – 7951 Jacks Way #116

Action Items

Responsibility	
Consultant Team	<ul style="list-style-type: none">• Send SRC 05 Materials for committee review/comment<ul style="list-style-type: none">◦ Meeting Summary/Notes◦ Meeting Presentation◦ Connection Strategies◦ Intersection Strategies◦ Refine Downtown Concept 1 (as discussed in SRC 05)• Make adjustments/refinements as necessary or per SRC feedback• Implementation plan/strategies/priorities• Assemble Draft Plan Document
SRC	<ul style="list-style-type: none">• Review and provide feedback on materials provided from SRC 05
Jace/City of Horace	<ul style="list-style-type: none">• Send Luther Drive plat – highlight or note future Collector alignment• Send Energy Drive plat – highlight or note future connection to FM Diversion Recreation infrastructure south of 100th Avenue

Attendees

- Joel Luing, City of Horace
- Ron Erickson, Horace Planning & Zoning
- Jace Hellman, City of Horace
- Michael Maddox, Metro COG
- Luke Champa, SRF
- Tanya Olson (Virtual), Tallgrass
- Jim Dahlman, City of Horace

1) Welcome & Introductions

2) Workshop & Survey Summaries

3) Downtown Concept Refinement

Jace: Parking Analysis to be completed for Lakeview Development Proposal

Michael: something like 2.85 spaces per 1,000 sq.ft. in the Parking and Access Study -may want to reference.

Trail Connection: property owners have signed intent to purchase memoranda with the MFDA.

*Paige: if one property doesn't go through will MFDA approach the City?
Or could things fall apart?*

Jace: maybe, everyone has signed at this point

Michael: this is meant to be visionary though, do we still show as part of a 20-year (2045/2050) vision?

Jace: tender, loving language if shown in the plan (trail along railroad ROW)

Maddox: suggests leaving a scenario for potential piecemeal of off-street trail along rail line through incremental property development and/or turnover.

Ron: Fire Hall likely never to switch hands or redevelop though.

Jace: MFDA is trying to avoid public sale – adjacent property owners are being offered ROW to the centerline

*Jim: have wanted a utility trunk line through here but hasn't advanced.
Perception the plan is going to force private property decisions*

Michael: is the trail on RR ROW part of the vision?

Jace: don't want to scratch it entirely but really need to think about how the message is crafted.

Paige: how many property owners are we talking about along the stretch from Wall Ave to Park Dr?

Jim: 30 properties

Maddox: Let's push to keep as a concept.

Jim: I'd keep back alley between Center and Park for emergency response out of the west side of the Fire Hall – by ordinance would require a sidewalk

Tanya: showing expanded Fire Hall

Jace: is there space for angled on-street parking?

Tanya: yes, plenty of space on the west side of Main however, less likely on the east side of Main St.

Jim: is angled even recommended on a Minor Arterial with future significant traffic volumes?

Michael: it is context driven at the end of the day. Speed, adjacent land use, etc. play a huge role.

Jim: have spoken to Lakeview developers about angled-parking

Michael: assume, less time to parallel park than angle park?

Jace: parallel parking both sides of Main, angled on west side south of mid-block crossing between Wall and Center.

Grain Elevator Block: proceed without option that shows public street, instead use the one with meandering path.

Michael: northwest corner of Wall/Main St very limited development options.

Dahlman: correct

Jace: like the parking lot there actually the middle school or one of the school baseball teams is going to use Freed Park and could serve as a good overflow parking area for events at the Elementary School, Freed Park, and other places downtown.

Paige: Agreed.

4) Transportation Strategies

Jim: Route 4 east side gap from 76th Ave to the south is programmed for 2026.

Maddox: keep in there for now. Have ran into issues with local jurisdictions removing federal funds from projects, don't want to have it drop out of the plan just yet.

Jace: Drain 27, Willow Grove 2nd addition have 50' lot along Drain and Gap from Willow Grove to 64th

- Park Drive will not extend across Drain 27*

- Cub Creek South on east side of drain*

- S of 100th have a 4 acre parcel owned by Park District and then 40' ROW along southern embankment – platted as way to incorporate future Diversion Recreation Plan recommendations and future connection directly to the Diversion Channel (Horace Energy Park Plat)*

Paige: paths through Independence Park in '25 or '26, pursuing grant, if received will be '26.

- Why no School Zone on CR 17*

Jim: County has had control there and controls speed

Jim: wouldn't call rural local streets pedestrian facilities. Have had one resident recently ask about sidewalk.

5) Policy Brainstorm

Michael: why considering both Grade Separation and HAWK/Pedestrian Hybrid Beacon?

Luke: cost, primarily

Michael: 100th Avenue S – new thought is for a north-south route on the west side of the diversion channel and leaving 100th Ave. up to the City as arterials will need to be built out no matter what.

- Liberty/Luther future Collector*

6) Next Steps

- SRC review of transportation strategies*

- Consultant team to refine final vision for downtown based upon SRC 05 feedback and put together implementation strategy.*

- Next time SRC sees this information it will be in a draft plan document*

-Pursuing individual meetings with council members to discuss recommendations of the plan, instead of having a final public open house.

-Schedule for final plan/deliverable timeline being discussed between consultant team.

Horace Downtown Neighborhood Plan

Engagement Summary (Aug. - Sept. 2024)



Community engagement will guide the Downtown Neighborhood Plan:

The Downtown Neighborhood Plan will help the community celebrate and enhance the identity and character that makes Horace such a great place. Throughout the nine-month planning process, we will work with residents, businesses, and visitors to guide the vision, direction, and recommendations of the study. The initial phase of engagement introduced the study to residents, both online and in-person, and established the foundation for planning and visioning that will guide outcomes and recommendations included in the study.

How we engaged with Horace residents and visitors:

The study process connected with community members and stakeholders to solidify project direction, identify community vision, and explore what Downtown Horace should or should not become in the future.



Stakeholder Engagement

The project engaged a variety of stakeholders at the outset of the project, including city staff, local businesses, and interested residents to introduce the Plan and gather feedback.



Bean Days (Sept. 6 & 7)

Bean Days was an opportunity to meet residents and discuss the Plan. The project participated in the local parade and hosted two pop-up events. More than 300 residents were engaged at Bean Days, with several different input opportunities designed for all ages to participate.



Community Survey

An online survey was promoted broadly and open for public input from September 1 through October 1. The survey received 502 responses and asked questions about transportation, development preferences and future priorities.

Key themes from our initial community engagement:

- Feedback highlights the importance of preserving small town feel while also considering the potential for growth and development that aligns with these values.
- The community recognizes rapid growth in Horace. There is desire to maintain small-town character.
- Residents requested easy access by all modes (biking, walking, golf carting) to downtown, parks, schools, and community spaces.
- There is a clear preference for community-centric and traditional downtown development concepts compared to modern development and high-density styles presented.





The Fargo-Moorhead Metropolitan Council of Governments and the City of Horace are preparing a Downtown Neighborhood Plan to address growing multimodal mobility needs to and from downtown as the community grows.

The Horace community has been growing and changing. The city's population has increased by over 500 percent since 1990. The Downtown Neighborhood Plan will look to build upon the small-town character of Horace by collaborating with Horace residents to explore and define the community's identity and envision the future of downtown.

The planning process will help the community identify and strengthen the central business district, enhance multimodal mobility, and improve access to commercial destinations, social activities, and cultural experiences in and around downtown. The goal is to develop consensus around a long-term vision for the city's core and identify implementation strategies and project priorities to further the vision.

What is the project schedule?

Public engagement for the Horace Downtown Neighborhood Plan will be conducted in several phases over a 12-month period. In the summer of 2024, the Project will engage the community to set the goals and vision for the plan. During the fall and winter of 2024, the Project will develop concepts and alternatives based on public input received and re-engage the community to gather feedback to help refine concepts. A final round of community engagement will occur in the spring of 2025 to confirm Project goals and objectives and ask the community to provide input on the recommendations identified.

Get involved!

We will be engaging with residents, businesses, and visitors throughout the process. Look for our booth Saturday, September 7th at the Bean Days event to learn more and share your ideas!

To subscribe for updates and find more information on the Downtown Neighborhood Plan, please visit www.fmmetrocog.org/projects-rfps/Downtown-Horace



Keep Your Pets Leashed

Please remember that all pets must be leashed when in public areas. This law applies anytime your pet is not on your private property, without exception for the time of day or location. Even if you don't see any other people or pets around, it is still necessary for your furry friend to be leashed.

The City's ordinance specifies that a physical leash, whether retractable or fixed, must be used. Electric collars are not allowed. While we understand that many pets are well-behaved, it's essential to consider that not all dogs are as friendly or well-socialized. Keeping your pet leashed is a responsible way to ensure everyone's safety.

It's important to note that there are penalties for failing to comply with this requirement. A first offense may result in a verbal warning, while subsequent violations can lead to written warnings or fines. In the City of Horace, the first offense carries a \$20 fine, and the second offense within the same calendar year increases to \$100.



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Horace Downtown Neighborhood Plan



What is the Horace Downtown Neighborhood Plan?

Metro COG and the City of Horace are partnering to create a Downtown Neighborhood Plan that will provide direction to address development and transportation connections to and from Downtown and the neighborhoods surrounding it as the community grows.

Why does Horace need a Downtown Neighborhood Plan?

As Horace continues to grow and change, thoughtful planning is essential to ensure that the community will continue to be a welcoming and safe place to live, work, and visit. A vibrant, accessible downtown is vital to Horace's long-term success.

Schedule



We want to hear from you!

Throughout the process, we will be engaging with residents, businesses, and visitors. Help us spread the word and share your ideas for the future vision of Horace.

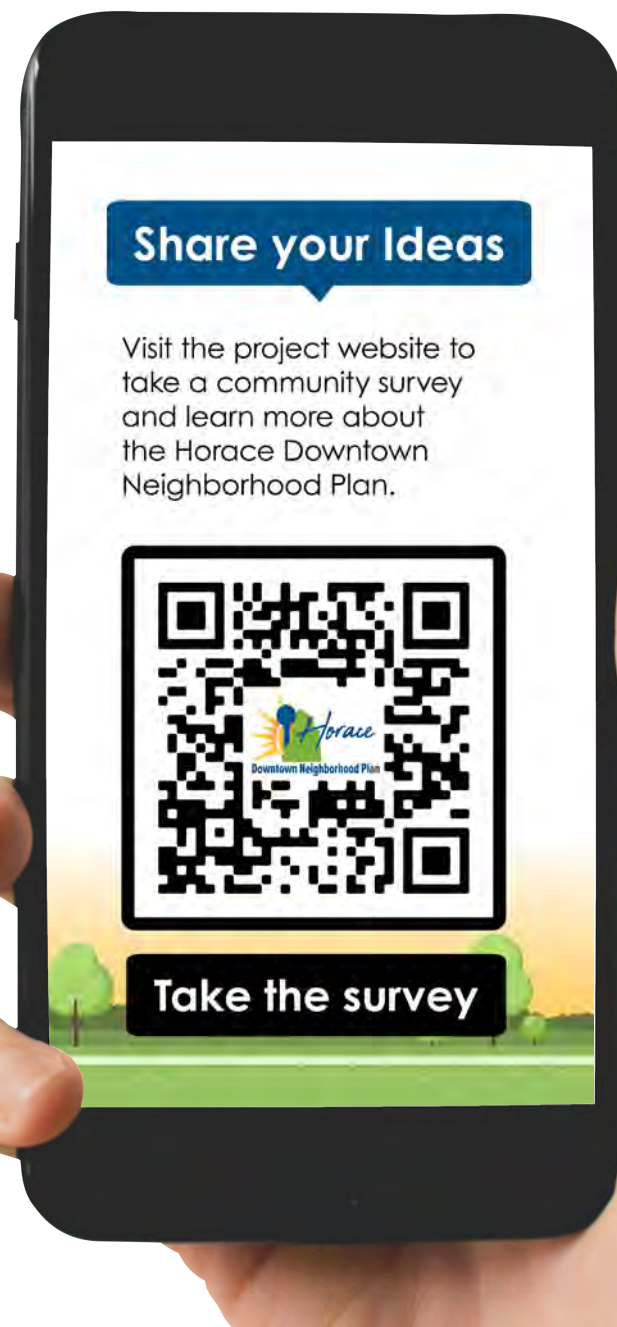
Stay Involved

Visit the project website to learn more and subscribe for project email updates.

Project Contact Info

Michael Maddox
Project Manager
maddox@fmmetrocog.org
(701)532-5104

www.fmmetrocog.org/projects-rfps/downtown-horace



METROCOG

ARGO-MOORHEAD METROPOLITAN COUNCIL OF GOVERNMENTS

Horace Downtown Neighborhood Plan



What is the Horace Downtown Neighborhood Plan?

The Horace community is growing and changing. Metro COG and the City of Horace are partnering to create a Downtown Neighborhood Plan that will provide direction to address development and transportation connections to and from Downtown and the neighborhoods surrounding it as the community grows. The goal is to develop consensus around a long-term vision for the city's core and identify implementation strategies and project priorities to further the vision.

Share your ideas!

Visit the project website to take a community survey and learn more about the Horace Downtown Neighborhood Plan.



SCAN ME TO TAKE
THE SURVEY



www.fmmetrocog.org/projects-rfps/downtown-horace

Stay Involved:

Visit the project team at Bean Days on Saturday, Sept. 7th to learn more and share your ideas!

Throughout the process, we will be engaging with residents, businesses, and visitors.



www.fmmetrocog.org/projects-rfps/downtown-horace

SRF Consulting Group Inc
1 North Second Street
Case Plaza Suite 102
Fargo, ND 58102

Scan the QR code to subscribe for project email updates.

HORACE BEAN DAYS 2024 EVENT SUMMARY

When: Friday, September 6th and Saturday, September 7th, 2024.

Where: Horace, ND – Main St & Central Ave intersection (Vendor Fair), Meadowlark Park (Kids' Fest).

Why: Public & Stakeholder involvement is a critical component of the Horace Neighborhood Downtown Plan and will be used to guide plan strategies and recommendations.

Participants: 300+

Figure 1. Bean Days Parade



EVENT BACKGROUND

Bean Days is an annual community celebration event that gives Horace residents and visitors an opportunity to celebrate the history, growth and future of Horace.

Bean Days 2024 included activities such as the Horace Lions Pancake Breakfast, 5k and Fun Run, a parade, a street dance, BINGO, fireworks, a car show, vendor fair, Kids' Fest, a dog show, food and beverage vendors, and a street dance with a featured musician.

HORACE DOWNTOWN NEIGHBORHOOD ENGAGEMENT ACTIVITIES

PARADE

The Bean Days Parade took place on Saturday, September 7th, at 10:00 am. The Horace Downtown Neighborhood Plan (Plan or Project) was represented by a float comprised of a construction truck and staff who handed out Project flyers with community survey and Plan webpage QR-codes and candy distribution for kids.

Figure 2. Parade Float



POP UP BOOTHS

Two pop-up engagement booths were set up to engage residents throughout the event.

1. Vendor Fair at the intersection of Main Street and Central Avenue (in front of grain elevators near Big Erv's).
2. Meadowlark Park on Lost River Rd from 11:00 am until 5:00 pm on Saturday, September 7th.

The booths were set up to make it easier for participants to participate in multiple ways. This allowed for thorough discussions on the Plan, collection of written comments, and fun engagement activities for adults and youth.

Figure 3. Meadowlark Park Booth



Figure 4. Vendor Fair Booth



FEEDBACK RECEIVED

DOWNTOWN CONCEPT ACTIVITY: SPILL THE BEANS

An estimated 211 people participated.

Participants were asked to choose the downtown image(s) components that best fit their future vision of Horace by placing their beans into the jar(s) below their preferred images. Each respondent was given 10 beans, which they could distribute any way they wanted.

Table 1. Spill the Beans – Results

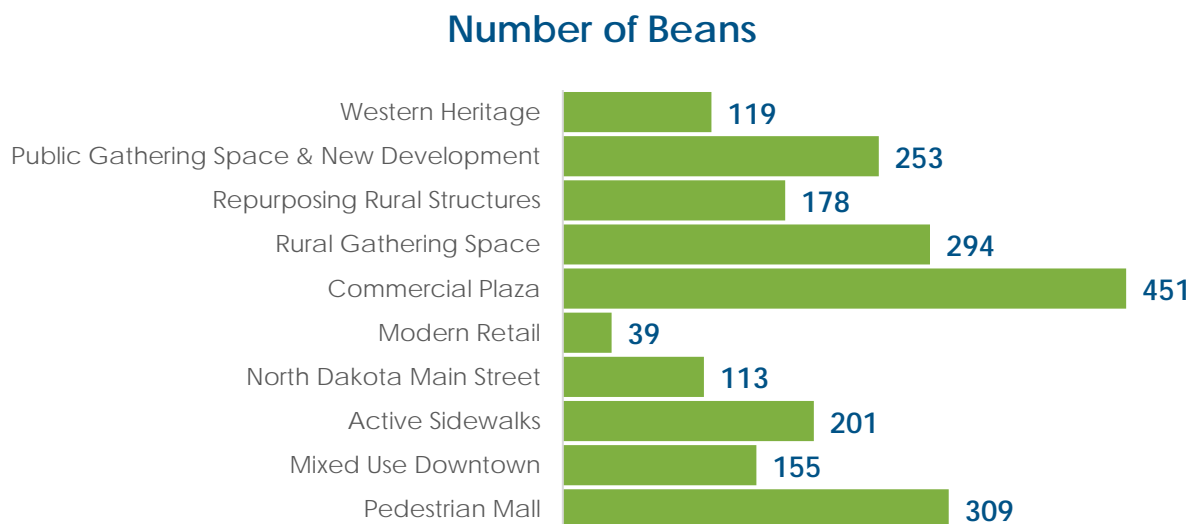


Figure 5. Spill the Beans Activity Setup



People strongly favored concepts that promote social interaction such as commercial plazas, pedestrian malls, and gathering spaces.

There is notable interest in preserving rural character and heritage, though these concepts are less prioritized compared to commercial and public spaces.

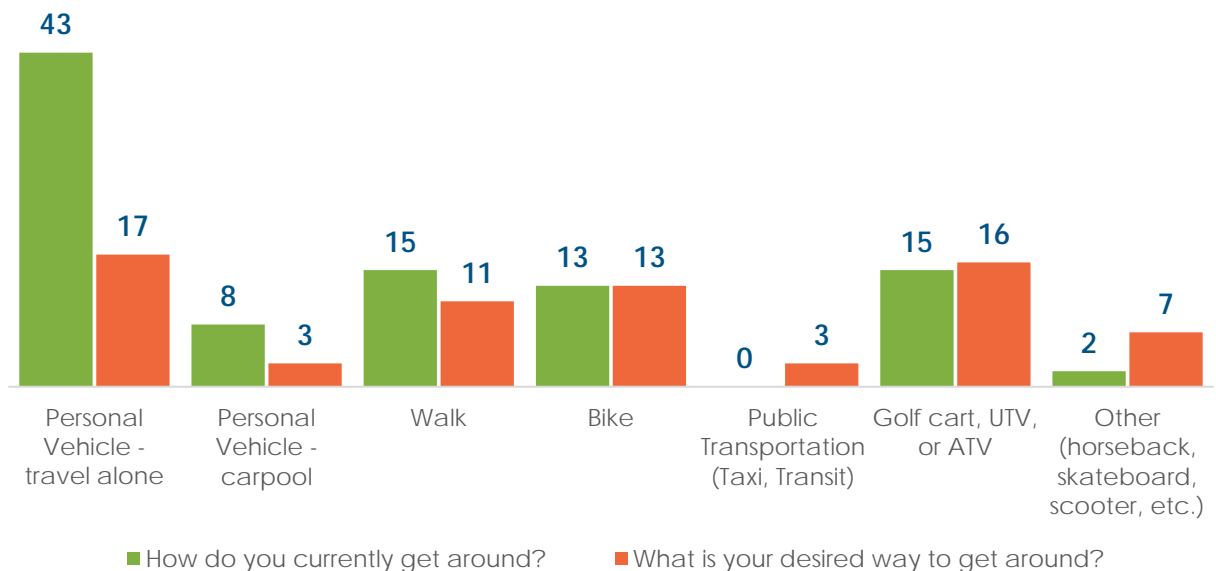
TRANSPORTATION AND MOBILITY ACTIVITY

An estimated 83 people participated.

Participants were asked to put a sticker on the current way(s) they move around Horace and way(s) they would prefer to travel in Horace. Each respondent was given stickers to indicate current and desired modes on a large map of the community.

Table 2. Transportation Preferences - Results

Transportation Preferences



Additional Comments

If people's desired way to get around was different than their current way of getting around Horace, they were asked: **What is keeping you from using your desired way to get around?** Themes of comments are summarized below.

Sidewalk and Pathway Improvements. The need for more sidewalks and walking paths was identified, especially on the south side of town, and along Main Street and Lakeview. The lack of sidewalks makes it difficult for pedestrians and those using bicycles and electric scooters to get around safely.

Distance Concerns. Some respondents mentioned that destinations are too far away from existing pathways, making it difficult to use preferred methods of travel.

Traffic and Safety Concerns. Several residents pointed out that drivers do not stop at signs, particularly near the elementary school crossing of County Road 17.

[illegible]

PRESENT, PAST AND FUTURE OF HORACE MAD-LIB ACTIVITY

For this activity, the participants filled in a single unique word or phrase for each blank to tell a story of Horace, also known as Mad-Lib.

Figure 7. Mad-Lib Activity



Current places residents enjoy visiting. Popular locations include Dairy Queen, Meadowlark Park, Big Erv's, The Headquarters, Casey's. Additional destinations include a park, grocery store, ice cream shop, and library.

Favorite ways to travel. Preferred methods include scooters, golf carts, cars, bikes, and walking.

Wishes/ideas for downtown. This section includes a dog park, clothing store, grocery store, petting zoo, new buildings, gardens, and bike pathways.



MAPS: PLACE OF LIVING, DOWNTOWN, THIRD PLACE

An estimated 101 people participated.

- 89 where you live dots
- 89 where Downtown is dots
- 125 other places or third-place dots

During the map activity the participants used color-coded stickers to identify where they live (orange stickers), where they think the heart of downtown is (blue stickers), and another place they often go to in Horace.

Figure 9. Bean Days Pop-Up Booth



Figure 10. Map Engagement - Vendor Fair

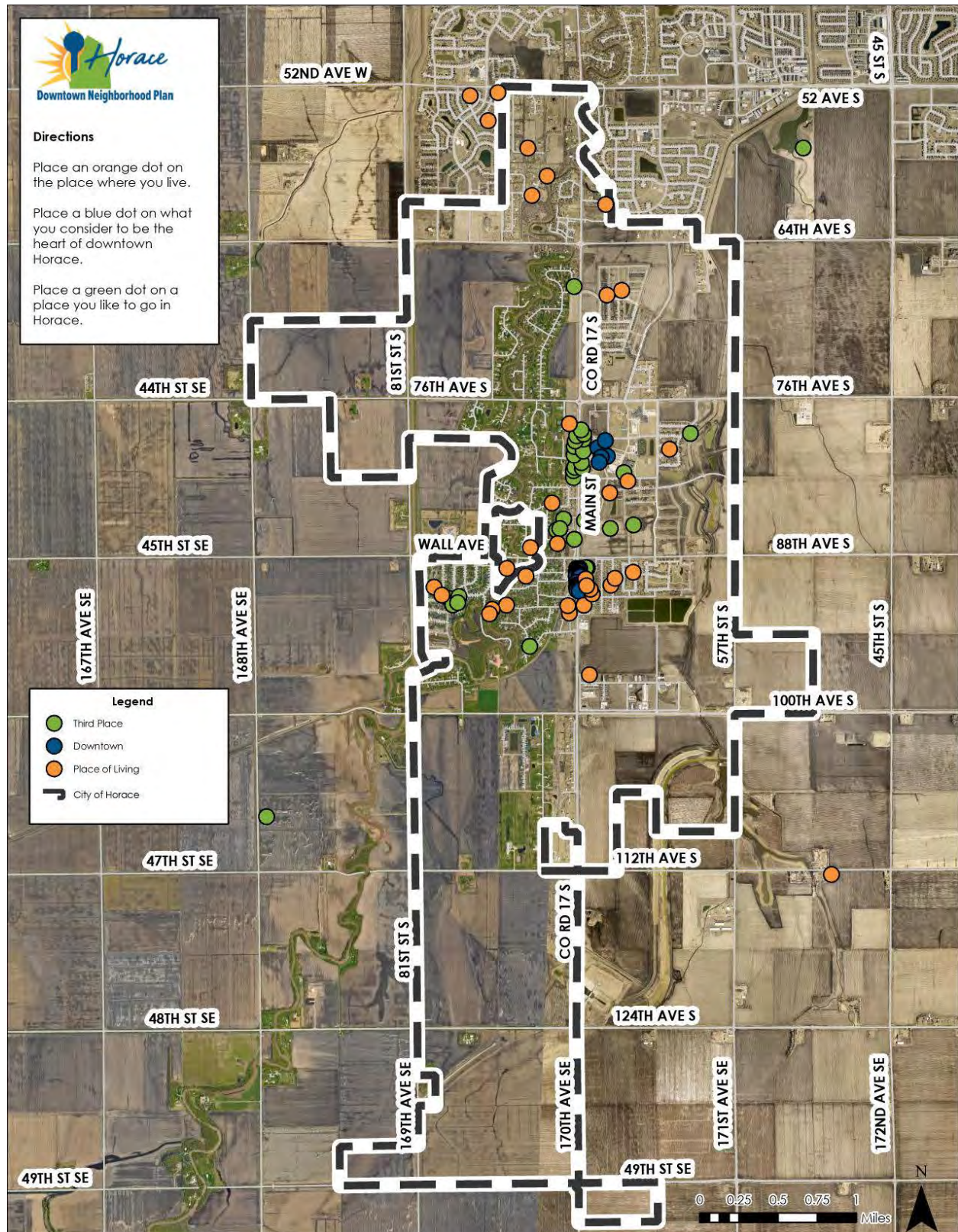


Figure 11. Map Engagement - Meadowlark Park

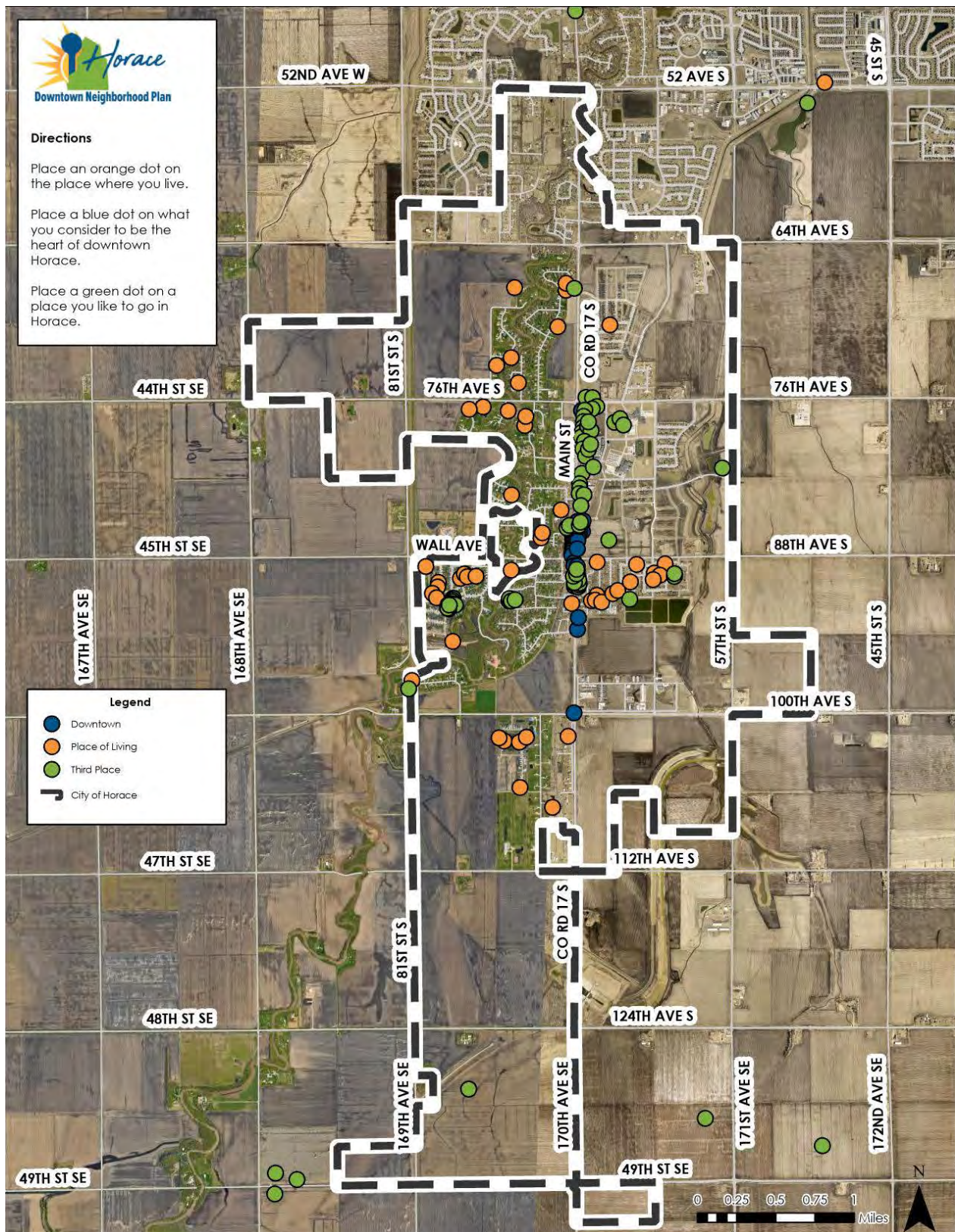


Figure 12. Downtown Core According to Participants - Heatmap

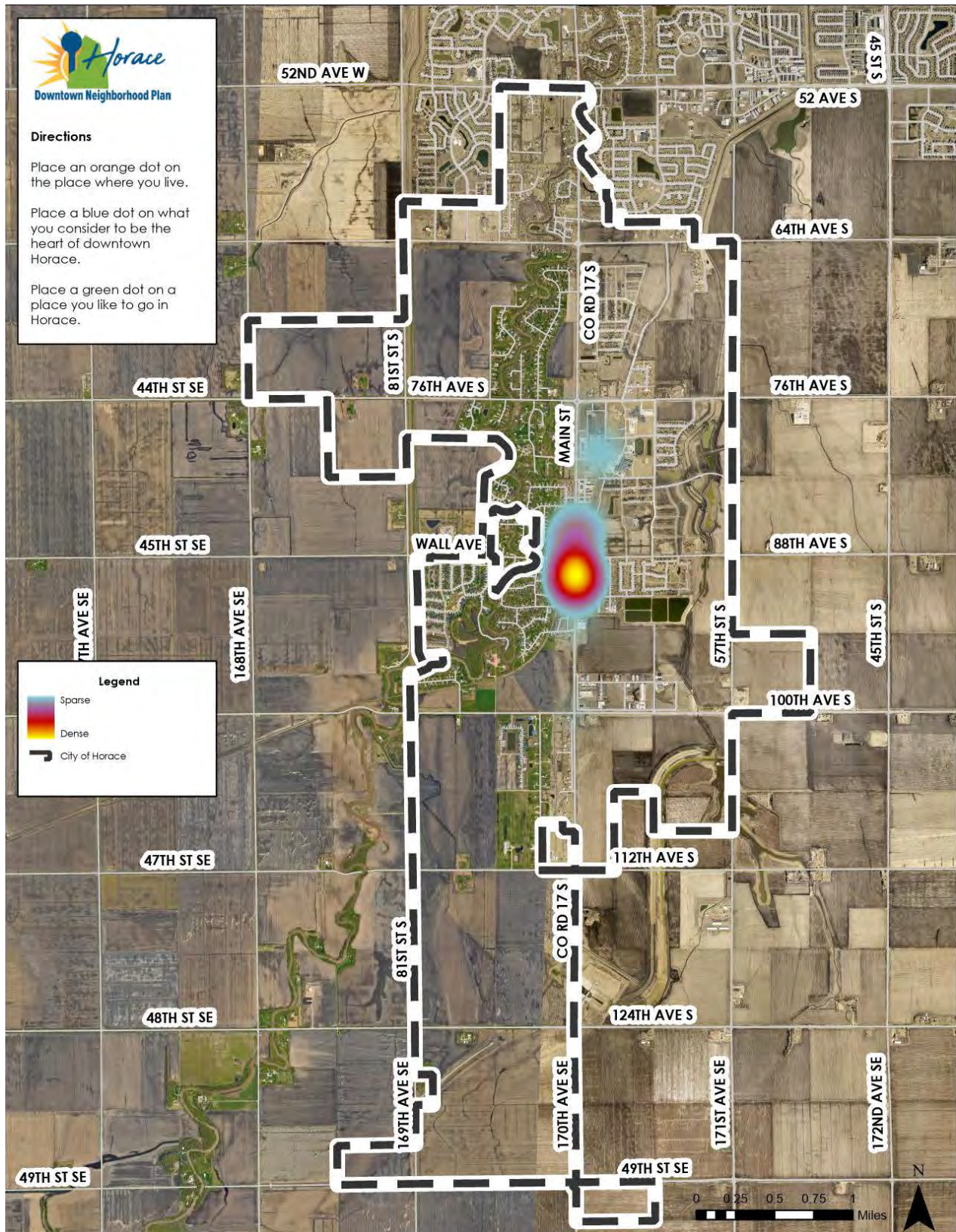


Figure 13. Places People Like to Visit - Heatmap

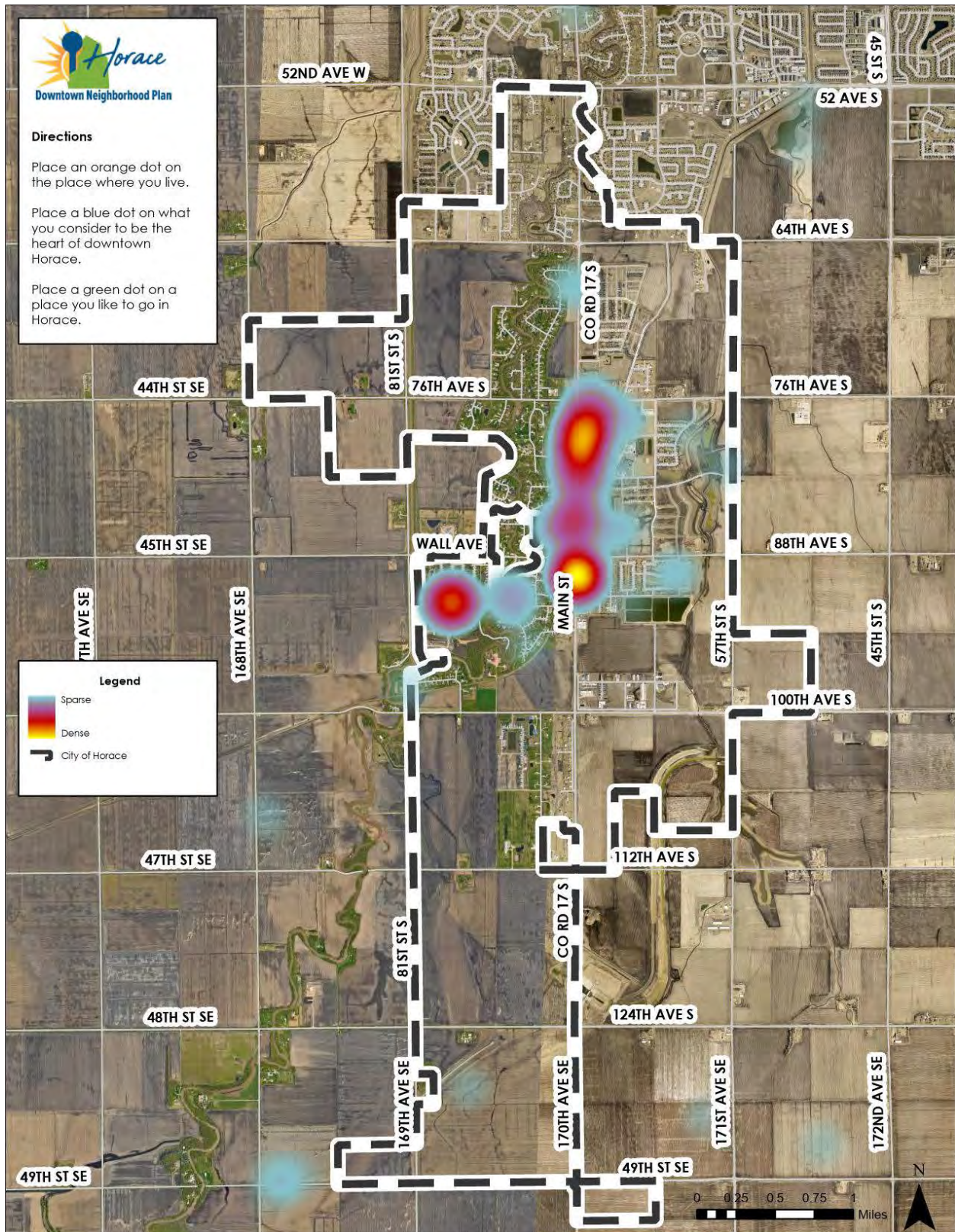
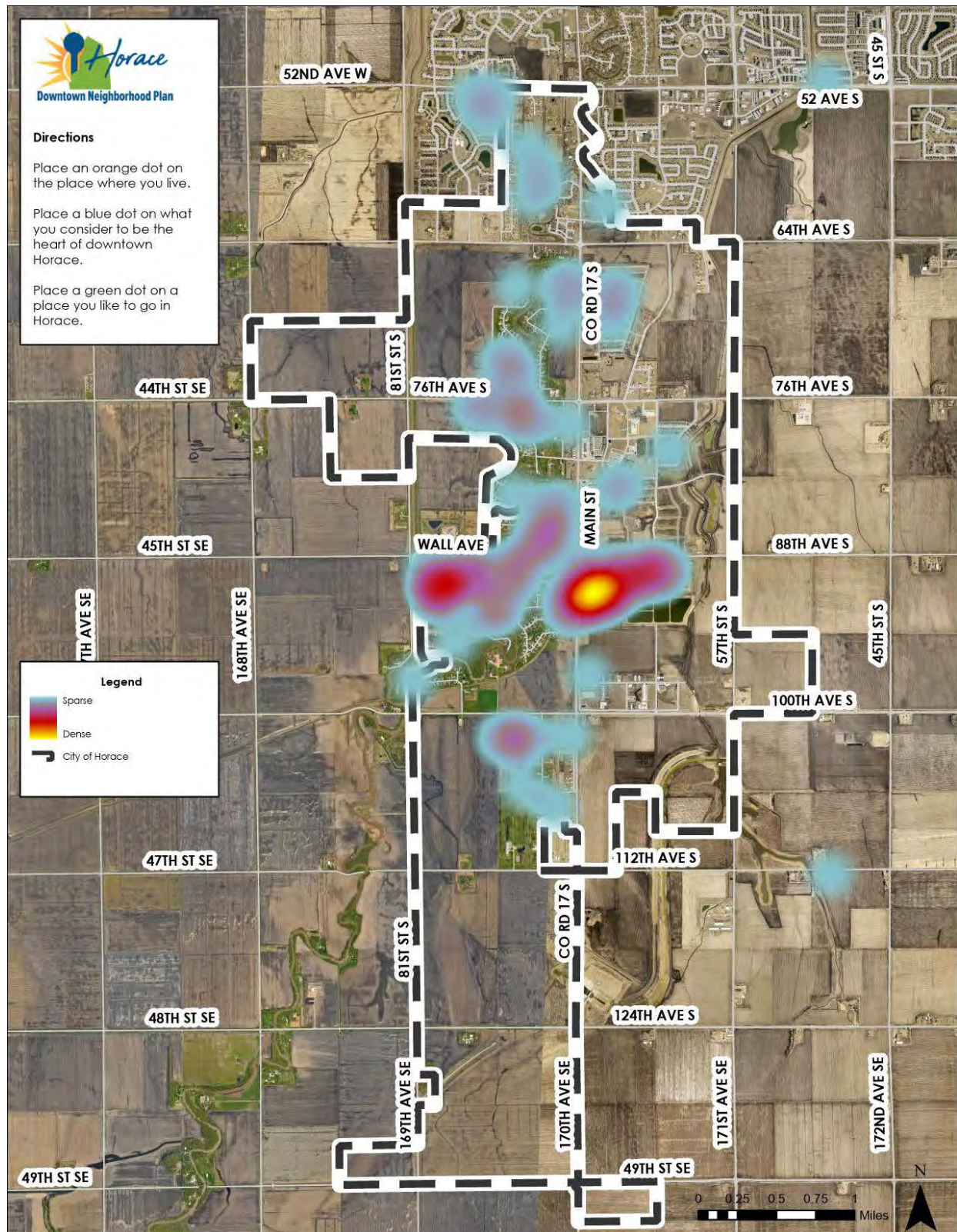


Figure 14. Where People Live - Heatmap



What words come to mind when you think about Horace as a community?



Response	Count
Small town	16
Small town feel	8
Small town.	5
Growing	4
Small	3
Friendly	3
Overgrown.	2
Home	2
Growing too fast	2
younger generation, lots of children, growing community	1

Wholesome, family-oriented, safe	1
When I moved here 3 years ago I thought of it as quiet calm neighborhood. Small town feel. Now it's growing too fast.	1
When I first moved I thought small town but it's feeling small town wanting to be big despite the residents concerns.	1
Welcoming	1
We are growing so fast infrastructure is falling behind. Great people. Fun. Small town with big city access.	1
very disjointed.	1
Used to be a small bedroom community but now is a big bedroom community.	1
Up and coming	1
Unsustainable growth. Developer greed. Loss of small town identity. Unaffordable housing. Gentrification. Loss of community identity.	1
Unorganized , expensive	1
Unorganized	1
Town in the grow	1
Too many rich, rude, people moving in.	1
Too many people shoved into one spot	1
Thriving young community that battles with older community	1
The old and the new	1
The lovely trailer houses	1
The little town that wants to become a city. New members of the community want stuff, convenience and don't take pride in the simpler way of life.	1
The city that makes it hard for meaningful businesses to come to Horace.	1
The city council appears to be more interested in new growth & developments than respecting the wishes of 20+ year established residents.	1

The "leaders" are trying way too hard to make it just like Fargo and West Fargo. They are ruining the community of Horace. People should just stay in Fargo or West Fargo if they want all those amenities!!!	1
That is gotten too big way to fast!! The schools are over-crowded. Main Street needs some stop signs cause it's very hard to turn out of the school onto main. It's not safe for kids crossing the streets to get to school. With bus routes being cut there's more kids walking.	1
Taxes	1
Swinger Central	1
Suburb of Fargo	1
Spread out. Continuation of West Fargo.	1
Small-town, quiet, bedroom community	1
Smaller than Fargo/WF	1
Smaller connected community.	1
Small/smaller town feeling. Peaceful. Neighborly/respectful. Clean and cared for properties.	1
Small. Quiet. Relaxing	1
Small. Hopefully stays small.	1
Small, simple, residential not business focused	1
Small, rural town feeling.	1
Small, quiet, rural	1
Small, Quiet, Peaceful, Uneventful	1
Small, quiet, friendly, tight knit	1
Small, quiet town	1
Small, low key community, detached from the fm area chaos	1
Small, friendly, quite	1
Small, friendly, family, heritage, elevators,	1
Small, family friendly, farming	1

Small, cozy, rural, safe, farming.	1
Small, close-knit, slow pace, family friendly	1
Small, charming, quiet	1
Small, family oriented, and peaceful community	1
Small Quiet "Country living"	1
Small town. Safe. Neighborly. Growing faster than infrastructure seems able to accommodate. Growing tax expenses to current residents to bring more residents in.	1
Small town. Rural. Farming.	1
Small town. New schools	1
Small town. Low crime. People that work and have money. Super clean. Good minded people that care about the city they live in.	1
Small town. Keep the small town vibe. Growing.	1
Small town. Great sense of community.	1
Small town. Friendly. Low crime. ATV friendly.	1
Small town. Friendly. Cozy.	1
Small town. Quiet. Invaded. Expensive.	1
Small town, with the essential amenities needed close by.	1
Small town, very isolated/spread out.	1
Small Town, Safe, Family Friendly, Law Enforcement Presence	1
Small town, safe, community.	1
Small town, rural,	1
Small town, roaming the streets, community fires, bean bag tournaments, off road vehicles	1
Small town, relaxing easy living. Rural, friendly.	1
Small town, quiet, serene, safe	1

Small town, quiet, outside the busy part of Fargo and growing quickly.	1
Small town, quiet, growing quickly	1
Small town, quiet, friendly, rural, relaxing,	1
Small town, quiet, country feel	1
Small town, peacefully living	1
Small town, peaceful how America should be.	1
Small town, peaceful	1
small town, nice houses, an escape for upper middle class	1
Small town, neighborly, cozy, quiet	1
Small town, Main Street, local, safe, walkable	1
Small town, luxury, quiet, family friendly, spacious, nature, close to downtown and open natural areas.	1
Small town, local, safe, quiet. Bedroom community.	1
Small town, home, community, family, friendly	1
Small town, good community	1
Small town, country, trees	1
Small town, close community	1
Small town Local Farming community	1
Small town Growing Horace Schools	1
Small town Friendly Quiet Rural	1
Small town Friendly	1
Small town with large potential.	1
Small town with big aspirations. Embracing it's big suburb future. Conflict between those that resent i t and those that embrace it.	1
Small town vibe, quiet and comfortable town. The MOST expensive city in ND with taxes	1

Small town vibe,	1
Small town trying to be a big city	1
Small town that's growing fast	1
Small town that isn't afraid to be modern.	1
Small town that is not a small town anymore it's a suburb.	1
Small town that is growing so fast that the city can't keep up with it.	1
Small town that has turned into a south west fargo	1
Small town living Quiet Community based	1
Small town living close to the city	1
Small town living	1
Small town ideology with selective modern conveniences.	1
Small town growing city too fast	1
Small town feeling.	1
Small town feeling Friendly neighbors Clean	1
Small town feel.	1
Small town feel, growing too fast	1
Small town feel, friendly, safe, quiet	1
Small town feel, family community	1
Small town feel Family friendly Quiet	1
Small town feel	1
Small town feel that growing fast and is located perfectly near Fargo/West Fargo's rapidly growing south end.	1
Small town feel close to the big city.	1

Small town everyone takes care of their neighbors and makes sure they are okay! Check on everyone! People who are moving here have never had that experience of loving thy neighbor. How do we get back to that?	1
Small town environment and values.	1
Small town country living	1
small town community, quiet, you take care of your neighbors	1
Small town community	1
Small town close community	1
Small town charm Neighborly	1
Small town but convenient location.	1
Small town vibe	1
Small town atmosphere, quiet	1
Small town atmosphere	1
Small town appeal	1
Small town and safety.	1
Small Town America	1
Small town Clean Friendly	1
Small quaint town.	1
Small intimate community.	1
Small friendly town. Fast growing community.	1
Small friendly place to live.	1
Small community	1
Small but safe	1
Small and nice people/community	1

Small Remote Quiet Safe	1
Slow down!	1
Should have been kept a small town - city leaders ruined the town with all the new homes.	1
Scenic, quiet, peaceful	1
Safer than Fargo, West Fargo area	1
Safe, small, friendly, family	1
Safe, small town at its best	1
Safe, small & comfortable.	1
Safe, quiet, generational, connections, relationships, wildlife, country, farm,	1
Safe, friendly, convenient, fun	1
Safe, family friendly, up and coming	1
Safe, family friendly, small town feel, community, luxury, outdoor space, refined, parks, cozy	1
Safe, people know each other	1
Safe town to live away from the woes of fargo.	1
safe	1
Sadness and loss. I moved to Horace, as I was drawn to the small town community. Sadly, I didn't research the future enough, as I did not know that would be stripped away in less than a decade.	1
rural. booming into a larger cookie cutter city, without any city conveniences. Horrible parking in the developments.	1
Rural, small town, tight knit	1
Rural, small town feel	1
Rural, safe, friendly, relaxed.	1
Rural, home, small	1
Rural, growing pains, low income style developments	1

rural, bedroom community	1
Rural feel with big town amenities Safe Growing Great city employees Welcoming	1
Rural	1
Redneck trailer park	1
Rapid growth, losing small town feel	1
Quite community growing out of control.	1
Quiet, trees, plants	1
Quiet, small, trees, beautiful.	1
Quiet, small town, know my neighbors, quality services	1
Quiet, small town, freedom, cliques	1
Quiet, simple, & safe	1
Quiet, safe, small town	1
Quiet, peaceful.	1
Quiet, peaceful and not Fargo or West Fargo	1
Quiet, outskirt living, with good people!	1
Quiet, Nice, Clean	1
quiet, growing, safe, family friendly	1
Quiet, country, clean, warm	1
Quiet, clean community	1
Quiet Easy access to Fargo Family oriented.	1
Quiet small town.	1
Quiet small town feel	1
Quiet small suburb of Fargo.	1

Quiet semi rural small town	1
Quiet peaceful	1
Quiet and quaint. Sleepy town. Great community. Quiet. Quiet quiet. Beautiful.	1
Quiet and enough businesses to keep a small town feel but not overwhelming with industrial, commercial, retail space, etc. Horace is getting the basic essentials it needs - a small clinic, dentist, coffee shop, some restaurants, daycares, schools, gas stations - all it is missing is a grocery store. Feel of small town living while getting the benefit of living close to the big city.	1
Quickly growing	1
Quaint, friendly neighbors,	1
Quaint but growing rapidly	1
Quaint and cozy town.	1
Poor leadership and awful future planning. No expense knowledge and zero count ability for spending.	1
Pool Recreation center Sports fields Recreational activities for families	1
Peaceful, serene and private.	1
Peaceful, rural, freindsly	1
Peaceful	1
Peace and quiet, small town feel, safe, beauty.	1
Park, swimming pool, maybe build grocery store or better convenience stores	1
Overcrowded	1
Over taxed	1
Over crowded with a city council on the take to the highest bidder	1
Out of touch	1
Older community charm with large oversized yards, mature trees, and people that care about where they live.	1

Not what it used to be.	1
NOT THE SAME FEELING AS WHEN I GREW UP THERE.	1
Not small town anymore.	1
Not progressive	1
Not fargo/west fargo	1
No Retail	1
Nice, safe, wholesome	1
Nice small community and quiet.	1
Nice people, cute town	1
Nice clean city. Peaceful, small, not like Fargo	1
Nice and quieter slower pace than fargo	1
Nice	1
Neighbors that look out for each other.	1
My neighborhood is aweso.e neighbors are outside most of the time and are very friendly	1
Money, construction, small town	1
modern but true as time.	1
Lot's of whining	1
Lost hometown spirit	1
Losing that small town feeling	1
Lets go back to 2007 when we dont have to ask this...can we ride?? We had freedom to ride ATV, 4 wheelers, mini bikes, golf carts and dirt bikes. I see track houses in hundreds of houses coming into Horace from North Sheyenne and its not My Horace!! Its something that the people who have lived here for 20 plus years moved here for a small town feel!! residents who moved here didnt want to live like this is in a small town... we dont need a downtown we need a main street!!	1
Keep it small	1

its trying to be a big city but crowding all the houses in and looking like just a nunch of little houses to close together.	1
It's gotten too big.	1
It's an extension of West Fargo. I've heard some people call it "West Fargo Lite".	1
It WAS small town..... It WAS quiet.	1
It was a nice small community	1
It used to be small-town feel. And having lived here over 20 years, we LOVED it. Unfortunately, that small-town feel has quickly deteriorated. Even my kids, 23 and 17, who have lived in Horace their whole lives are sad about it. Progress has to happen, but it didn't have to be too much too soon like it is.	1
It used to be small town however, now it seems to be construction! I do love the city that I live in, it's just getting really big.	1
It used to be a wonderful small town where you knew most everyone in town. That's no longer the case.	1
It used to be a small town with farm land and now there are too many houses that are built too close together and all look the same.	1
It has become a living nightmare. Having to watch the city council get bought off to let developers destroy the town.	1
Inviting	1
I used to think of it as a small town that was quiet. Neighbors you didn't know would wave at you. Now...not so much.	1
I used to think of it a small, friendly and safe. Away from traffic and chaos. Now I don't know what it is, sadly.	1
I use to think of small town living, now I think of it as a poser a town and council trying it's hardest to be like Fargo.	1
I love that it doesn't feel as compact as other neighborhoods. Feels like we have more privacy.	1
I like living in a small town. People that don't live here should have absolutely no say in what happens here. Why are you trying to make this an extension of Fargo? If you haven't noticed Fargo is becoming a dump. They just keep throwing up apartments and cheap over priced housing driving up the cost of everything around us.	1

How dumb they don't have a gym	1
Horace is a bedroom community. We love our space, privacy and hometown charm.	1
Horace is a beautiful bedroom community just south of West Fargo. Accessible and affluent.	1
Horace has a small town feel while still being close to a bigger city.	1
Home, change, growth.	1
Helping each other when people need it.	1
Growth. Perfect mix of rural and urban.	1
Growth too fast, not the small community I moved here for in 2015. Took away the fast road to Fargo. That's what the big draw to this town was. Small town yet close to town.	1
Growth (maybe too fast) but controlled growth is great.	1
Growth	1
Growing, small town mentality but we're becoming a bigger city, neighborly.	1
Growing, school children, friendly. Home.	1
Growing, expanding, community	1
Growing, community	1
Growing Small town Bedroom community	1
Growing Neighborhood Families Small-town turned suburb	1
Growing Community-focused	1
Growing way too fast!! Do not have services in place to accommodate this growth. Take time to get feedback from existing neighbors before diving into a new project please.	1
Growing way too fast and at a density that detracts from the reason we moved to Horace	1
Growing too fast. Not enough infrastructure, give us a dang grocery store!	1
Growing too fast. Has lost its small town feel.	1
Growing too fast.	1

Growing too fast at the expense and contrary to long term residents. Dumpy trailer park in the middle of town.	1
Growing too fast and not even a grocery store nearby.	1
Growing to fast!!!!	1
Growing to fast! Too populated. Loosing our small town atmosphere. Over extended in specials. Too much traffic. Crime is growing faster than population. Construction sounds constantly, even weekends and holidays.	1
Growing small town that is family friendly.	1
Growing small town	1
Growing really fast!!	1
Growing quickly	1
Growing pains,	1
Growing pains and managing it. Too many small lots and dense population priorities of developers. No character, charm, or coziness In them. Just rows of houses and retention ponds.	1
Growing pains	1
Growing faster than infrastructure can support. Poorly planned development. City officials have no backbone. Corrupt.	1
Growing community, great neighbors and good places to walk with the public sidewalks in our neighborhood.	1
Growing community but very welcoming	1
Growing but not quite quick enough	1
Growing and vibrant	1
Growing and small town feel	1
Growing , welcoming, community, good schools	1
Grocery store, food options, park and pool, more then two snow plows	1
Great school district Quite community	1

Great place to live.should try to keep the small town vib!	1
Great community	1
Good schools, growing	1
Good schools and parks, warm community welcome	1
Good old boys club. Slow to change. Anti-business.	1
Good but changing.	1
Getting too big - No more small rural town feel	1
Getting lots of foreigners in this community because of hand outs to get homes. Need sidewalks to more parks to bring the community together. Grocery and hardware store would be nice.	1
Getting greedy	1
Generous, Family-oriented, Peaceful, Safe, Caring, Hawk Pride	1
Full of potential.	1
Friendship	1
Friendly, welcoming, small town, rural, open spaces	1
Friendly, rural atmosphere and lifestyles	1
Friendly, fun, entertaining. Provides something for everyone and a great community that comes together often.	1
Friendly tight-knit neighborhood.	1
Friendly faces but getting busy	1
Foliage. Whatever you do, it needs to keep the "out of town" feel.	1
Fasting growing city in North Dakota. That's why I moved here.	1
Fast moving Growing quickly Young	1
Fast growing, suburb, small town feel	1
Fast growing New community	1

Fargo escape	1
Family, welcoming, growth, and offer options for the community.	1
Family, Safe, Rural	1
Family, growth,	1
Family oriented, safe, community	1
Family Oriented community with small rural town feel.	1
Family oriented and safe.	1
Family friendly	1
Extension of Fargo suburbs	1
Explosive uncontrolled growth. Growth with inadequate planning as seen from the resident's perspective.	1
Explosive growth	1
Expensive. Short staffed.	1
Expensive, special assessments, overtaxed, no stores, houses on top of houses	1
Expensive, friendly people, and good schools	1
Expensive housing and nothing to do in town.	1
Exciting, growing, energy	1
Established and family oriented.	1
Dog shit	1
Division of community. Us vs Them	1
Division	1
Divided Over taxed	1
Divided	1

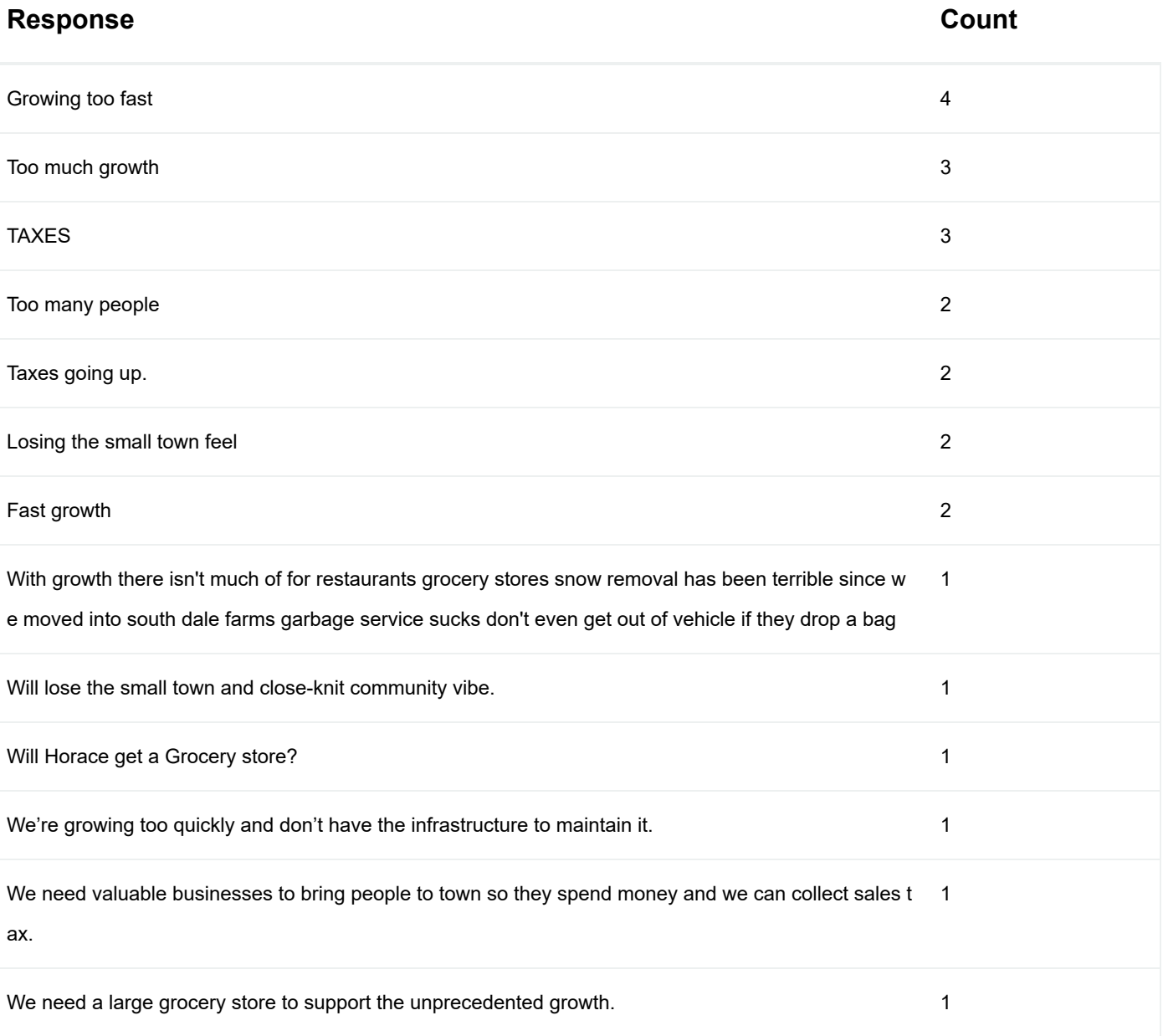
disorganized with no real leadership or plan, expensive and just another metro city. the small town fee I has been destroyed in the last 7 years. people who have been here a long time can't afford to stay. the proposed specials and the ones already in place are astronomical. I can only imagine how much we are going to get hit with when they go to fix the streets that have been destroyed by all the truck traffic on them in the last few years. that should all go to the new developments, they are the reason 15 dump trucks a day go by my house.	1
Dis-connected.	1
Discombobulated	1
Dirty, run down, lack of businesses	1
Dirty, old, boring, nothing to do except drinking	1
Developer's coming first over community wants and needs. Rapid growth with little to no forethought. Big money buying their way into small town.	1
developer-ran, no concern for public opinions	1
Crackheads	1
Cozy. Country. Caring.	1
corrupt city government. isolated neighborhoods. no community events for everyone. "Old Horace vs New Horace" mentality. no commercial industry. inconsistency from city staff. Extreme specials-not livable.	1
Concern for city leaders decisions on allowing new zoning requirements on new developments. No consideration for long time residents. Look on the east side of county 17 and the city disregarding established developments on the west side and considering rezoning to allow commercial and row housing in back yards of the Westwood Woodland development that John Kautzman starting developing with 1 acre lots in 1975.	1
Community, small town feel with rapid growth	1
Community, growing, developing, evolving, small town	1
Community, good schools, small town feel with enough convinces nearby.	1
Community of citizens who care about each other, small town feel,	1
Community in the grow	1

Close, fun, happy, proud	1
Close knit, safe, small town	1
Clean and well kept up, new and modern, family oriented	1
Charming, small, opportunity for growth in a positive way	1
Charming, friendly, clean	1
Changing Money driven by developers and commission Loss of the reasons we moved here	1
Challenges with growth	1
calm, nature, community	1
Busy and cramped	1
Bursting!	1
Building to fast	1
Budding, quiet	1
Broken	1
Booming.	1
Booming, family friendly	1
Booming	1
Big lots Small town	1
Before it expanded so fast. Family friendly. Now unsafe	1
Bedroom community. Charm. Uniqueness. Quiet a place to "be."	1
Bedroom Community. Rural. Quiet. Community. Good People	1
Bedroom community, growing, simple, safe, nice	1
Bedroom community with potential for unique growth	1
Bedroom community that doesn't have a hardware store, grocery store, or the essentials.	1

Beautiful everywhere but downtown!	1
Balance of small-town charm and accessibility to Fargo/West Fargo.	1
As a resident for less than 5 years, it's imperative that we maintain mature growth of the landscape and the 'hometown' feel	1
A small farming town outside of Fargo	1
A slice of small town feeling.	1
A quiet town, growing quickly and on the cusp of figuring out its identity.	1
A mess. The city cannot control what's here, nor enforce consistent expectations and grounds for all residents and contractors. It's a free-for-all and no pride in decision making, only greed. This a pothole of money and greed, with entirely too many residents left to figure it out or leave.	1
A growing community with no basic needs commercial businesses (grocery store, hardware store, etc.)	1
A growing community with MANY opportunities in front of it. A place where residents must adapt to change and embrace new opportunities.	1
A continuation of West Fargo and Fargo and without a unique identity.	1
A community that is losing the small town feel.	1
A close community	1
A bedroom community	1
"Home" was the word that used to come to mind. Now, it's "poor leadership", "money" and "sellouts"	1

Answered: 418 Skipped: 84

What is your biggest concern for the future of Horace?



We moved to Horace to have less traffic and be more safe for my toddler to be able to play outside and not have to worry about cars, people, or unsafe environments. My concern would be how to keep it safe and have a small town feel while it's developing into a city.	1
We have an incompetent city council, who has authority to invoke large impacts on residents with and recklessly. Affordability and rural living is no longer the heart Horace had been. You can be "for the people" but when you sit silent or just wildly approve things this city doesn't need, you are for your pocket and popularity. There is no plan for the growth. It's all talk, changing plans, and it only takes one city council meeting for council members to undermine the residents and value. There's no common sense nor business mindedness in those elected. It's a popularity contest they continue to win. WE ARE NOT FARGO OR WEST FARGO!	1
We desperately need a grocery store due to the large increase of population.	1
way too much high-density housing already - we don't need more, we don't want commercial businesses, so much traffic creating longer drive time into Fargo/West Fargo	1
Urban sprawl Losing it's charm Uplift in crime (like our neighbors Fargo/Moorhead)	1
Unsustainable growth	1
Uninformed decisions	1
Under resourced for the pace of growth. Needs a grocery store.	1
Uncontrolled growth- this city focuses on growth more than it does on the existing residents	1
turning it into Fargo	1
Turning into West Fargo and Fargo!!	1
Trailer park	1
Trailer courts need to be moved away from Main Street	1
Traffic, taxes, to many low income houses	1
Traffic going in and out of town	1
Traffic control, speeding, crime as the city grows	1

Traffic and noise on 76th Ave. ESPECIALLY construction vehicles. Building so many houses on teeny lots. Growing for the sake of getting bigger - but not for the better or more beautiful, and at the expense of or contrary to current long time residents. Finally the dirt bikes and 4 wheelers need to STOP on 76th before someone gets killed (from large construction vehicles that are destroying the road)	1
Too rapid of growth without planning. Developers get whatever they want and the rest of us pay for things they should have paid for like parks. People driving way too fast around children. Especially on W all.	1
Too much, too fast. Taxes increasing by leaps and bounds, and we live on a fixed income. Also, more people, more crime.	1
Too much Riff Raff being brought in by apartments and the like	1
Too much residential development and not nearly enough commercial development.	1
Too much rapid residential growth without many local businesses to support residents basic needs. making more money leave the community for west Fargo and fargo	1
Too much light and noise pollution for residents	1
Too much housing, no planned grocery store.	1
Too much high-density housing and too many roundabouts	1
Too much growth. We need to slow down and get our small town feel back.	1
Too much growth. I don't want to be an extension of Fargo. Keep the small town feel.	1
Too much growth, infrastructure doesn't support the boom in population.	1
Too much growth too fast. Loosing the small town feel and amenities.	1
Too much growth too fast. Horace is not prepared with the infrastructure for the growth we've had already. We can't even get enough teachers for all of the classes at the schools. The schools in Horace are already getting way bigger than they needed to be with the booming housing. This is not an asset, it is something that will create issues faster within the schools and our community. And stop letting them build starter home communities where houses are 3 feet apart when they were posed as homes on bigger lots to start with and then that's not what is being built. Even people who moved here just 5 years ago are disappointed with the gigantic amount of growth. It's sad.	1
Too much growth too fast.	1

Too much growth to where it turns into an extension of West Fargo, and loses the small town atmosphere and feel.	1
Too much growth and large developers coming in to turn it into a concrete jungle. We don't need mixed use building or large complexes. Keep housing spacious and don't stack homes on top of each other.	1
Too much growth	1
Too much focus on commercial growth without concern for how that affects residents.	1
Too much expansion	1
Too much development too quickly.	1
Too many people. Too many starter homes, too many apartments, too many strip malls. Forced to have 3 acre lot when the land behind me could be turned into condensed living. Too big of schools.	1
Too many people. Roads are too slow now with all the added people. It's really not good planning at all for that.	1
Too many people, not Enough supporting stores.	1
Too many people! Too many Track housing developments driving our housing costs down if there are more \$260 grand or lower houses vs higher house values we will suffer!! Its a fact!! Even though your driving more people to come and pay taxes it doesnt work that way.. Are the apartment buildings paying high taxes like we are? Or did they get a tax break to move in to Horace?	1
Too many people and cookie cutter neighborhoods.	1
Too many houses with very small lot size. Heavy traffic. Too much truck traffic with all the diversion stuff!	1
Too many houses jam packed into tight neighborhoods! More traffic than community designed for.	1
Too many houses being built in areas where makes the town feel like a small town. It seems like every day we drive behind the catholic church, 10 new houses are being built. The peace and quiet we enjoy is about to disappear. They already built the apartments right across the high school. Not a great asset as far as a resident of the this town.	1
Too many cookie cutter homes that will look poorly in the future	1

Too many cheap, ugly cookie cutter homes that no one will take care of and will look like garbage in the near future. Also, the trailer parks in town need to be addressed - they look like the inner city of Chicago - unacceptable.	1
Too many apartments/twin homes popping up.	1
Too many apartments and/or townhomes. The community cannot sustain that type of growth in population.	1
Too commercial and too many rentals (apartments or townhomes)	1
Too big	1
Too much high density housing	1
Too much growing and growing extremely fast	1
Too fast with out the necessary planning and oversight needed to become a bigger city in the region	1
Too fast of growth sacrificing the safe feel	1
Tiny cookie cutter houses that draw in short term home owners and rentals instead of residents that are invested in the community.	1
Tight over packed neighborhoods and school class sizes	1
Those people moving in	1
There's no infrastructure and it's growing like weeds.	1
There really is no way to keep the small town feel the way we are going. Cramming too many houses into small areas.	1
There is no gym	1

There is a need for improved planning in new developments. The construction of houses that are identical and closely spaced lacks uniqueness and character. Moreover, there is a significant parking issue, as many streets in these new developments have restrictions that prohibit parking. This raises concerns about the family-friendliness of the area, particularly for families with teenage drivers. Such issues could have long-term negative effects on the community. Additionally, the close proximity of the houses limits space for landscaping and may present fire hazards. In the event of a fire, the densely packed nature of these developments could potentially endanger entire neighborhoods. Also, There is going to be a travel issue. To address the travel time issue, I recommend that city metro areas invest in an outer beltway to connect the periphery of the cities with the main highways. Additionally, I suggest adding an exit from Highway 29 at 70th Avenue to improve accessibility.

The way the city is laid out is awful and there really is not a downtown that I would spend time in if you consider Big Ervs and the trailer park downtown.

The transition from small town to a city has a lot of challenges with infrastructure and city management. People need to understand that this all costs money and I feel the developers need to start paying their fair share of this to lessen the tax burdens associated with this growth.

The set up of Horace is a mess. The set up of roads is not adjusting to the traffic sizes and we need a grocery store!

The quick growth will take away the small town feel and quiet community.

The potential for rising taxes, special assessments getting too high

The overcrowded elementary school, the never ending construction, the lack of parking everywhere.

The money it will cost to build. The noise and light pollution. The people it will attract. It needs to stay quiet.

The massive amount of houses with no amenities to justify them. And the HUGE security risk of apartment buildings overlooking the schools.

The growth. Not small town living anymore as per a resident there

The growth seems to be attracting more businesses and apartment type complexes that if built will create more traffic and lessen any privacy we currently have with apartments near our neighborhoods that when on their balconies can see into our yards. Traffic is also out of control and more roads with access to interstate will be needed.

The growth rate. What is happening to our small community so fast and west Fargo growth coming into Horace

The growth is overwhelming especially for people who were born and raised here. It's so sad that the elementary school has to send children away because they don't have any more room. This town was never meant to be this big. It's very concerning that all they want is growth, and the cannot even maintain what they already have. It's never big enough for the people that are running this town. I used to dream of raising my family here, but that is far from the truth now.

The growth and taxes pricing people out & use of taxes for things not needed. Ex: Freed Park baseball lights didn't need to be replaced they were just raised money/put up 5 years ago. Another example: the new development to the East has solar powered pedestrian lights that are wildly unnecessary. Example: the city hall buying land when it was my understanding that this was voted down by residents, they did it anyways? A lot of excess, it'd be nice to see a return on taxes if not using (another example, paying for the school when alternative schooling/private schooling/homeschooling). I will not be paying another 1.46 million for adding on to the school they just built (that is wasted space in the design). Lastly, when they took down the original street lights that were small town/orange hued (better for seeing at night) & put up those awful blue/purple LED lights. I don't know how anyone can see at night, especially during winter!

The fast growth.. need to keep everyone informed of future plans, horace happenings is a great tool. Don't rush into progress just make smart decisions.....

The fact that the land around us is proposed for a ridiculous amount of houses. Why not preserve the larger plots to make it look cohesive and not gaudy like all the over developed housing development.

The extreme growth of the row houses. Too many that are too close to each other.

The density of the neighborhoods is too high.

The cost of living. Lack of needs like a local bank and grocery store. No bike ways on 100th ave to the main drag.

The constant growth This causes construction and roads hard to get around and then they always need to be redone. We also DESPERATELY need a stop light or lights to control traffic around the schools.

The commerce side (grocery stores, shopping, etc.) won't be able to keep up with the growing population

The city is over taxing new homes with special assessments. Often times new homes have special equal to the cost of the lot. I feel they need to find an alternative and get sales tax in this town to help reduce the burden on home owners for taxes. Developers should be sharing the cost of these developments as well.

The apartments have already brought in concerning citizens and I would really like to keep the town as a whole more high end and desirable. This is also going to help bring in businesses and not just bars. Another concern is resale value since taxes are so high here. I would love to see the trailer parks removed.	1
The amount of growth and it taking away from the small town values of the town	1
The abundance of poor quality high density developments in the short term will lead to run down rental communities in the long term.	1
That you will allow more apartment complexes to be built. Keep them out of our community. They never add to communities in a positive way. Instead they bring more litter, congestion, and landlords that do not care.	1
That we will lose that "small town feel". With new apartments and bigger businesses coming in, it will start to feel like aren't living outside of the larger metro area	1
That we will lose some of the small town feel	1
That we will continue to grow and stuff money in the pockets of developers while passing on the cost to people who gain absolutely nothing by doing so. If a road needs to be fixed because it's old great. If a road needs to be fixed because truck traffic destroyed it while building a new development, the new development pays for it, period! Acting like the more we get the better off we will be is bullshit and the board only cares what affects them personally, what's best for the city is not always best for the people.	1
That we grow with decent businesses and not become too industrial	1
That they keep letting more and more new developers come in and we get too many houses to sustain the town. I hate that they're allowing rental properties and townhomes in what were thought to be single family home residential areas. Would've never bought the lot I did had I know there would be rental town homes. It brings in some good people sure, but also a lot of riff raff.	1
That the small town, close knit community will no longer exist. We'll lose the "perks" of 4 wheelers and horses in town.	1
That the people that make decisions are obviously in cahoots with developers because they are wrecking the appeal of this town. Not keeping this a small town community is my biggest fear. To many people are trying to make money out here on developing everything they can. It's gross!!! As soon as this happens, then comes all the micro managing, more crime and less freedoms that a small town offers.	1
That the lots remake larger & not to grow too fast. Continue to be a friendly family community.	1

That the growth is controlled by developer not worried about long term organization and function. That for a buck now they build too much of what may be slums later	1
That the city will continue to grow and rules/laws will continue to get broken or ignored leading to a more dangerous town. (Example: little children operating motorized vehicles on public streets and grown adults driving recklessly in residential areas)	1
That the city structure (gov't, employees) can't keep up with the growth. Many examples of poor communication and city upkeep that will continue to be a problem as long as the population continues to grow.	1
That the city retains its rural feeling but also welcomes needed growth--finding the right balance between small town and community that sustains its residents.	1
that the city is getting to busy with many little houses and not the nice big acre lots people moved here for in the first place,its starting to be nothing but cookie cutter houses.	1
That people are trying to turn it into Fargo, Fargo can keep their shit with them and not come here	1
That our city leaders want the wrong kind of growth.	1
That it's going to get to big and be just like west fargo or fargo	1
That it will turn into Fargo. Too big and too busy. Apartment buildings, overcrowded, no small town feel, crime, etc.	1
That it will turn into another West Fargo, growing too large too fast	1
That it will lose the small town feel with all the new convenience store, grocery store possibly being added.	1
That it will blow up and lose sight of the small town feel	1
That it will be full of the same things (same houses, same shops, same look as other massive neighborhoods ringed in businesses) It will just become a place where new money centered people "know what's best" for the community.	1
That it maintains its small town vibe	1
That it is losing its small knit community	1
That it is going to get too big, crime rate will rise, and that it will no longer have the small town vibe that we want.	1

That it has lost all of the reasons we moved out here over 16 years ago.	1
That it grows faster than the current infrastructure can sustain.	1
That it becomes swallowed up by West Fargo.	1
That it doesn't get so big that it is not fun to live in Horace.	1
That I won't be able to afford to live here.	1
That Horace will become too big and we'll have to move further out of town. And further building of apartments and townhomes	1
That Horace turns into Fargo / West Fargo.	1
That Horace might grow as a dependent bedroom community of Fargo, instead of investing in its unique identity and fair share of businesses.	1
Taxes, safety, crime,	1
Taxes! Specials!	1
Taxes going up and forcing long term residents out.	1
Taxes and specials. The city does a great job of spending the home owners money. We have lived here for 4 years and we have seen our taxes sky rocket and we didn't have specials and now are facing a special assessment that is beyond comprehensible. The taxes and specials will start pushing people out and we are one of them that are looking to move out within the next two years	1
taxes and specials	1
Sustainable growth and infrastructure	1
Strip malls, strip malls, and more strip malls	1
strip mall	1
specials that cause foreclosure on residence and high priced homes. it is not "starter" home quality or quantity the average starter home should not be over \$350K. too many residential homes and not enough commercial. City crashing because of incompetent administrator. trying to keep "small town feel" while expanding and causing more issues than anyone can handle.	1
specials and taxes on our home of over 30 years	1
Special assessments	1

Smart growth, providing services and recreation opportunities for Horace residents in Horace (not having to go to Fargo or West Fargo for shopping, dining, recreation etc.)	1
Smart and thoughtful development.	1
Small lots Traffic Child safety	1
Slow in getting infrastructure in place prior to all the new house developments.	1
Shopping and grocery accessibility.	1
Selling out to greedy investors who want to overpopulate and turn it into Fargo 2.0. No rental properties/multi-family dwellings. Every new build I see seems like houses are crammed together and built cheap to maximize profits. How about add a grocery store instead of another gas station or residential development?	1
Seeing too much dense housing and too much commercial. Traffic getting too busy and loud. Also not wanting specials or an increase in property taxes!	1
Safety. There are way to many motorcycles and drag racing going through town already. Between Wall, Front and 63rd if there aren't vehicles speeding through their is empty alcohol containers littered throughout. It's also scary how dangerous wall is for kids getting to school. The road is so narrow as it is, vehicles speeding, construction and the blinding sunrise is asking for an accident. Sidewalks should be critical on wall. Not to mention the ditches are awful. It's asking for another flood with all of the trees and debris in the ditch.	1
Safety. People speed all the time and it's only a matter of time before some kid gets hit. I never see any patrolling officers in the neighborhood.	1
Safety, quality public areas	1
Safety, concern that as the town grows crime rates will increase. It won't feel quiet and small as it does now.	1
Safety of our community. I know smaller towns that have police and yet we are good size and only growing yet none here. And no street stop light by the school	1
Rural, quaint, unsure	1
Road safety	1
Rising cost of living, too much growth out instead of up	1
Responsible growth without losing the Horace charm	1

Residents in the older parts of town being overruled and ignored in favor of big business and newer wealthier residents.	1
Rapid growth that we aren't ready to support	1
Rapid growth outpacing infrastructure with the cost borne by long time homeowners due to the tax abatement for new construction currently in place.	1
Rapid growth with little to no amenities. Infrastructure not keeping up with current demand i.e. terrible water pressure in lost river due to lack of planning and total residence. Schools are being filled with more Horace students.	1
Rapid expansion that the city is not equipped for.	1
Putting commercial property and apartments in established neighborhoods. Also there are too many small lots with cookie cutter homes on them. The city is trying to allow them into neighborhoods that have custom built homes that been here for 30 plus years.	1
Pushing the core area family's out of town with the additional costs of living in Horace now.	1
Property tax expense to support growth and what's going up in my neighborhood.	1
properties along county 17 will be forced to sell	1
Proper zoning for proper growth. To not put special interests first. When I went to the city office to inquire of the developing on the east side of county 17. The response was "we need to provide housing for all income levels." They have accomplished that. For sale & for rent signs are visible there. Unfortunately they again put special interests first. The 19 foot garages do not provide adequate room for 2 cars, lawn mowers, snow blowers, trash cans etc. Snow removal will be a problem on the streets for the city & residents.	1
Preserving the historic buildings that have made Horace what it is today. Paving 64th Avenue all the way east to 45th Street to alleviate congestion. Providing/creating a city police force presence for Horace.	1
Population growing way too fast with no plan of development. Leaders have no clue what they are doing	1
Population density will change the character of the city. Bad planning will cause issues related to services needed to include police fire snow removal water sewer parking.	1
Please don't tear out the mature trees	1

Please be open to residents concerns having an equal weight as the big- money developers desires to push a plan down the throats of both the city & residents impacted by said plan(s).	1
Planning and zoning preparing for growth.	1
People that have lived in this community for years are here because they enjoy a quieter lifestyle. Developers are now trying to force the older neighborhoods in Horace to become busy and noisy. There are no sidewalks in the older neighborhoods, no parking lots (and they don't want them). If people want gathering spaces, they should be well thought out plans in the new neighborhoods. Leave the existing neighborhoods alone.	1
People realize too expensive to live due to taxes and specials too high	1
Overgrowth. Too crowded and unfriendly. Becoming a Fargo atmosphere	1
overexpanding and being swallowed up into West Fargo/Fargo	1
Overcrowding	1
Over taxation and running the middle class out of town	1
Over population, destroying things to make room for unnecessary housing. Losing the elevator	1
Over industrialization. Too much large construction and commercial traffic is undesirable	1
Over development	1
Over crowding, red tape bureaucracy, high taxes, government over reach	1
Out of control taxes, special assessments	1
Our biggest concern is that Horace will grow too fast and have lots of traffic and have no services like a grocery store. We are also concerned that the specials will continue to rise and we will be stuck footing the bill for things for which the developers should be paying.	1
Only one I can think of is getting a grocery store in the near future	1
Older citizens stuck in the past and not wanting to pay for things the city needs.	1
Not proper roads for traffic/over crowding tight dense housing	1
Not planned growth shitty infrastructure horrible city administration	1
not maintaining what Horace has currently and continuing to build businesses and housing developments that will not be kept up.	1

Not having the resources we need	1
Not having some of the necessities close by, like a grocery store	1
Not having lot of activities and not having lot of park or soccer field	1
Not expanding to have more amenities	1
Not enough solid and sufficient infrastructure to support the population boom	1
Not enough commercial opportunities to bring in sales tax.	1
Not enough access to businesses or affordable housing units.	1
Not embracing changes.	1
No more apartment buildings and car washes	1
no major concerns or opinions	1
no grocery stores, i think we need one.	1
No grocery store, no hardware store, and the ugly trailer homes on each side of the road on Main Street.	1
No grocery store High taxes	1
No grocery store and lack of a swimming pool do not keep business or family activities in Horace. Eventually, families will move elsewhere to be closer to where their kiddos can participate without the deprivation	1
No full-time police or fire department	1
No consideration of building up the main street to hold a true small town feel	1
No businesses, high price of gas. Shouldn't have to drive all the way to 13th Ave to do some shopping.	1
New development happens so quickly that existing infrastructure suffers.	1
Needs to better embrace it's rural past. Make an area that's more small town. Needs to better plan for future growth. Should try to take in places like sheywest. They would tie both types of Horace people together and provide entertainment options. Let them take city land along county 17 closer to deer creek area at a good discount. Maybe if they can afford it get them by the apartment building by the school. Would be huge.	1

Needs a grocery store	1
Need police force and non attractive growth.	1
Need more businesses for how many residents there are now. Too many new home rentals, brings in less than ideal neighbors	1
My concern is this city is growing too fast without concern for how overpopulated our elementary school became in just 3 years. We spend more money on our parks then our fire department. It feels like the city wants to keep making developers happy but not the residents who have lived here awhile. Some of the roads in "Old Horace" are garbage. The Senior Center needs updating or we need a better facility to hold events such as the Halloween party that the city puts on each year. It is way TOO CROWDED in the Senior Center and Horace Elementary. I don't want a lot of big businesses to come into Horace and cause more traffic and noise. I think a few small shops, an event center, a swimming pool, and plenty of bike/walking paths is what we need more than more houses being crowded into neighborhoods.	1
My concern is how high the taxes have gotten with all of the growth. Luckily, we can afford it, but I'm curious about those that might not be able to. It could potentially force people out that have been here for years.	1
My biggest concern is the taxes in Horace and the community growing at a fast rate.	1
My biggest concern is losing the identity of Horace as it expands. Last thing I want is for us to turn into West Fargo or Fargo and be stripped of our uniqueness and freedoms in that we can use various modes of transport within town (Snowmobiles being a huge piece in Horace as well as golf carts, 4-wheelers, etc), and the fact that we still have that small town feel.	1
Multi-family housing	1
More people than town can support with schools, traffic, etc. Having people that bought homes with less specials required to pay for other additional infrastructure when there is improvements that could be done in existing neighborhoods. Specials on new developments that don't benefit me.	1
More parks/trails/sidewalks	1
More developments with houses and apartments stacked ontop of eachother, a new south Fargo feel. With no small town feeling. The developers run the city. More money for them and less freedom for us. Yes we do need a grocery store and thats all I am asking for on my wish list!!	1
More concern for the new developments than the older parts of town. Older parts of town have terrible street conditions but newer developments get all of the attention	1

Mine is already lost, it has lost it's small town feel. You use to be able to go to the small grocery and you knew everyone there. I don't know anyone anymore.	1
Meadowlark Park is nice but in a out way the way area	1
Making it an extension of Fargo. We need to keep our own identity. That is what makes Horace special.	1
Major growth brings crime. We are already seeing it.	1
Maintenance of the city moving forward. Especially the older part of town.	1
Maintaining the integrity of the existing communities and neighborhoods of Horace.	1
Maintaining small town life with dirt and gravel roads with a fun downtown area to congregate without a trailer park next door	1
Loud, urban, liberal, unclean	1
Losing the small town safe feel. It turning into the next WF or Fargo.	1
Losing the small town feel...which is almost gone already	1
Losing the small town feel.	1
Losing the quiet small-town appeal that brought us here. I already see it happening with the addition of Lost River and it's incredibly impatient residents (always speeding thru town and always tailgating). We moved out here for the sleepy town that it was. I don't see my family staying here much longer unfortunately due to this extensive growth surrounding the location of our home.	1
Losing the charm and community feel. Crowded schools.	1
Losing our small town feel	1
Losing its character and sense of community by adding apartments and strip malls. Completely takes away from the safe small town feeling we use to have.	1
Lose its small town feel & charm	1
Lose its quaint feeling because of greed	1
Loding it's small town charm	1
Leaders afraid of growth and inadvertently resulting in business not coming into town. Inexperienced leaders when it comes to a city with exponential growth.	1

Large apartment buildings being built	1
Lack of small town feel just turns into fargo like everywhere else	1
Lack of planning as the city grows. Increased traffic.	1
Lack of diverse businesses in the community, growth locations especially with how close the city of fargo has annexed to horace, lack of pedestrian facilities across the city, lack of clear growth plan.	1
Lack of community center, careless cheap architecture, focus on commerce and cars instead of a beautiful healthful environment for people	1
Keeping up with the rate of growth and no grocery store.	1
Keeping up with growth	1
Keeping the small town vibe	1
Keeping the small town feel. The trees. Being relatively quiet. Having a grocery store would be nice.	1
Keeping the small town atmosphere.	1
Keep it rural	1
It's becoming the new Fargo south due to everyone wanting to move here and still want everything Fargo has. If that's the case, stay in Fargo	1
It's no longer a small town. It no longer feels rural. It's just part of West Fargo now.	1
It's growing too fast. Horace already has lost its small town feel and now is an extension of west Fargo. We moved here for the small town making it Fargo/ west Fargo will drive people out	1
It's gotten too big and longtime residents are/will be moving away.	1
It's going to get too big and lose that small town feel	1
It's getting too big, too much traffic, too many residential areas, no longer the small town it used to be	1
It won't continue to grow and business will close like Tundra Nutrition	1
It will no longer be a small town but rather just a suburb of Fargo/West Fargo. It will look just like them and no longer be the town that people moved to in order to have more space and peace.	1
It will grow too big too fast, lack of quality infrastructure, too industrial, lose its charm, TOO MANY CHAINS	1

It will grow too big and too fast. It will lose that small town feel. All the reasons we moved to Horace will be gone as developers/housing projects/businesses will take priority for financial reasons vs caring why the people moved to Horace in the first place. I'm concerned if Horace becomes another statistic like West Fargo/South Fargo that we will have to sell our forever home and move to a community that truly values what it means to be a small town.	1
It will expand too fast that the schools can't keep up	1
It will be too expensive to live here. Taxes and specials will force long time residents to leave. It is becoming a community for the rich and privileged. It's growing too fast	1
It will basically connected to Fargo like how West Fargo-Fargo are connected	1
It turning into Fargo/West Fargo. Most people already cannot tell where West Fargo ends and Horace starts. Too much greed with the city and approving all the developments with tiny lots and apartments. The small "affordable" houses are just turning into rentals and majority of renters couldn't care less about appearance or upkeep of property. Again greed of the developers and management companies.	1
It is growing way too fast and too many houses crammed in areas. Too much traffic in our already established neighborhoods.	1
It is growing too fast. It is losing the charm we once had.	1
It is growing too fast.	1
It is going to become unaffordable for middle class families.	1
It is becoming too expensive to live there with all of the new projects and special assessments that will come from them. With growth and expansion and already limited services the community may be unrecognizable in just a few years and it's charm will be gone.	1
It growing too much and losing the small town feel, and peace and quiet.	1
It growing too much.	1
It growing into a bigger city. It getting over populated, prices of everything going up. Losing the small town, quiet, safe space it used to be. Building too many houses which it already has, especially the cookie cutter tract housing with no yards.	1
Infrastructure to keep up with the growth. Grocery store, gas stations etc. It took a long time to get a gravel road paved when lots of people were traveling it. Making sure Infrastructure keeps up/gets ahead of the growth.	1
Infrastructure not meeting demands of growing population.	1

Infrastructure Cost of specials Too rapid growth	1
Infrastructure	1
Ineffective lesders allowing outside contractors and real estate agents determine the future of the city instead of the people who have lived here before growth started.	1
Increasing prices and construction driving people out	1
Increases in taxes.	1
Increased traffic Maintaining the small town feel Supporting schools especially with the growth	1
Increased taxes and specials that comes with the development.	1
Increased specials and taxes	1
Increased crime and that the older parts of town continue to look more junky	1
Increased crime and congestion with tye growth.	1
In need of a grocery store	1
If the town will be able to expand and be able to offer conveniences to the residents. Grocery store, splash pad - things like that. Horace is growing, no sense in trying to stop that, instead control the growth into a family and quiet environment. A place that's from for people to go on an open weekend day.	1
I'd like to see intentional growth to keep the small town feel. Also would like to feel "downtown" is safe, and the run down housing next to the small downtown is a barrier to that.	1
I'm concerned Horace is growing too quickly with a short term mindset. I understand we can't meaningfully expect Horace to not grow, but we need to be careful to grow in a responsible way that our utilities and infrastructure can sustainably support. Those of us who are established residents in older homes don't want to be saddled with special assessments to support the cookie cutter developments. And we vote accordingly.	1
I want to make sure that the rapid growth is intelligently controlled. I want the neighborhoods to be connected. Multi-use paths are key. Well-kept and well lit parks and other public features should be a priority. I hope we bring in businesses that offer unique amenities that will get people shopping, dining, and congregating locally. The West Fargo POW/MIA plaza is a great example. That area was largely avoidable in the past, now it's great with a unique brewery, cocktail bar and a nice outdoor space.	1

I myself and many others feel the town is growing way too fast with all the developments. At some point we all know we'll lose the small town feel which made us move here in the first place, and begin to feel more like west fargo or fargo. At the rate it continues to go a lot of us we'll be looking elsewhere to live.	1
I moved to Horace 4 yrs ago. We wanted the small town feeling. Then, within a year ugly apartments and strip malls were built. Ruined the whole town. So sad. My concern is the town will continue to grow and suffer the influx of people and crime. I polled all the wildlife and they want to keep their small town just that, small and countryside land.	1
I love the small town single family home feel and hope that that is mostly maintained over multifamily complexes.	1
I hope the old areas of Horace change to reflect the new growth of the city. For example, I cringe driving by the dumpy looking trailer park en route to my \$400k+ home. I know that sounds horrible, but if the city wants to continue to have new homes and businesses built, the appearance of the city should be better.	1
I fear it being absorbed by Fargo/West Fargo, it might lose its biggest asset—its charm. For those of us kids who grew up in the country but have big city jobs, Horace is the perfect balance and escape in the metro to have some peace and taste of home in the evening.	1
I don't want Horace to be swallowed up by West Fargo. I have loved the small town feel.	1
I do not want to lose the small town, quiet feeling.	1
I do not want it to fill up with apartments and low(er) income homes.	1
I believe it is losing the small town feel that we wanted when we moved here. It feels like the council is pushing the city to be like West Fargo. Horace should build its own reputation as a small community of single family homes and close knit neighborhoods.	1
I am concerned that the growth heading north on 17 will take away from any efforts to have a revitalized downtown.	1
I am concerned due to rapid growth we will lose our small town liberties to unelected bureaucracy.	1
How to find land I can build on, once the interest rates again normalize, in order to get out of whatever town feel is being created. New housing districts continue to be built, while our road is pieced to get like Frankenstein. I am now pay specials for I have no idea what; everything was sufficient for over a decade. What am I paying for, roads that are new for others, while we have potholes that will break an axle?	1

How it's growing to fast and trying to be a downtown feel	1
How if I want a loaf of bread I have to drive 7 miles	1
How I will afford to keep living here. The specials are ridiculous and the city seems to not see any problem with this at all. The city does not appear to have a long term plan for development. they should be looking for and recruiting commercial to bring to the city instead of pushing them away. residential neighborhoods can not support a city by themselves.	1
Horace wants to become Fargo/West Fargo and that's not a good thing. Most of us moved out here to get away from the city. I've lived in Horace for 26 years. At most, all we want is the grocery store that the city keeps dragging their feet with. The last thing Horace needs is a venue center and a "downtown". We like our peace and quiet. Leave all that other stuff to West Fargo and Fargo.	1
Horace outgrows its roads and infrastructure and becomes just "part of Fargo"	1
Horace needs a grocery store, more restaurants and a better library option than the West Fargo Library option.	1
Horace isn't a rural area anymore so stop trying to make it seem rural	1
Horace is growing too fast and losing its small town feel. More traffic causing longer commute times. I feel like there is not a good plan for how it's growing.	1
Horace has lost the hometown feel.	1
History and old down town will be destroyed.	1
High taxes and specials.	1
Having enough room at the elementary school for the influx of students.	1
Handing out building permits without having the needed infrastructure to handle the growth	1
Growth. The majority of people who live here do it because it's a small town. Randomly spamming properties with cookie cutter style houses is not good. Do what we can to try and reduce taxes for the existing residents and tax the hell out of new housing.	1
Growth too quick	1
Growth that is to substantial for the city to handle.	1

Growth that has been and will be so rapid that it outpaces infrastructure. In and of itself, that wouldn't be too bad, but with the 2 year tax abatement for new home owners, that means existing property owners are tapped to pay. I'm afraid of future special assessments and increased property taxes which I can't afford in retirement. It makes me mad.	1
Growth takes precedence over existing residents.	1
Growth is too fast. I like it when it was a quiet community with a rural feel.	1
Growth is too fast development. Focus is on the wrong things.- our city leaders are catering more to developers than the residence of the city.	1
Growth is already happening too fast. Very concerned about Horace outgrowing it's small town charm. A major reason many people have moved here or have been long term residents	1
Growth	1
Grows too fast without implemented infrastructure	1
Grows too fast and loses it's charm, causing me to want to move away.	1
GROWING WAY TOO FAST, FEELING LIKE FARGO.	1
Growing too quickly, houses too close together, apartments,	1
Growing too quickly for the infrastructure currently in place	1
Growing too much and too fast	1
Growing too much .. not at all what it was. Traffic is terrible now.	1
Growing too fast. Trying to become a suburb and losing it's small town feel.	1
Growing too fast. Too many neighborhoods with houses packed together. Not the nice bedroom community we used to be. Roads are now congested. We should've never put the tax abatements back in. Heard our debt to property value is 24%. Very very concerning!!! Credit rating is concerning. Tougher and tougher to get financing. Slow down!! Limit apartments...bedroom community is such a nicer living experience! Our schools and water flow are going to struggle keeping up. Those of us that have been here a long time are not interested in a bunch of new specials to pay for the 'new neighbors'. Disappointed the grocery store isn't coming.	1
Growing too fast. Losing the small town feel.	1
Growing too fast.	1

Growing too fast, people moving in with expectation of small town feel but Horace looks like it's trying to outgrow itself constantly right now. The developers are ruining this town.	1
Growing too fast, not enough to do	1
Growing too fast! We don't need to be a big town!	1
Growing too fast without proper infrastructure (not enough arterial roads to connect to Fargo/West Fargo.) Single lane roads will slow commute times as the city grows.	1
Growing too fast with no proper plans on how to do it.	1
Growing too fast to keep up with everything	1
Growing too fast losing its character	1
Growing too fast causing overcrowding in our newly built schools. Anticipating ever more growth with the FM diversion.	1
Growing too fast and not looking at the entire picture. Such as allowing developers to cut corners and putting in too many blocks of boring housing. Letting the highest bidders build what they want. And not creating shopping for the community (please bring us a grocery store!!)	1
Growing too fast and making poor planning and zoning decisions. Losing green space and the charm of why people originally moved to Horace	1
Growing too fast and losing the small town feel.	1
Growing too fast and leading to higher crime rate	1
Growing too fast and becoming too much of a city. Losing the elevator.	1
Growing too big. I miss the sleepy little town feel we had. Traffic is rough.	1
Growing too big too quickly and losing the small town feel that most people looked for when moving to Horace	1
Growing too big too fast. Not establishing enough jobs through businesses in the area to accommodate all the new residents, specials becoming too expensive.	1
Growing too big too fast, losing the small town feel/appeal.	1

Growing too big AND wanting to be like Fargo. Many people moved to Horace to have that small town living with the convenience of a larger metro just minutes away. Small towns don't have a downtown. They don't have public transportation except maybe taxis. Stop letting developers determine the growth and squeezing in neighborhoods with postage stamp lots. Look to bringing in some key businesses for revenue rather than hundreds of crammed houses for property tax.	1
Growing too big and losing small town appeal.	1
Growing too big and fast.	1
Growing too big and fast, and no longer feeling like a small town.	1
Growing too much and too fast	1
Growing too much	1
Growing too fast. Small yards too crowded developments	1
Growing too fast.	1
Growing too fast without the city being ready for all the growth.	1
Growing too fast for city services	1
Growing too fast and water issues as it's pretty much a swamp they keep building on. Not complete sidewalks for safely walking.	1
Growing appropriately. Need for grocery store and utility services to keep up with growth	1
Groceries	1
Getting too commercialized	1
Getting too big. Fargo spilling in. Won't be able to enjoy ATVs or small town living.	1
Getting too big.	1
Getting too big too fast.	1
Getting too big	1
Getting too many small house packed neighborhoods. I don't mind the growth of neighborhoods but too many cheap small house neighborhoods could start attracting the wrong crowd.	1

Getting to be too big of a town and not have the small town feel that Horace currently feels like and why so many people have moved out here.	1
Getting big	1
Getting away from small town feel	1
Getting a grocery store in Horace.	1
Generic growth without creating a walkable downtown area	1
Fargo and West Fargo will explode around Horace and we will be left behind trapped between them.	1
Extremely high taxes not going to work for citizens efficiently nor timely. City can't keep up with roads, sidewalks, snow removal and basic needs of the town, but will burn cash on all sorts of plans that go over budget and time.	1
Explosive population growth. poor city planning. Population density. Poor infrastructure development. Developers having total control over developments rules. Unfair enforcement of neighborhood covenants based on who built your home.	1
Expansion without infrastructure	1
Expanding too big and attracting the fm area chaos	1
Excessive tract housing packed in on too small of lots and too narrow of streets in order to maximize developer revenue. These revenues are being subsidized by current residents via tax abatements to new home owners who would buy in Horace anyway due to relatively lower property taxes vs. Fargo and West Fargo. There is no good reason besides realtor and developer political influence and pressure for the Horace council to have reversed their decision that eliminated these abatements. Just because all the other cities are offering them is not valid. Interest rates were rising at the time of the elimination of abatements so that wasn't the reason for slower housing starts in that short period. Horace has the advantage of room to grow and the previously mentioned property tax differential to attract new families. The high proportion of all the new tract housing cheapens our small town image and current residents are sadly facilitating this by paying higher taxes than we should be.	1

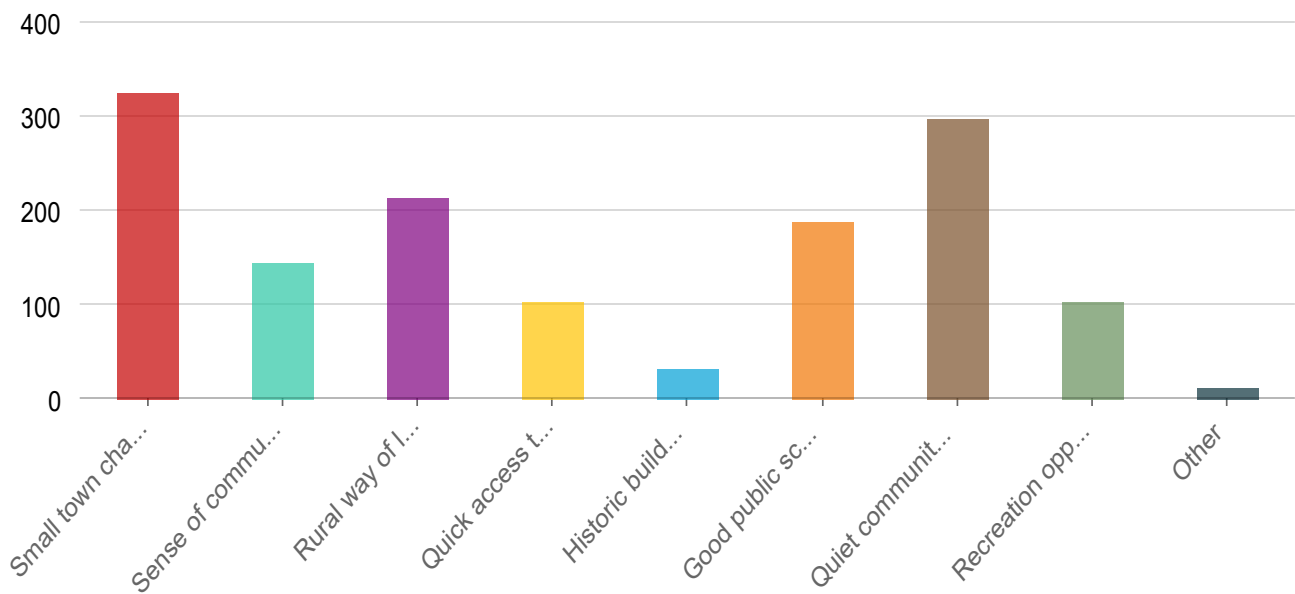
Excessive building allowed on too small of parcels. While I understand that smaller lots may be necessary sometimes due to infrastructure cost, there is outdoor enjoyment lost when you are crowded up against your neighbor. People want to interact with loved ones without others so close/involved, or go on a walk/run by themselves without feeling the need to interact. I've witnessed this living in Grayhawk Estates with West Fargo neighboring us. The lot sizes between these two areas represent about an 8 (West Fargo lots - 0.16AC) to 1 (Grayhawk lots > 1AC) difference. 1 AC lots for all areas is not realistic. Crowding people together isn't either. I witness regularly West Fargo residents walking with their kids, riding bikes, walking dogs, or with strollers in Grayhawk due to space and atmosphere. My other main concern is lack of commercial businesses coming in, seems we are behind on that front based on population growth, and we should be more than just a bedroom community.	1
Development is moving to fast that is what got Fargo into trouble	1
Developers are driving the growth, and the city officials roll over and do as the developers want.	1
Developer interest trumps quality of life. People moved to Horace to enjoy rural life close to Fargo, but that is rapidly being eroded due to greed by developers who are just interested in turning a buck.	1
Debt, growing too fast all at once, not having a vision in place	1
Crime. Starts with building apartments	1
Crime, outrageous taxation and losing our identity as a small town.	1
Crime will rise, noise pollution. This street dance is loud! The fireworks are loud! The early morning home builders waking up the neighborhood on the weekends. It needs to stop, only going to get worse. We need patrolling police and stop worrying so much about people speeding on the main road. We don't need a downtown.	1
Crime	1
Cramming in too many house that are too close together	1
Cost to live here	1
Cost of improvements.	1
Cost and allocation of specials.	1
Cookie cutter houses that are already taking over	1
Controlled growth	1

Continuing to develop small, tight neighborhoods instead of quaint private larger lots. People moved to Horace to be a part of a different kind of community. Large lots, nature, small town charm is why people love it here. Dont take that away..	1
Concerned that Horace will no longer feel like a small town rural community.	1
Commercial development placed in/near existing older neighborhoods, poorly planned residential areas with too many low end housing units crammed into too small spaces, those units turning into rental houses with 3-4 adults, ruining the small town feel and the aesthetics of our little town	1
City government is contradictory. Paying high taxes for the park district. Allowing new developments to butt up to existing ones and not showing any interest in helping the long term residents maintain their property values	1
City council trying to stop progress because they want it to stay a small town. It's unfortunate for a city council to govern for 600 and not the other 6000.	1
Can't keep up with the growth	1
Building was too many houses and not having the infrastructure to support.	1
Building upwards.	1
Building areas where children can stay active. Safety! How fast it is expanding. Will it eventually have its own police force?	1
Build more business and a grocery store	1
Big side walks- a grocery store in close proximity-	1
Big developers taking over like dabbert or heritage. Would love to see more small developers get a chance to help shape the area.	1
Better street surfaces in some part of city, ie: hole/rut on Park drive just before county 17/the Horace road - still not fixed and has been that way for a long time now. Need of a grocery store and hardware store. Support schools and their sports.	1
Being too "suburban" with strip malls and parking lots.	1
Being taxed out of my home. I built my home and raised my children here. We've been here 35 years and have seen many changes. My taxes have more than doubled!	1
Being able to keep up with and maintain thenn massive growth.	1

Becoming too much of an extension of Fargo. Bringing in tax money has been more important than offering something different.	1
Becoming to "fargo" like	1
Becoming southwest fargo	1
As a homeowner I can see having to move because special assessments constantly being added and taxes we will not be able to afford our home soon.	1
Apartments, chain businesses, losing the small town feel.	1
Apartments, becoming too big	1
Anything resembling the eyesore off of Sheyenne, namely the cluster of tiny homes crammed together onto tiny lots, 5 feet apart. It's very disappointing that Horace allowed this to happen. We also want to keep commercial in the current downtown area (get rid of the defunct elevators and utilize that space or south of town to 100th on the east side of the road. The land by the school is already commercial so utilize that space.	1
All trees/landscape to be removed	1
All the specials with which we're going to be slapped. Like a heavy duty road for a plumber that put his shop in a residential area. Jackass.	1
Adding a Bus line that would ferry in out-of-town people that would cause trouble (harassment, crime, theft) and litter. Horace residents can go to Fargo/WF/Moorhead for complex offerings. Leave Horace simple and quiet.	1
Access roads to Horace are extremely limited.	1
A lack of businesses to help support the tax needs of a growing community. We cannot simply be a bedroom community to West Fargo and Fargo with sales tax dollars going to those cities while Horace will continue to see an increased need for those sales tax dollars. If we as a city are unsuccessful at attracting and building our business base the city's residents will be required to carry the increased need for tax revenues through property taxes. This would continue to further impact the affordability of Horace. While change and growth of a city can be hard for long standing residents Horace cannot and will not return to a small town. We must be forward thinking and smart about Horace's inevitable growth.	1
"Old Horace" is trying to be made over into a hip place to be. We want peace and quiet. If there are gathering spaces and concert areas, they should be on the new neighborhoods.	1

Answered: 439 Skipped: 63

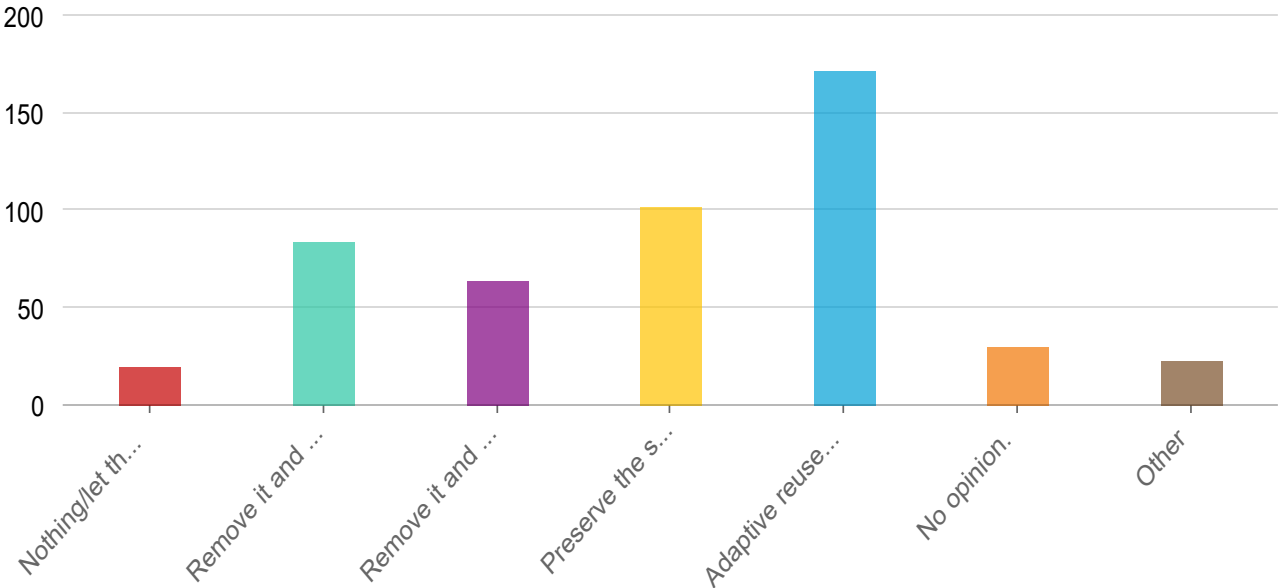
What is most important to you to preserve about Horace?



Answers	Count	Percentage
Small town character	325	64.74%
Sense of community	145	28.88%
Rural way of life	214	42.63%
Quick access to Fargo/West Fargo	104	20.72%
Historic buildings	32	6.37%
Good public schools	188	37.45%
Quiet community	298	59.36%
Recreation opportunities	104	20.72%
Other	12	2.39%

Answered: 498 Skipped: 4

What do you think should happen to the historic grain elevators?



Answers	Count	Percentage
Nothing/let the market decide.	20	3.98%
Remove it and move on.	84	16.73%
Remove it and repurpose materials in future art/placemaking opportunities Downtown.	64	12.75%
Preserve the structure and surrounding space.	102	20.32%
Adaptive reuse (e.g. enhance the structure and surrounding space) .	172	34.26%
No opinion.	30	5.98%
Other	23	4.58%

Answered: 495 Skipped: 7

Where are one or two places you regularly go in Horace? (could be alone, with friends,...



https://survey123.arcgis.com/surveys/1e5fe572b8e04b799bb8be09e2df35b6/analyze?position=0.what_words_come_to_mind_when_you&chart=0.... 50/121

Caseys	25
HQ	25
Gas	24
Casey's	22
station	21
school	19
Lutheran	18
walking	16
coffee	15
Post	14
Office	14
center	14
restaurants	13
Erv's	13
Independence	12
town	11
senior	11
&	10
shop	10
schools	10
neighborhood	8
elementary	8
Bar	8

pharmacy	8
River	7
City	7
paths	6
Fargo	6
walks	6
Lost	6
family	6
Taco	6
Revolution	6
store	6
house	6
Home	5
Parks.	5
grocery	5
place	5
park.	5
bike	4
frequent	4
west	4
love	4
playground	4
restaurant	4

friends	4
Lions	4
businesses	3
back	3
pond	3
visit	3
Station.	3
Daycare	3
pickleball	3
courts	3
north	3
community	3
Fire	3
Medicine	3
wine	3
Diversion	3
Headquarters.	3
roads	3
St	3
Sanford	3
trails	3
walk	3
Horace.	3

shoppe	3
local	3
year	3
Holiday	3
Chiropractic	3
queen.	3
area	2
traffic	2
beautiful	2
fargo.	2
events.	2
path	2
area.	2
neighborhood.	2
salon	2
schools.	2
Grove.	2
Country	2
nature	2
events	2
doesn't	2
leave	2
entertainment	2

shopping	2
-	2
tree	2
ride	2
garbage	2
here.	2
small	2
Great	2
massage	2
shooting	2
Hall	2
Benedict's	2
activities	2
restaurants.	2
school.	2
Erv's.	2
eat	2
town.	2
people	2
neighbors	2
quiet	2
moved	2
sports	2

business	2
family.	2
Don't	2
original	2
part	2
kids	2
biking	2
care	2
general	2
wide	2
High	2
Erves	2
safe	2
food	2
to.	2
ervs.	2
occasionally	2
lrvs	2
2	1
30	1
Work	1
lot.	1
dangerous	1

inexperienced	1
bikers.	1
explore	1
oriented	1
frequently	1
somebody's	1
yard.	1
sponsored	1
countryside	1
cub	1
creek	1
Vive	1
mom's.	1
items	1
preserved!	1
son	1
works	1
there.	1
gad	1
toddler	1
south	1
elementary.	1
Road.	1

Shoppe.	1
places	1
find	1
food.	1
Micah	1
Casey's.	1
Harvest	1
Valley	1
Heritage	1
coffee;	1
fuel	1
C-Store	1
items.	1
shop!	1
good	1
coffee!	1
parks!	1
grain	1
elevator	1
made	1
is..	1
farmers	1
hard	1

working	1
families	1
railroad	1
community!	1
track	1
housing	1
Cashwise	1
Hornbachers??	1
answering	1
want..	1
burn	1
goodness	1
2nd	1
restaurant!!	1
downtown.	1
District	1
barn	1
horse	1
shitty	1
absolutely	1
blessing.	1
bunch	1
stuff	1

moves	1
close	1
hoping	1
bring	1
crime	1
us.	1
Sunrise	1
St.	1
Ben's	1
gravel	1
ervs/headquarters	1
meetings	1
church.	1
Bingo.	1
firehall	1
coffe	1
Horace!	1
clinic	1
Event	1
Playgrounds	1
(elementary	1
meadowlark)	1
Levi	1

run	1
dogs	1
eat/dessert	1
lots	1
paths.	1
south.	1
backyard.	1
tracks	1
dogs.	1
opportunity	1
give	1
worried	1
things	1
walks.	1
Na	1
go.	1
front	1
porch	1
patio	1
neighbors.	1
Medolark	1
watch	1
Freedom	1

massive	1
cottonwood	1
tree.	1
retention	1
bike/walking	1
Friend's	1
house's	1
park!!	1
sidewalks	1
Tessa	1
Terrace	1
surrounding	1
Erva	1
era's	1
service	1
spots.	1
places.....the	1
kids.	1
meal	1
Jubilee	1
Equine	1
"old	1
Horace"	1

turn	1
rest.	1
Preserve	1
aspect	1
green	1
space.	1
Terra	1
Gardens	1
park/pond	1
favorite	1
forgotten	1
about.	1
waking	1
roller	1
blading	1
streets	1
crowded	1
drivers.	1
(needs	1
better).	1
planning	1
redo	1
year.	1

of.	1
happen	1
store!	1
preserved	1
touristy	1
type	1
vibe?	1
integral	1
Erv's!	1
disappointed	1
day.	1
baby	1
stroller	1
utilize	1
don't.	1
office.	1
ashamed	1
evident	1
Bean	1
Days	1
council	1
expects	1
pay	1

tab.	1
lived	1
years	1
street	1
terrible	1
shape	1
bearly	1
cars	1
curb	1
gutter.	1
stupid	1
put	1
Thue	1
ridiculous.	1
HQ/big	1
eras	1
casey	1
(will	1
complete)	1
Hawks	1
Football	1
games	1
daughters	1

dance	1
class.	1
banned	1
Kelly	1
Visto	1
ass	1
hole!!	1
wait	1
carwash!!	1
convenient	1
dog	1
friends/family	1
Home.	1
development.	1
Higher	1
income	1
communities	1
they're	1
spacious	1
attractive	1
Herritage	1
middle	1
Evrs	1

options	1
desperately	1
store.	1
inclement	1
weather.	1
(terra	1
gardens)	1
go!	1
scared	1
thing	1
weekends	1
(bounce	1
Midwest	1
Bounce).	1
feel	1
peace	1
supper.	1
Erv's.	1
anything.	1
shopping.	1
cheaper	1
now.	1
daily	1

only.	1
Ervs;	1
drive	1
past	1
Wild	1
Rice	1
Walcott	1
Abercrombie.	1
roaming	1
city.	1
feelings	1
halt	1
growth	1
page	1
Boulder	1
Pearl	1
Street.	1
updated.	1
head	1
quarters	1
houses	1
Fishing	1
Horace....	1

offer	1
attractions	1
shops/business	1
trailer	1
developments	1
stop	1
me.	1
restaurant.	1
Downtown	1
shops	1
(Grove	1
etc)	1
it's	1
busy	1
(walking	1
dogs)	1
(late	1
night	1
bite	1
pickles)	1
(pickleball).	1
Dental	1
escape	1

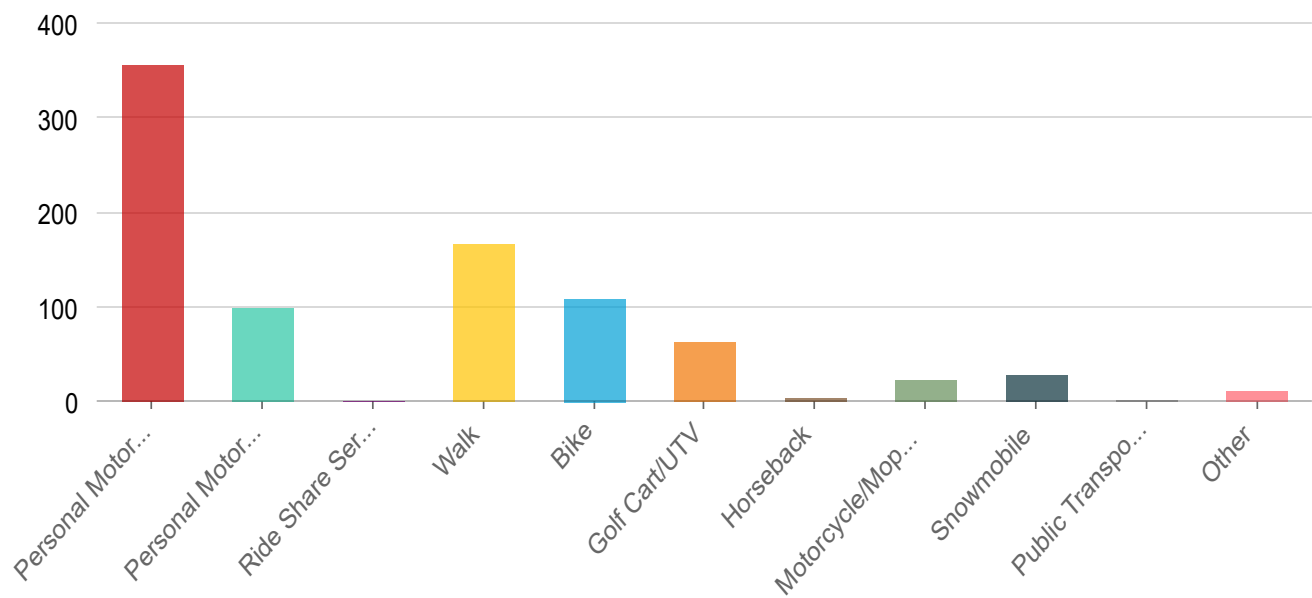
construction	1
noise.	1
Corey's	1
Customs	1
Collision	1
resturants	1
Center.	1
usps	1
eat.	1
drink	1
ally	1
too.	1
money	1
spent	1
Bug	1
BeWell	1
dentist	1
bars	1
Bingo	1
@	1
sr	1
occasion	1
Benedict's	1

traditions	1
backyards	1
Bodywork	1
Skylar	1
Rose	1
Freed	1
adult	1
public	1
spaces.	1
childhood	1
Dakota	1
Ave.	1
all.	1
Meadowlark	1
ATV	1
rural	1
paths—love	1
them!!	1
staple.	1
atmosphere.	1
trail-sidewalks	1
awesome!	1
pickle	1

ball	1
Baseball	1
field	1
community.	1
Bar/	1
/DQ/	1
Casey's.	1
randomly	1

Answered: 439 Skipped: 63

How do you typically get to these destinations?

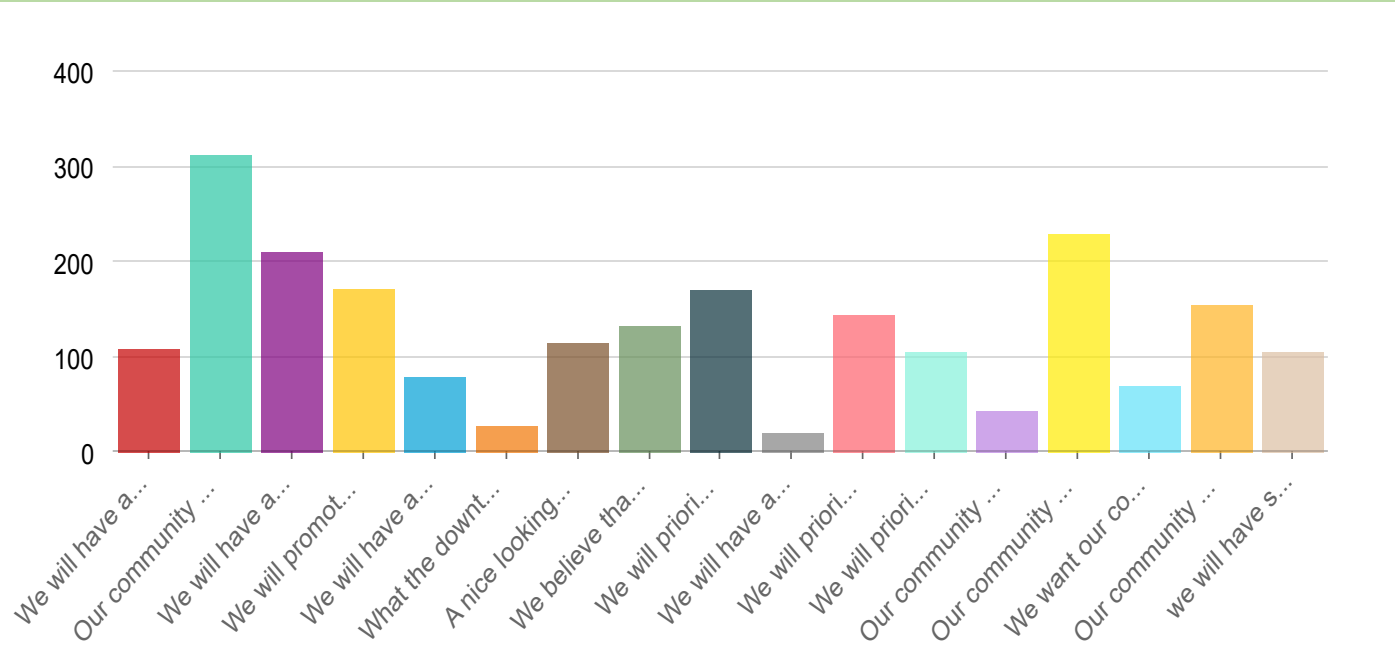


Answers	Count	Percentage
Personal Motor Vehicle - Travel Alone	356	70.92%
Personal Motor Vehicle - Carpool	99	19.72%
Ride Share Service	1	0.2%

Walk	167	33.27%
Bike	109	21.71%
Golf Cart/UTV	63	12.55%
Horseback	4	0.8%
Motorcycle/Moped	23	4.58%
Snowmobile	28	5.58%
Public Transportation	2	0.4%
Other	11	2.19%

Answered: 428 Skipped: 74

What statements fit your vision for the future of Horace as a community?

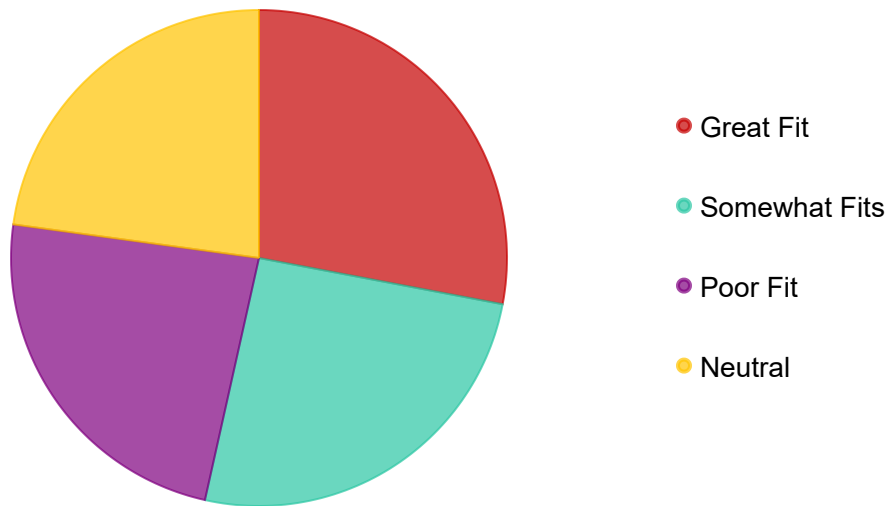


Answers	Count	Percentage
We will have a community where businesses thrive	109	21.71%
Our community will have a small town identity	312	62.15%
We will have a community accessible by walking, biking, and driving	211	42.03%

We will promote having high quality of life with easy access to downtown, parks, schools, and other community places	172	34.26%
We will have a downtown with unique opportunities and specialty shopping	79	15.74%
What the downtown looks like is less important than the mix of businesses	28	5.58%
A nice looking downtown will help attract the types of businesses that we want in Horace	115	22.91%
We believe that accommodating growth requires investing in things that make our community attractive, more livable, and more connected	133	26.49%
We will prioritize maintenance of what we already have before investing in new downtown improvements	171	34.06%
We will have a downtown and transportation system that incorporates strong community identity	20	3.98%
We will prioritize the needs of our children and young people when building new community improvements and connections	144	28.69%
We will prioritize the needs of our agricultural surroundings when building new community improvements	106	21.12%
Our community will have a more suburban identity	44	8.76%
Our community will have a more rural identity	229	45.62%
We want our community to have strong social ties	70	13.94%
Our community will have lots of activities for all ages	155	30.88%
we will have something for everyone in our community	105	20.92%

Answered: 499 Skipped: 3

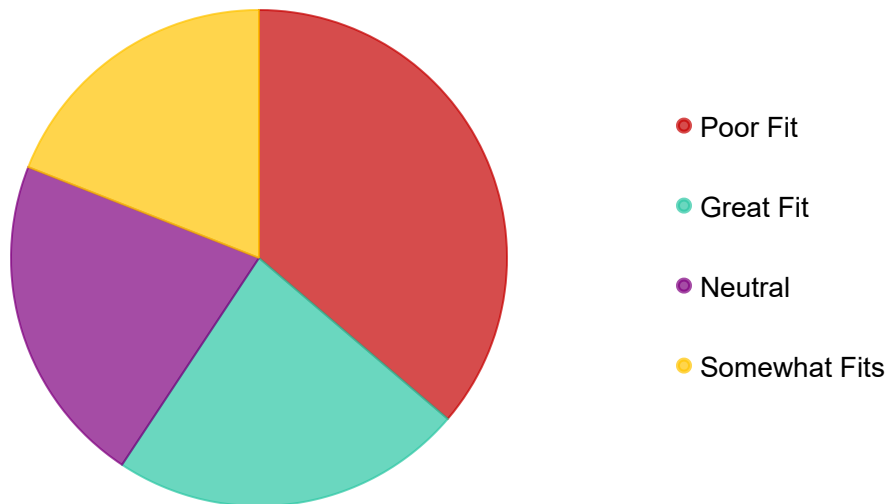
Active Sidewalks



Answers	Count	Percentage
Great Fit	136	27.09%
Somewhat Fits	124	24.7%
Poor Fit	115	22.91%
Neutral	111	22.11%

Answered: 486 Skipped: 16

Commercial Plaza

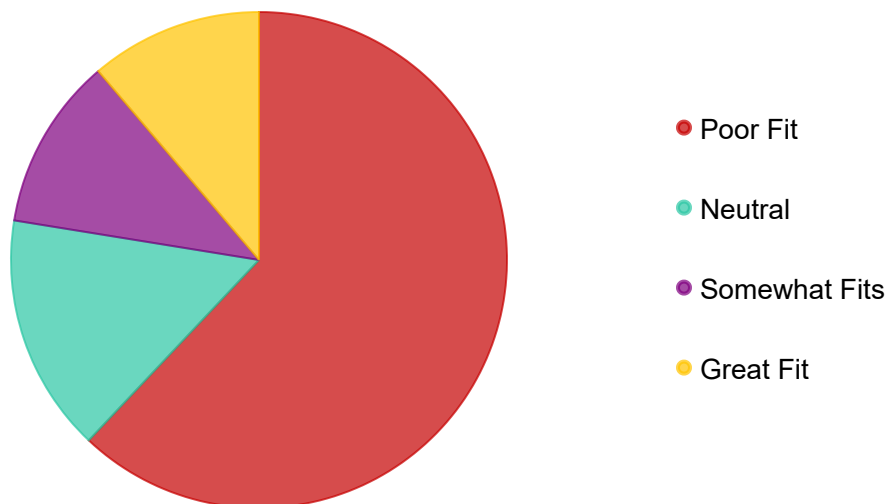


Answers	Count	Percentage
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Poor Fit	179	35.66%
Great Fit	114	22.71%
Neutral	107	21.31%
Somewhat Fits	94	18.73%

Answered: 494 Skipped: 8

Modern Mixed-Use Downtown High Density

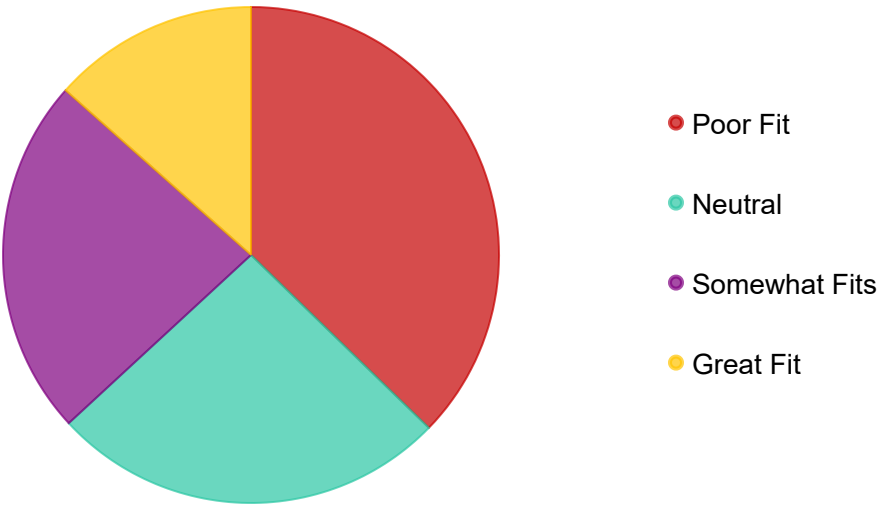


Answers	Count	Percentage
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Poor Fit	304	60.56%
Neutral	76	15.14%
Somewhat Fits	55	10.96%
Great Fit	55	10.96%

Answered: 490 Skipped: 12

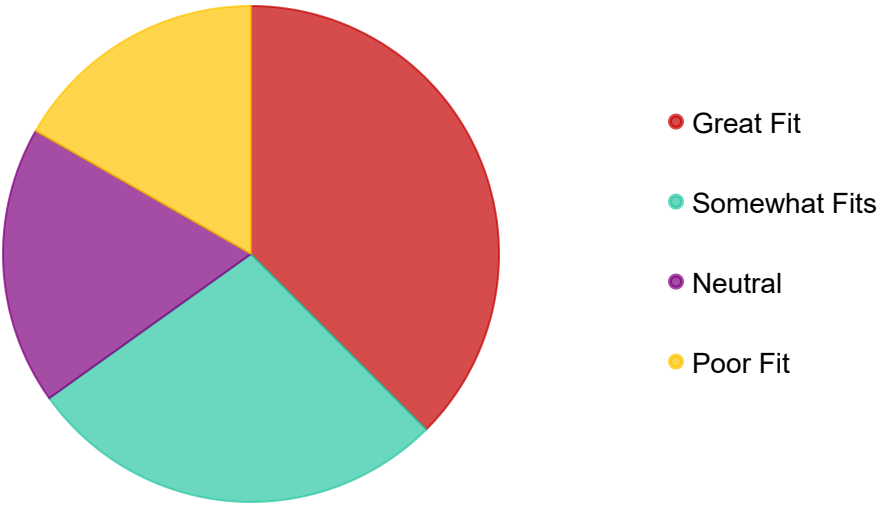
Modern Retail



Answers	Count	Percentage
Poor Fit	183	36.45%
Neutral	127	25.3%
Somewhat Fits	115	22.91%
Great Fit	66	13.15%

Answered: 491 Skipped: 11

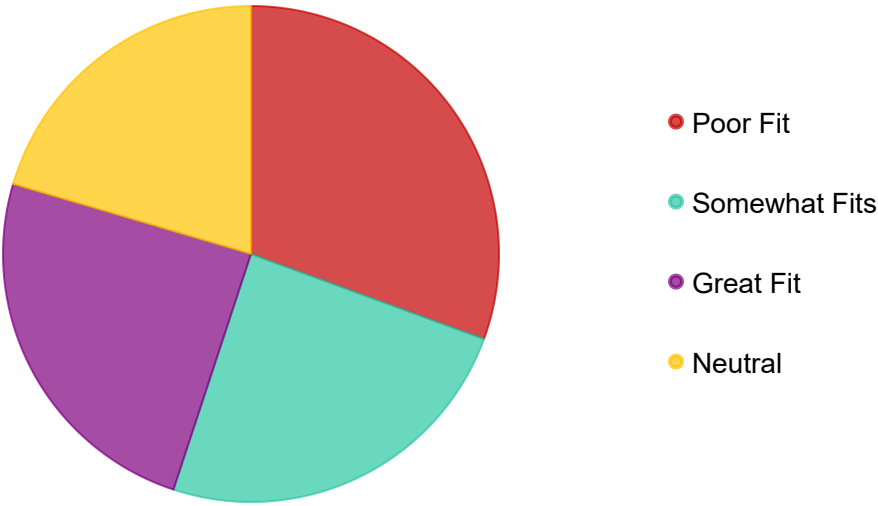
North Dakota Main Street



Answers	Count	Percentage
Great Fit	186	37.05%
Somewhat Fits	137	27.29%
Neutral	90	17.93%
Poor Fit	83	16.53%

Answered: 496 Skipped: 6

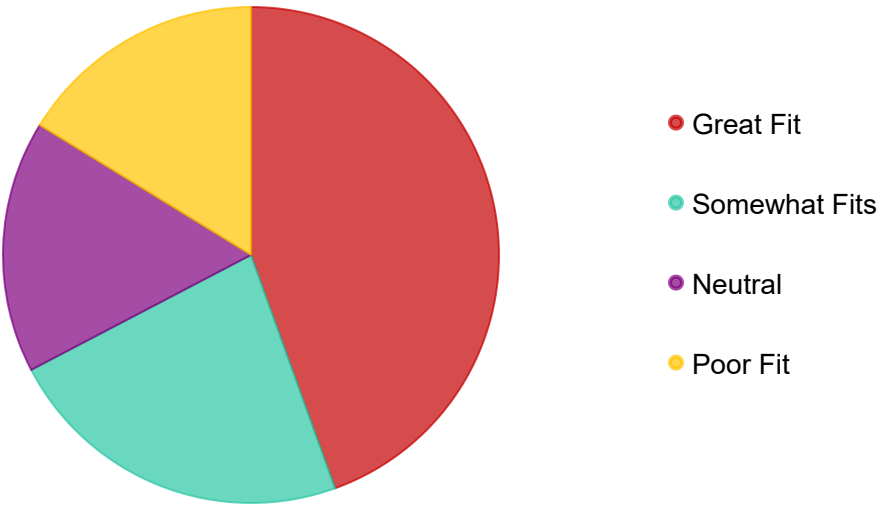
Pedestrian Mall



Answers	Count	Percentage
Poor Fit	151	30.08%
Somewhat Fits	121	24.1%
Great Fit	121	24.1%
Neutral	101	20.12%

Answered: 494 Skipped: 8

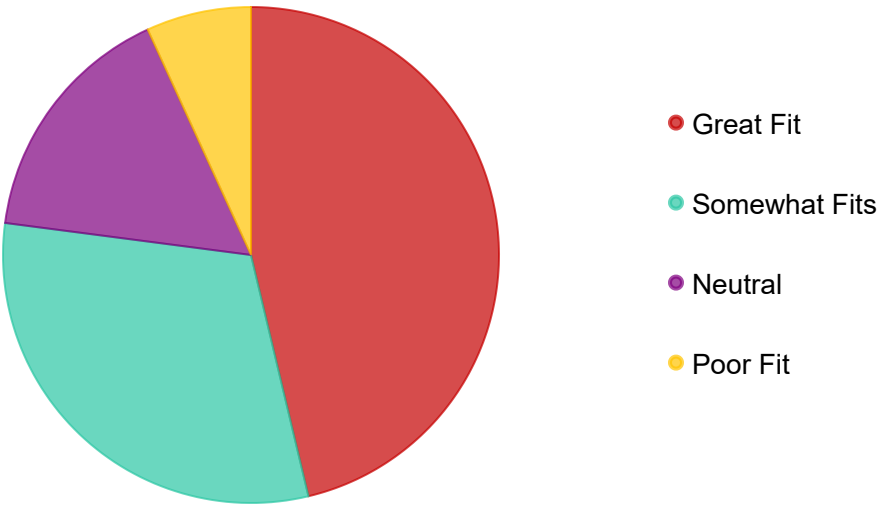
Repurposing of Rural Structures



Answers	Count	Percentage
Great Fit	222	44.22%
Somewhat Fits	114	22.71%
Neutral	82	16.33%
Poor Fit	81	16.14%

Answered: 499 Skipped: 3

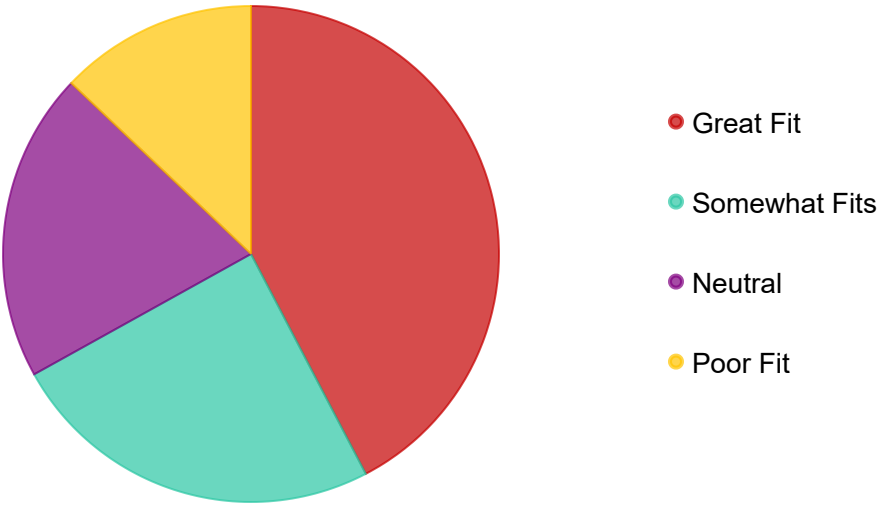
Rural Gathering Space



Answers	Count	Percentage
Great Fit	230	45.82%
Somewhat Fits	153	30.48%
Neutral	80	15.94%
Poor Fit	34	6.77%

Answered: 497 Skipped: 5

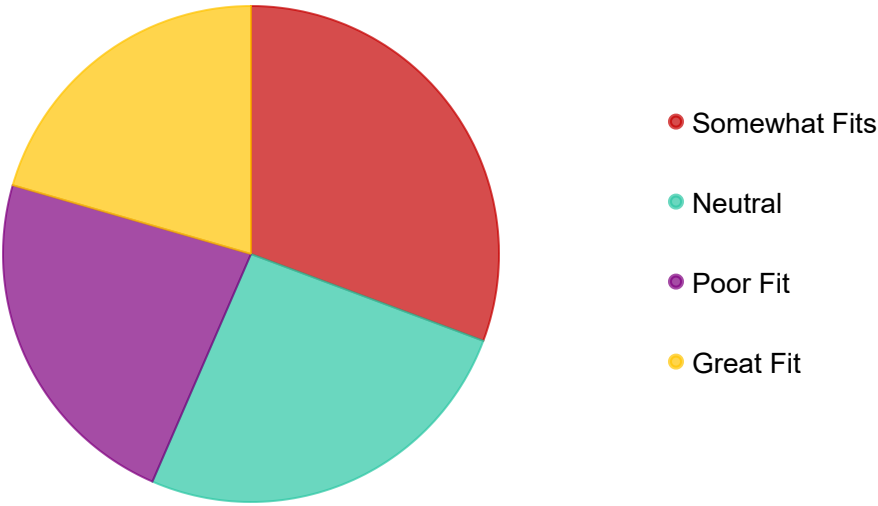
Traditional Main Street



Answers	Count	Percentage
Great Fit	210	41.83%
Somewhat Fits	122	24.3%
Neutral	100	19.92%
Poor Fit	64	12.75%

Answered: 496 Skipped: 6

Streets as Public Gathering Space & New Development



Answers**Count****Percentage**

Somewhat Fits

151

30.08%

Neutral

127

25.3%

Poor Fit

113

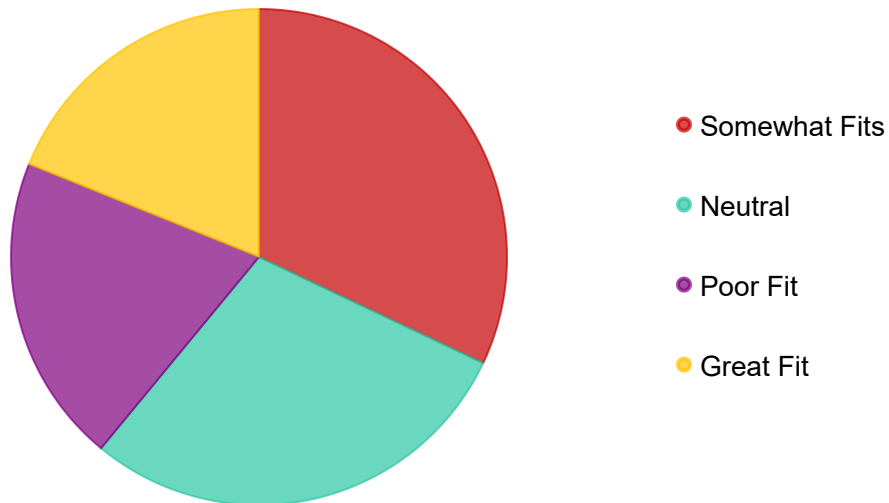
22.51%

Great Fit

101

20.12%

Answered: 492 Skipped: 10

Streets as Public Gathering Space in Traditional Development**Answers****Count****Percentage**

Somewhat Fits

156

31.08%

Neutral

141

28.09%

Poor Fit

98

19.52%

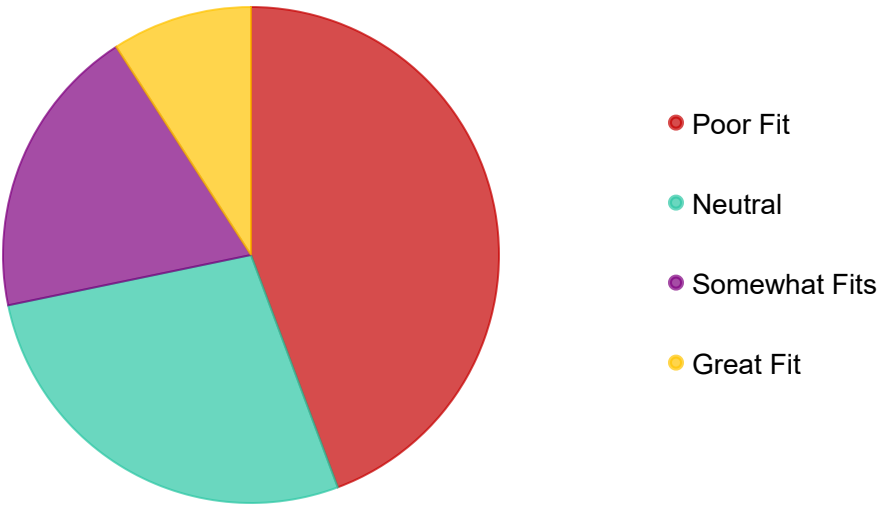
Great Fit

92

18.33%

Answered: 487 Skipped: 15

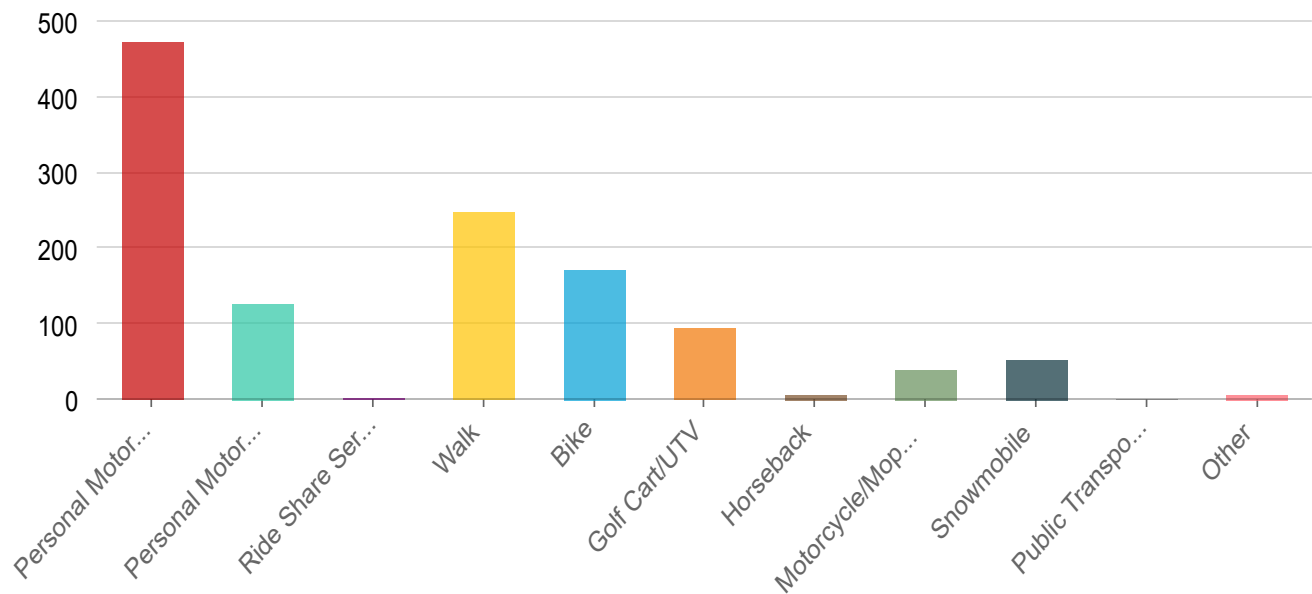
Western Heritage



Answers	Count	Percentage
Poor Fit	218	43.43%
Neutral	135	26.89%
Somewhat Fits	94	18.73%
Great Fit	45	8.96%

Answered: 492 Skipped: 10

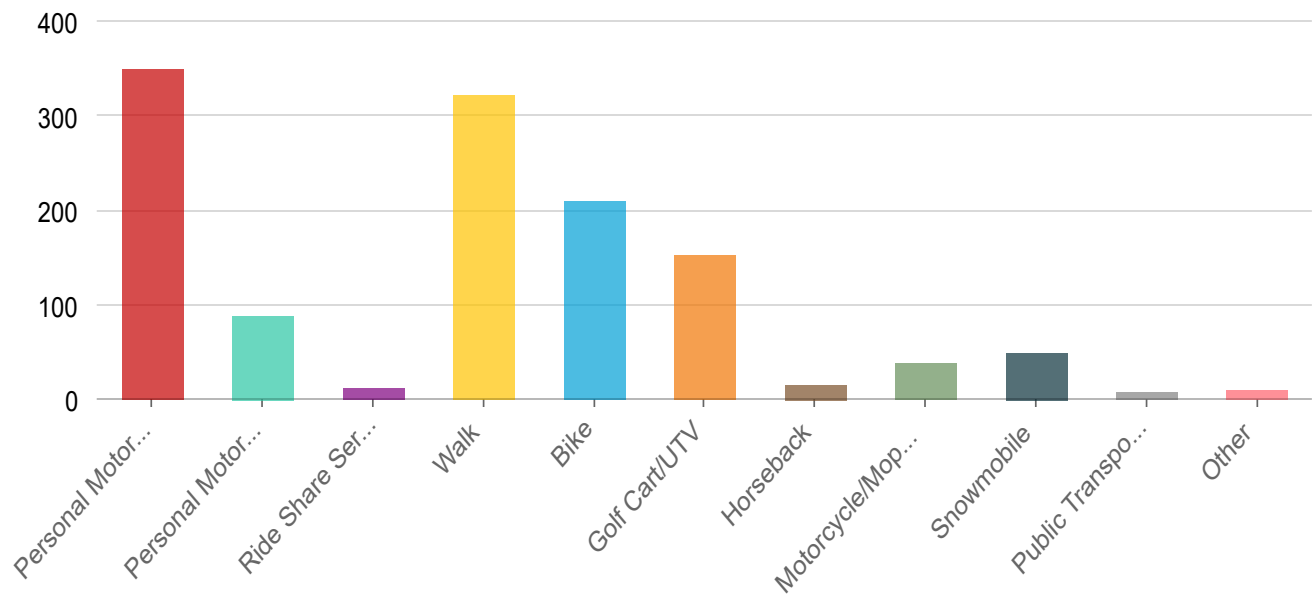
Currently, how do you get around Horace?



Answers	Count	Percentage
Personal Motor Vehicle - Travel Alone	473	94.22%
Personal Motor Vehicle - Carpool	127	25.3%
Ride Share Service	2	0.4%
Walk	248	49.4%
Bike	172	34.26%
Golf Cart/UTV	95	18.92%
Horseback	7	1.39%
Motorcycle/Moped	40	7.97%
Snowmobile	53	10.56%
Public Transportation	1	0.2%
Other	7	1.39%

Answered: 502 Skipped: 0

What mode of travel would you prefer to use when visiting Downtown Horace?



Answers	Count	Percentage
Personal Motor Vehicle - Travel Alone	350	69.72%
Personal Motor Vehicle - Carpool	89	17.73%
Ride Share Service	12	2.39%
Walk	322	64.14%
Bike	210	41.83%
Golf Cart/UTV	153	30.48%
Horseback	16	3.19%
Motorcycle/Moped	39	7.77%
Snowmobile	50	9.96%
Public Transportation	8	1.59%
Other	10	1.99%

Answered: 496 Skipped: 6

Do you have any other comments or suggestions regarding Downtown Horace?...



Response	Count
Need a grocery store	2
Grocery store	2
Your asking for trouble!! When you open it up your gonna have to face problems like never before do we seriously need a down town?? WE DON'T!! ITS not a small town feel if this is what the City wants its not what we want!!! Peace out Erin S.	1
You need parking whether it's the senior center,the fire hall Big Irvs. Whoever bought the building adjacent to Big Irvs has caused a lot of the issues creating only one entrance and one exit to the parking lot. This makes it difficult for customers delivery trucks garbage pick up etc. across the street is the event center that says parking for event center only couldn't those signs be up just during events? Accessibility to the new business district is horrible to get to the Dairy Queen it's quite the maze same with Sanford etc. a frontage road perhaps or an entrance like the dental office not an urban planner but what a pain. It would be nice to have a small grocery store for basics and also a bank.	1
You keep talking about a Downtown Horace as if it's a done deal. Why? Were us residents asked this? Or just your social circles?	1
Yeah build stores things to do etc Maintain properties such as fields over grown etc. build more parks Maybe buy more snowplows we pay so much in taxes and half the winter our streets aren't even clear Maybe the next school build bigger instead of wanting to add on two years later you are in a growth spurt, you are not done growing yet. My child understands this why can't the city see that	1
Would love to see the small town return or at least be preserved. House on top of each other and very limited areas for ATVs, snowmobiles don't lend themselves to the small town lifestyle.	1

Would love to see accessible walking path from maple lake estates south of 100 to the school.	1
Would love to see a splash pad for the kids in the community and a dog park!	1
Would love a grocery store and a general merchandise retailer like Target or Walmart!	1
Would love a fitness center and car wash.	1
Would like to see an upscale eatery or unique venue like Brewhalla	1
Would be nice to have a few businesses for more of the commercial shopper but not necessary since Fargo is easy to get to. Would give them up to keep the small town feel. Other than maybe a grocery store being closer.	1
Would be cool to see something similar to the Silos in Waco, TX in terms of repurposing the elevators, adding a shopping space and family activities.	1
Why do we need a downtown? Get a grocery store, swimming pool, a few restaurants or cafes and call it good. We don't need to be Fargo or West Fargo. If I want to go downtown, I will go to Fargo. I don't want the noise of cars & people close to my home along with littering that comes with having a downtown. I want to enjoy the quiet of my yard that is becoming a distant memory with all the speeding cars, sirens, or back up sounds from construction equipment. Stop building so many houses out here. It's getting ridiculous. Our schools can't keep up with the pace you continue to allow being built. I miss the quieter days in Horace.	1
Whatever the city does have control over, worry more about the quality vs quantity of projects and development.	1
We would love to see the old Northern Pacific railroad bed surfaced out to where the tracks are blocked off by ties... there is a little over 3 miles of potential for a walking/biking/running trail following the historic rail line! I think the community would love this!	1
We understand the need for business to offset residential home taxes. We do not feel it's a great look to jam a bunch of homes and multilevel businesses into the backyard of people who cherish the privacy and space they currently enjoy.	1
We need business to help pay for things, taxes, roads, sidewalks.	1
We need a Walmart!	1
We need a Trader Joe's or another grocery store.	1
We need a grocery store. Please make this a priority	1

We need a grocery store	1
We moved to Horace for the small town feel, and creating a downtown will diminish that.	1
We moved to Horace because of the small town identity. I lived here as a kid and I love the small town quiet feeling.	1
We moved out here to enjoy the quiet. Please don't ruin that.	1
We don't need developers telling us how Horace should be, they can take their ideas elsewhere! I'm tired of their plans which strap extra specials on me without gaining anything. The "beautiful people" need to pay for their own neighborhoods and not make the rest of Horace pay for them. How about fixing the infrastructure problems we have currently, such as the poor drainage systems / lift stations in some neighborhoods.	1
We don't need all the latest and greatest new and shiny things. We are a small rural community and we should keep that at the heart of any future development and should be the lense through which the city looks at any possible "improvements" or "enhancements".	1
We came to Horace from NY because we were sick of crowded suburbs with traffic and stoplights. We want Horace to be a no stoplight town. We want to keep a small town/rural feel with enough services for the locals, like a grocery, but without encouraging an influx of people from other areas.	1
Walking/biking paths needed from every addition to the school buildings! Kids riding bikes along the highway is dangerous!	1
UTVs and ATVs tend to race in our area which is scary at times due to the park nearby with small children. It's hard to call for police presence since they are gone by the time anyone shows up. Biking, walking, etc. are just quieter and more prone to respectful observance of traffic rules. Revving engines late at night also really takes away from a quiet and peaceful experience that Horace is known for. Revamping the elevators in town would be so awesome to see!! Also a "rails to trails" option on the old railroad tracks would be excellent for walking/running/biking much like what is happening in Minnesota. Love Horace and its great potential!	1
This needs to be done right. the current mix of Petty property lines and mis purposed buildings is not working. The fire department needs to learn to share or put up permanent fencing. across the street, The orange fencing looks trashy and needs to be resolved immediately. the Legion occupant needs to go. since they took owner ship it has ranged from unmowed lawn and various stages of remodel to downright trash all over the yard. The elevators need to go. add multiuse buildings with retail on main and apartments above. There are many buildings on main street in similar disrepair.	1
This is so exciting!	1

This is going to be a difficult project because there is no old historic downtown part. I do not want to see any more special assessments for this project come though. The city will need to get creative and find commercial means to fund it. I thought take view was going to be the new city center. The pictures shown of it look good and paid for by non-tax dollars :)	1
There should be more paths for biking and walking. Please keep Horace a small town. Everything we need we can drive to in West Fargo or Fargo. People have moved to Horace because it's a quiet and peaceful community.	1
There is no space in the current down town to rebuild. Do not tax us to make a space. Use the space behind Casey's and let the developers pay for it all.	1
The trailer park is ugly and will always be an eye sore to the downtown area. Even a privacy fence would be nice. They are someone's home, but it's not pretty.	1
The trailer court on main, the fenced in junk yard on main, and industrial businesses need to be moved appropriately. It would probably take a millionaire to pay these people and business to move though.	1
The town needs revamped to keep up looking good with all the new houses. Main looks junky and old along with all the trailer houses along there	1
The great think about Horace is the small town feeling. There's no need for me businesses or fancy and modern buildings. The cookie cutter houses are plenty.	1
The gis map wasn't very phone friendly so I couldn't use it. The questions about fit for Horace and all the pictures, it wasn't clear if I this is a current for future desired fit. I chose the ladder. You have a duplicate " copy" questions below.	1
The elevator is an eye sore with risk for fires. Many improvements for Freed park are wonderful.	1
The division of the different development citizens.	1
The council has already made decisions that I don't agree with so this survey is more of a way for them to pay themselves on the back, then to really know my thoughts. The truth is they really don't care for the old Horace it is a black eye to them that is why they are moving everything to the north of the elevator. The park board is out of control on the amount of tax money they receive. Horace was a wonderful bedroom community where you knew your neighbor, you could allow your children or grandchildren outside to play, on my block there is a crack house and a pedafile, I don't even want to be outside let alone my grandchildren. Our so called law enforcement can't be seen on a Friday or Saturday evening when the drunks from Big Ervs are speeding away but they are out in force on Sunday morning making sure the stop people going 5 miles over the speed limit. Even changing the name of the fire and rescue shows how ashamed they are of Horace in general. Personally I can't wait to move.	1

The city needs to think of the community and the people in it. Not grow into what west Fargo has become or bring big buildings like the lights into our community and keep the small town feel	1
The biggest challenge is where do you want the center of the town and what do you want it to look like? Right now Horace feels like it's missing a downtown where people can shop, eat, explore.	1
The best part of Horace is no one outside the community comes here.	1
Strongly suggest keeping commercial in our downtown. Keep the integrity of the existing communities.	1
Stop with the strip malls!	1
Stop unnecessary development.	1
Stop the growth or control the chaos that will come with it	1
Stop taxing people for developers to make money. WALSTREET FOR A EXAMPLE.	1
Stop spending my money. I can't afford it	1
Stop making Horace feel like west fargo/fargo! Let Horace be a small community!!	1
Stop growth and stop cookie cutter house building!	1
Stop approving developers plans, stop spending all the city money on fancy parks, stop buying city vehicles and wasting money, get rid of some the city personnel that do absolutely nothing.	1
Start taking care of what is already here before you create all of these elaborate plans on how to ruin this city further. Horace is is terrible place to live now because of all the growth. Start thinking of our kids and the teachers and how they are suffering because of huge class sizes! GET IT TOGETHER!!!	1
Splash pad, live music, farmers market	1
Spend the time to do this right. You have a clean slate right now and fixing mistakes later is costly.	1
Spacing do not crowd	1
Smart development, ability to coach the resisters to change, don't let the nay sayers stump progress and development, development Sheyenne as zoned and allow Enclave to develop as planned and keep managing the growth.	1
Sidewalks along 88th would be wonderful. The steep ditches on either side of the road are dangerous for pedestrians especially with the increase high speed traffic	1

Should be unique to the community and the history of Horace. Should be different than what has already been done in Fargo/West Fargo/ Moorhead. Important to new and old residents to maintain the small town rural feel of Horace, that's why we have chose to live and stay living in Horace.	1
Safe sidewalks to and from new developments (maple lakes, etc.)	1
Road improvements and sidewalks are a must!! more parking!! Need to get rid of the eye sores around Big Ervs and enhance our communities beauty. (Trailer parks and old grain bins) Could really discontinue the noon six and 9 o'clock whistle.	1
Remove the vacant buildings and promote new developments in those spaces.	1
Remove the trailer park off of main and make the houses off main clean up their back yard that is full of garbage that people see while driving through town	1
Remove the grain elevators, they are an eye sore.	1
Quit trying to put Comercial property next to housing on 1776. And the last question about how much our household income is, is non of your business.	1
Put in a community pool!	1
Put a huge fence up around trailer park or get rid of it, it is an eyesore.	1
promote commercial. stop special assessing your community. it doesn't work! Downtown isn't by the grain elevator. It is falling apart and will cost too much to maintain - its already too dangerous. downtown should be on main street by all of the new commercial prospects where its better developed for larger crowds. This area can be grown appropriately. the current "downtown" does not have parking or space to grow unless you force people out of their homes and that is unethical.	1
Prefer no retail Wide sidewalks and bike paths to and from West Fargo Keep the city a small bedroom community	1
Please slow the growth and make the growth that does happen make sense. It does not make sense to put commercial and high density housing on the north end of town where there currently in NOTHING. Fill in the open spaces in town that won't ruin our neighborhood before coming for us. (Sunnyside and 76th Ave)	1
Please please please don't try to be like Fargo. We continue to see crime rise in Fargo and don't want Horace going down that same path.	1
Please no public transportation.	1
Please make it a safe place for children and the disabled.	1

Please limit the population boom! Horace has grown too fast with too many small lot homes! Let's think about the future and keeping this community controlled and safe! Too many rental homes/apts will take away from our small community feel!	1
Please keep the community diverse, people wise, economic wise, and so forth. It makes me nervous when I hear things like "...that we want in Horace..." Who is we? Does it include the many people that live here and not just those with money or influence? I was once told I should vote for someone because "They know what we want for Horace." However, they did not know what I want for Horace, nor do I think they cared based on the person who told me.	1
Please keep in mind if we can afford to attend events. Everything expensive	1
Please don't strip the small town feel from this great little town.	1
Please don't fuck up the town	1
Please don't destroy our existing neighborhoods in your attempt to create the new. There are plenty of open areas to allow your high density neighborhoods. If you destroy existing neighborhoods in the process you lose the generational pride and sense of history in our town.	1
Please don't make this into a mini Fargo, with Fargo and West Fargo so close it would be nice to keep it as a small town with a small town feel. Too many housing developments going in with yard space that looks like "the projects" you can literally touch your neighbor. How about having some larger lot sizes with homes that aren't cookie cutter homes. Apartments have already shown up in Horace which take away from the small town feel. You've opened up the school to open enrollment which needs to end, it's already run out of space. Property taxes are also out of control in your efforts to build a mini Fargo. The Horace most people moved here for is disappearing. Sad.	1
Please don't make the Main Street a bunch of parking lots. Dilworth had done a great job with their downtown!	1
Please don't let Horace become another Fargo. Keep Horace the small town its residents are fitting so hard to keep. We love where we live and we'd hate to leave because the city only cared about making money off of developers and future business opportunities.	1
people should not be in the street	1
Parking seems to be limited to bring in any other new businesses	1
Open a grocery store!	1
Only build something if there are guaranteed tenants. Nothing worse than seeing buildings get put in and sit empty	1

Now is the time for investment, not savings	1
Non	1
No twin homes anywhere.	1
No more tax to residents	1
No more strip malls, we NEED a nice grocery store (not a big cash wise), trailer park needs some sprucing or relocating	1
No bus stops, less commercial chains and more local shopping and restaurants that are not main commercial retailers. Full time FD and PD. Fully stock all water features in the area, increase size of water features in the area.	1
Needs most are destination places and a greenhouse. Look at the new area in West dilworth as an example. Do that but bigger. Also need hotels	1
Needs gym	1
Need to clean up Main Street, fix and widen streets in town, get a grocery store	1
Need opportunities for grocery store. Would love to see a dog park or space for dogs to run available Community garden space would be a wonderful addition	1
Need more parking options, especially for businesses right on county road 17.	1
Need good restaurants and more gas stations.	1
Need better wide walking paths that connect in a way that makes sense. And a safe way to cross hwy 17. That is hopefully in the plans for sure. No crosswalks that are pedestrian controlled by push buttons is just not safe!	1
Need a grocery store and a community pool	1
Native plants for landscaping Pollinator gardens	1
Na	1
N/A	1
My husband and I come from small towns which is the reason we moved to Horace as we would like our kids to grow up in a smaller community. We also moved here because of the larger lot sizes and being able to put an additional garage or shop on our property. We would love for it to keep the small town feeling and sense of smaller community.	1

My family moved to Horace to get away from what you are considering what Horace should be.	1
Move loud events to a location away from Big Irv's. Tired of having my windows rattled until 1am.	1
Make sure to listen to the people and don't cater to a few that are in it for their own interests.	1
Make sure the kids can ride their mini bikes, 4 wheelers, mopeds, electric bikes, electric scooters etc on the approved routes and roads. Make sure the grumpy old people dont stop this from happening... let them be kids!!	1
Make it unique, if it is unique people will want to be here and less tax incentives for businesses. It's already an attractive place to be.	1
Make it humble but interactive, small but creative, the value is in the reservation and exclusiveness	1
Make downtown wall street and main. Stop building homes on top of eachother w little property. No more apartments. Keep Horace Rural!	1
Make convenience things a priority, for example, a small grocery store Work towards locally owned businesses	1
Make an actual downtown area. Right now there doesn't feel like there is one. Not growing up in Horace, my view of "downtown" is the development north of Wall Ave.. its actually bustling and having companies build. Please make this area feel more like downtown instead of not feeling like we have any downtown in Horace	1
Let our city grow. The people who did their research moved here for a new city.	1
Let Horace be a small quiet community. We are not west fgo nor fargo n prefer it that way	1
Less modern shopping, more attractive unique small businesses and boutiques and restaurants. PLEASE TRADER JOES OR WHOLE FOODS	1
Leave the residence out of your plans.	1
Leave the elevator alone it's private property. Should be more development on the north side of Horace.	1
Leave original Horace alone. Tear the elevator down and be done. All the new gather places can be built to the north in the new developments	1

Leave it the hell alone and don't put out a map of what you think people want. I don't care if it's "concept" or not, it's absolutely disgusting to see a plan with new buildings where people currently live and have no plans of moving. The people here are why you have a job here, let's start putting them first and be done with the crap.	1
Leave it small, we don't need the bullshit that comes with trying to make it like Fargo/moorhead	1
Leave it be, we don't need a downtown we are a small town let's stay that way! People moved here for what it was not what yall are trying to change it too!	1
Keep the small town feel, put a cap on how many new houses are built. Stop developers from claiming domain on the farmers around here	1
Keep the small town feel and keep golf carts and OHVs out of the equation. Stop catering to people that want it to be mini-Fargo.	1
Keep the quiet and small town charm at the forefront of the decisions. People have moved here to get away from the tight and overpopulated neighborhoods and we enjoy nature and outdoor life. Part of the fun is the ability to ride golf carts and 4 wheelers around.	1
Keep the elevator. Allow the homes along main ave to stay.	1
Keep the elevator and the whistle	1
Keep the commercial buildings, apartments and mixed used properties south of 76 Ave.	1
Keep snowmobiling and Off road usage	1
Keep our small town feel.	1
Keep main street where it is!!!!	1
Keep it unique.	1
Keep it small town vibes or everyone is gonna move	1
Keep it small town feeling!	1
Keep it small bedroom town feel... and if commercial is needed put it on the East side of County Road 17(by schools and apartments)	1
Keep it simple. Do not turn Horace into Fargo/West Fargo.	1
Keep it simple.	1

Keep it simple and true to small town North Dakota	1
keep it a small town with one acre plus lots, if people want tiny houses go to fargo or west fargo.	1
Keep downtown in downtown. We are a bedroom community with all we need in West Fargo. Less multi housing more suburban.	1
Keep Downtown from encroaching on established larger residential lots - it's not a good fit.	1
Keep commercial in proper areas not in the back yards of established neighborhoods	1
Just pause and reset. Let the current dust settle and go back to the table. The current first come first serve perception of decision making is killing most any ability to get this right. Engage with the longtime residents and you'll find many answers about what Horace has and should be about. Or engage with the those residents who came out to escape the city life. Start actually listening and not to who's pockets the city is filling. Contractors/Developers should be responsible for the cost they bring businesses or homes into, NOT residents!	1
Just do not turn it into a mixed use mess. Keep the tradition of a small town. We don't need condos, apartments or high density housing.	1
its time to let the elevators go! the small town feel has been destroyed in the last 7 years, this is just another suburb now. the hometown feel of knowing your neighbors and seeing people you know at the Sheyenne bar are gone. they have been replaced with cookie cutter houses all packed together and people trying to get rich by passing all the risk and expense on to someone else. get rid of the hazard that is the elevator buildings and try to attract some type of commercial. My wife has lived here her entire life and with the current way the city is run and being developed we cant move out of here soon enough.	1
It's important to grow and prosper while keeping horace a small community. Many of us moved here because of the small town feel and I would like to see renovations done on the dinosaur park and more updates to already existing areas in town like Main Street, some of the existing parks	1
It would be nice to have a c store.	1
It would be great to have the elevator torn down and have a veterans memorial. It would be a quiet and respected space. I don't want large, loud gathering spot in the middle of older homes.	1
It needs to be cleaned up	1

It is idiotic to think businesses will move to Horace. We are poorly laid out and the drive to get there is less than ideal. Poor planning has led to the knee jerk reaction that we "need" or can "support" commercial business. Likely the business will last for a short period of time and then close. We are unable to support these idiotic ideas.	1
In general we need more bike paths. Horace is surrounded by rural fast roads where developments are being erected	1
I'm from Park Rapids, MN so look at their Main St for example for a small town. It still remains as just specialty shops and local businesses. They have not needed to modernize it still to this day. Everyone loves it because of this.	1
If you want to expand and make new spots to build. Make bigger lots like the Sunnyside area of Horace.	1
If we want to go to a destination shop, we will drive 10 minutes to Fargo..... we want peace and quiet	1
I'd really like to keep the small town feel overall with a select option of a few new businesses.	1
I'd love to see downtown Horace provide all the commercial space needed for the entire town along county road 17 and avoid bringing commercial spaces off of the main road and into neighborhoods.	1
I'd like for Horace to have a public pool and a grocery store.	1
I'd love to see a brewery. I think we should limit the growth and scale back.	1
I would love to see the elevator stay. It tells a story about small town Horace. I used to drive my Dads grain truck there during harvest when I was a kid.	1
I would love to see the biking paths continue to be developed.	1
I would love to see new small businesses, but large corporations don't fit the feeling.	1
I would love to see Horace incorporate some of the characters of Blue Zones. Be very walkable, connect the bike paths and stop the use of golf carts/motorized vehicles on the bike paths. Have more community gathering places and events that don't revolve around the bar.	1
I would love a grocery store. With Sanford and the Medicine Shoppe pharmacy we are so close to being a one stop shop.	1
I would like to see a small grocery store and a small hardware store, like an Ace Hardware.	1
I would like a grocery store and some fast food options like pizza delivery and a taco place	1

I view downtown Horace as the elevator. The trailer parks near there are usightly and need to be addresssed. I understand there are people with low income/low income housing but most of those trailers look condemned or as if they should be condemned.	1
I think we have a great opportunity to innovate and learn from Fargo/WF/Moorhead. We can do a better job innovating what small town life is like and transforming what Rural America can accomplish to create a great sense of community without losing the charm, defaulting to tourism, or just "waiting till the boomers go". We got this!	1
I think there is huge potential to serve our community	1
I think the hardest part about your downtown vision is Horace has never really had a downtown. The most they have had is the bar and the old convenient store where the fire station is now. How do you plan to bring a downtown area into a fully developed area. The other area of concern is lack of streets running N-S in the city. There's currently only 1 roadway that could take you thru town to WF/52nd ave so there's a lack of options for people that want to avoid downtown currently. The other challenge I see, especially with where I identified as downtown, is if you want a multi-modal system for the city there's not going to be anyplace for people to park. You'd need a parking garage, which is fine, but where do you put that when everything else is fully developed. In addition what other businesses could you install in the area, since I consider downtown like 1.5 blocks so you'd likely need to buyout homes to install new businesses to expand the downtown setting.	1
I think the city of Horace needs a pool before downtown is revamped.	1
I think getting local businesses, whether that's restaurants or shopping, would be great opportunities. I do think we need to ensure that new construction fits the town's identity and that we don't start making ourselves just another part of Fargo. Keep us unique instead of making our buildings look the same as every other suburban community	1
I think a plaza would be fantastic, but it needs to be in a new development. Repurposing the elevator property and creating a gathering area around a fire station, which wasn't designed with different access routes, will hinder emergency services. The Fire Department had a working structure fire on Bean Days. county 17 access was shut down, which increased our response time by at least 1.5 minutes. This is a large amount of time when a house is on fire or there is a medical emergency. The fire department was designed to always have immediate access on county 17 and center Avenue. Officials need to meet with emergency services and learn what works and what doesn't. A meeting with Chief Mahoney and Sheriff Jahner is imperative!	1
I Strongly support the continued use of golf carts, UTVs, and snowmobiles within city limits.	1
I prefer to not see businesses with apartments above them that might hinder the privacy into yards of homes that have appreciated their space for years	1

I love the idea of a Downtown in Horace. We would LOVE to see new businesses flourish and hope there will be many, especially small businesses and not mostly huge chains. We also would love a spot in Horace we could spend hours walking with our child and bopping in and out of places, especially if the area is aesthetically appealing. We really love this community and are pleased with the growth so far and are excited for what's to come. We are also so excited to raise our child here, and hopefully more one day, we think it's a perfect place for him.

I live on what I consider downtown Horace. I don't want a bunch of businesses in a residential area. You better not be thinking of ways of forcing us off our property. If you mean where Lakeside is going to be and the shops like Dairy Queen, that's different.

I live near "Downtown Horace".. try and maintain city grass/drains in existing parts of Horace, instead of catering to new developments. I have city ditches that don't drain/you guys don't mow the grass AT ALL on the city land surrounding my property. Ridiculous. I know if I didn't maintain my yard, you would send me a complaint and a bill. Do I get paid for mowing grass that belongs to the City of Horace? NO.

I like the path that it's going but def needs a facelift and more business/access

I know it's beneficial to have them around, but I don't want Horace to just be a hub for steel sided industrial business. I want there to be places that are close and accessible that are actually businesses you want to visit. We need some additional amenities for convenience. A brewery or taproom with outdoor space would be awesome. If we invest in bike paths to safely connect our neighborhoods we can get our kids outside and enjoying our town. Also a small town pool would be great

I just don't want the population density getting out of hand

I find it hard to answer questions about Downtown Horace when there really isn't one.

I don't want a "Downtown" in Horace. Let's maintain our current city streets better. Remove the elevator. Make a walking path behind the elevator that follows the old railroad tracks. You could add benches and landscaping to make it a nice walking path. I think we need a smaller grocery store, a few small businesses, a swimming pool, and an event center. I don't really want to have all the noise that comes with a downtown. I miss the quietness of Horace. The quiet has been replaced with construction equipment, traffic, more emergency sirens, etc.

I don't think CR 17 is a good fit for any downtown establishment, the downtown should be an intersecting road so as not to disrupt all the heavy traffic that relies on CR 17 to get through town.

I can see the downtown area of Horace not only expanding but being the true heart of our small town. I see a big desire to bring in businesses and homes, but trying to keep the small town feel is extremely important. It should be a place where you feel safe with your kids running around without street racers - keep the law enforcement presence. Don't change downtown in a way that makes it inaccessible, but a way that could create a great "walking around" space - alternative route(s) for drivers going North & South on 17.	1
I am unable to drive myself, due to medical conditions. Recently I was told the only form of transportation in Horace was unable to help me, as I am not old enough to ride.	1
Horace is out of the barn on having a traditional downtown since business is filling in along the highway to the north and the current Main Street is occupied. A new town center would need to be developed with the area southeast of Casey's being the logical location.	1
Horace should have skyscrapers like downtown Fargo. Big buildings will attract more revenue for city coffers	1
Horace needs more trailer park involvement	1
Horace needs a voice. A small town with a big presence. Beautiful places to live and visit to encourage interaction and to draw businesses. This will require a lot of change, which will be expensive. Density is needed to create a core neighborhood which Horace currently lacks. People may oppose this but the alternative is that everyone goes to West Fargo or Fargo for their fun or business.	1
Horace needs a downtown of some sort. There must be a core area giving Horace an idea as a city albeit small. Horace's population will make it one of the bigger communities in the state. A downtown center is critical. Horace should not be a mere bedroom community. Look to Dilworth's rail district or the proposed but unrealized Lakeview project on 17 and Wall. Confining business to metal buildings and strip malls is short sighted. A downtown area must be planned now as an anchor to guide the development of commercial space.	1
Horace is a bedroom community that was quiet and convenient to Fargo or West Fargo and was the reason we moved there 20 plus years ago. The new direction with all the high-density housing, increasing travel time and proposed commercial and mixed-use placement is turning the area into what many of us moved away from. Growth is expected just not at the pace and density currently. When our development joined Horace and our address went from Fargo to Horace our expenses increased our benefits did not. Insurance went up considerably because of a volunteer fire department.	1
Horace has lacked a downtown so hard to know what the proper vision is for a bedroom community. Sustainability would be great to serve the residence living there to have some small town resources.	1

Horace doesn't have what I would consider a traditional downtown. Locally owned business would be great. Mom & pop shops & scratch kitchens.	1
Horace doesn't need to be buzzing all week/year round, having events, farmers markets and/or unique boutique like shops is enough action without messing with the quiet small town feel.	1
Horace does not need a downtown. If you want that go to West Fargo or Fargo.	1
Having a more central focal point for the town. Whether it be on Main Street or somewhere else that would be the go to for the community.	1
Growth should be slowed to allow acclimation for new residents	1
Growth is coming. As long as the sense of community and areas were things like this past weeks beautiful days can happen in more park like town center rather than spread all over.	1
Growth is apparently inevitable, but the rate at which we see growth is too much. I would think most people came out here for small town life, not far out of town. Unfortunately that feeling seems to be slipping further and further away and I would love to see a downtown plan that keeps small town life at the forefront versus modern retail.	1
Good environment Downtown will Attract lot of businesses and people	1
Get the pond fountains working in south dale farms	1
Get rid of the trailers houses that are on the road. There should be an easement requirement that makes this eligible.	1
Get rid of the trailer courts on each side of the road, south of Big Ervs. This would be a great place to expand on our current traditional downtown. The permanent homes would blend in to downtown structures just fine. I would love to see some small shops, while preserving the "Old Horace" feel. I'd like a better option to be able to walk or ride bike from south Horace to downtown.	1
Get rid of the stink first. City already spent money on SUVs that rarely get used. That money could've been used to get rid of the stink from the crap pond.	1
Get rid of the snowmobiles and dirt bikes in town	1
Gathering spaces and side walks	1
For commercial businesses, we would like a small grocery store and a small Ace Hardware store.	1
Focus on offering something different from suburban Fargo.	1

Focus on keeping people there it's not a small rural town anymore it's a city so make it a city you can't have both you guys keep up snow removal work on maintenance as you grow	1
Fix what's broke before raising taxes on new projects	1
Filling out this survey was a waste of my time because you guys that have final says in everything have already made up your mind to make this Fargo south south and this was for formality. Absolutely 100% regret moving here. When our families from out of town come to visit the comment and say house stupid all the cookie cutter houses look	1
Excited to see what Horace looks like in the future. Need a meat market and a grocery store.	1
Encourage a grocery store or a Dollar General to open up in Horace, but otherwise, leave the urban clutter in Fargo/WF. We can drive to the clutter and then return to the peace and beauty of Horace.	1
Don't overbuild or build any strip malls	1
Don't give TIF to greedy developers for this project.	1
Don't forget us senior citizens when making decisions. Cost and accessibility are very important to us.	1
Don't bring in so many businesses that need to keep expanding and moving	1
Don't put any apartment buildings	1
Don't overdo it keep as is	1
Don't need a downtown like Fargo. Just some simple eating spots other than bars - like Subway - Dollar Store - or a sandwich store	1
DO NOTHING! This is a private commercial issue now a city issue. Stop jacking up special assessments because you don't know how to budget taxes. As a citizen I had my specials raised to a point I might have to move!	1
DO NOT turn us into Fargo or West Fargo!!! I can not express that enough. ZERO apartments!!!!	1
Do not remove the grain elevator. It's a big part of history for everyone who has lived here and will upset people who have been here for a long time.	1
Do not buy elevator property till they are gone and cleaned up	1

Do a better job with urban sprawl. You cannot throw condensed housing behind a neighborhood with acre or more lots. Most of us who have moved to Horace 10 years ago or longer moved here because of the small town atmosphere and the want to get out of West Fargo and Fargo. What is becoming is what I wanted to get away from.	1
Definitely not into McDonald's or other such businesses. Make it easy/affordable for local residents to open business/get permits. Stop building apartments.	1
Currently there is no appeal to "downtown" Horace. I feel that there is great potential for an attractive downtown atmosphere at the intersection of Wall Ave/Co. Rd 17. It'd be nice incorporating the small town feel of repurposing the grain elevators with additional new businesses along the intersection.	1
Clean up the trailer courts and homes that are eye sores.	1
Clean out the eye sores of Main Street and keep the Main Street feel. Quit taxing and adding specials to the home owners	1
Clean it up.	1
City maintenance needs improved. When we have trees growing out of ditches it's a sad sight. I think having a community area similar to the lights is fine but need better ways for people to get there. Clean up sidewalks, ditches and roads before adding more. We need a better police presence. How can we incorporate "Flock" or something similar to stop drinking and driving and speeding before it gets even more out of control. We need to get it resolved now before the town gets even bigger.	1
Challenges- parking	1
Challenges are the ever increasing debt load to finance all of the new developments. We have so many parks that the tax burden is too high(\$1000/ per year for our property). Special assessments are forcing us to think about moving if the new policy is implemented with the multiple number per acre is used.	1
Challenge - avoiding bring in issues that are seen daily in downtown fargo The more family driven/ oriented the community becomes, the more growth you and opportunities Horace will have	1
Car atv golf cart	1
Can we buyout the trailer parks and relocate.	1
Building mix use building like Fargo has are going to result in same problem Fargo has they won't be filled. We already have an unfilled strip mall. Statically apartments degrade in both appeal and the residents inside them.	1

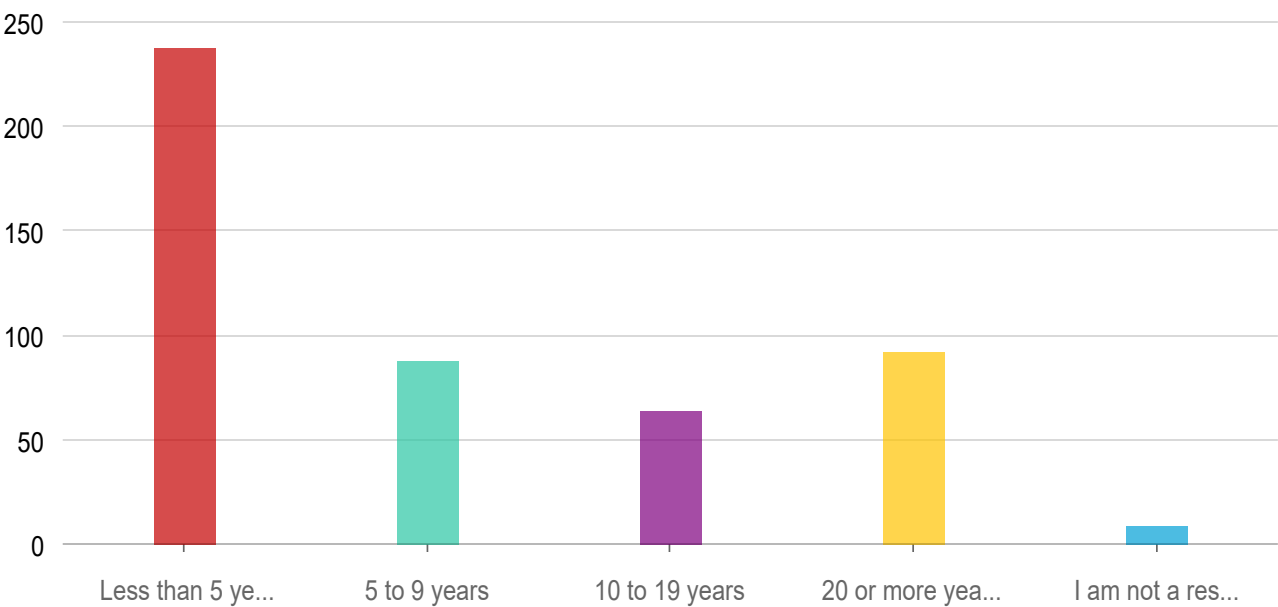
Building a grocery store	1
Build a grocery store	1
Bring something unique! It needs to be redone. Relocate trailer parks and industrial business	1
Bring in Grocery store	1
Bring in a grocery store and other meaningful businesses before Fargo puts it on their side line border ing Horace and they take all of the sales tax from it. Spend money in your own community to help it grow.	1
Be unique! Don't try to copy West Fargo or Fargo! If people want that they can move or drive to those locations! It's such a regret moving here. We filled out the 2045 survey and the "leaders" did and approved everything that the survey indicated the people didn't want!! No one wants to look at cookie cutter houses with 20 foot lots or big apartment buildings or strip malls. Again if people want that move back to Fargo. There is plenty of that stuff there already. Horace needs their own identity but greed appears to be the most important to the "leaders"	1
Based upon the current layout and existing/ongoing development of Horace, to me it seems to me most people are going to be of the position to say that their mode of travel would be by auto. The town currently is long or going long along Sheyenne/County 17 with width occurring at Wall Street and south it seems. Additionally without getting a number of business (banks, grocery store, hardware store, and possibly some other offerings) people will need to travel to get immediate needs (if not immediately needed they will use Amazon).	1
Any future buildings and businesses should match the small town vibe and charm of Horace.	1
Allow residents to have chickens	1
Affordable housing need not apply, that can be reserved for Fargo and West Fargo. With the high taxes we have, we need to keep the community high end.	1
A traditional downtown main road would fit Horace very well but that style is very much on the way out since people heavily rely on personal vehicles to get around. I would love something walkable with shops and restaurants but parking and roads definitely need to be taken into consideration	1
A grocery store would be amazing!	1
A grocery store is a must to keep people here for shopping needs. It's hard coming home, then needing to go another 20min out of town for essentials.	1

Since there is no way my feelings will halt the growth, take a page out of Boulder and Pearl Street. There was a fantastic variety and it was incredibly clean.

I appreciate that share use paths have been installed along the highway, and I hope that we continue to keep on installing more pedestrians facilities.

Answered: 258 Skipped: 244

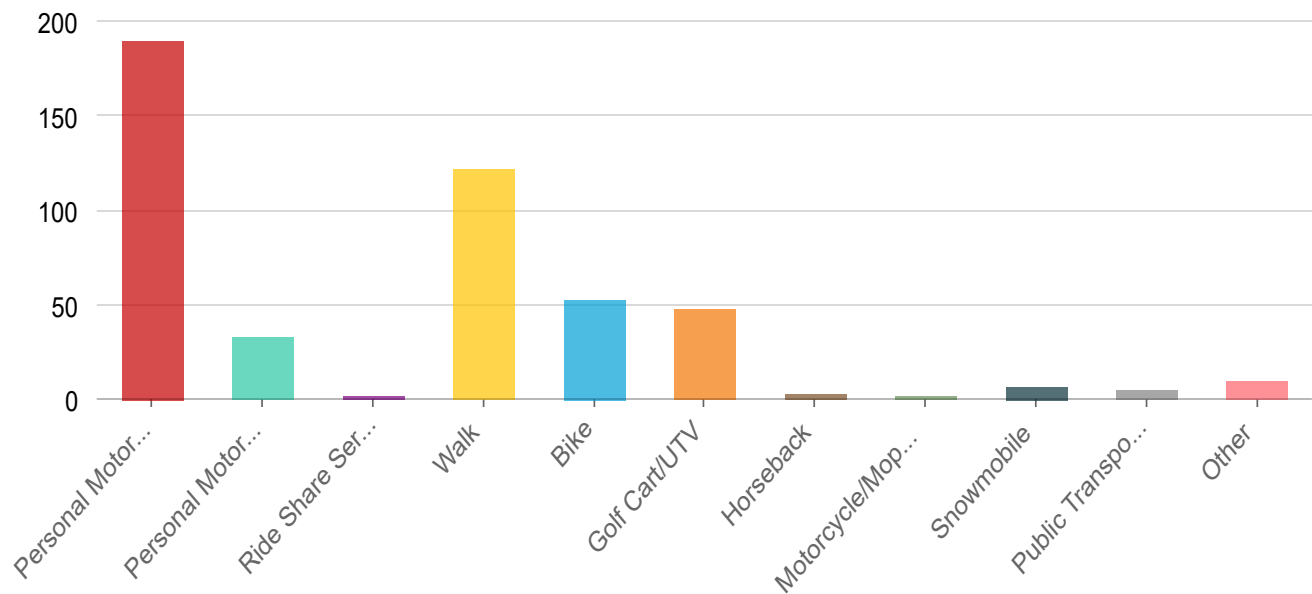
How long have you lived in Horace?



Answers	Count	Percentage
Less than 5 years	238	47.41%
5 to 9 years	88	17.53%
10 to 19 years	64	12.75%
20 or more years	92	18.33%
I am not a resident of Horace	9	1.79%

Answered: 491 Skipped: 11

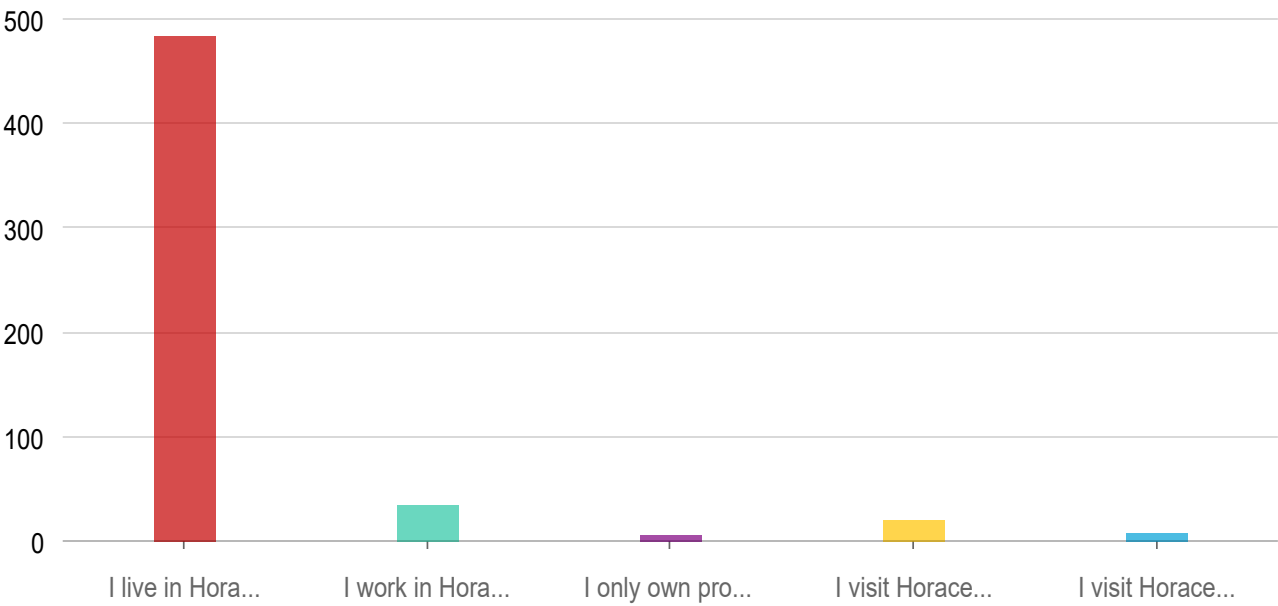
What mode of travel would you prefer to use when visiting Downtown Horace...



Answers	Count	Percentage
Personal Motor Vehicle - Travel Alone	190	37.85%
Personal Motor Vehicle - Carpool	33	6.57%
Ride Share Service	2	0.4%
Walk	122	24.3%
Bike	53	10.56%
Golf Cart/UTV	48	9.56%
Horseback	3	0.6%
Motorcycle/Moped	2	0.4%
Snowmobile	7	1.39%
Public Transportation	5	1%
Other	10	1.99%

Answered: 475 Skipped: 27

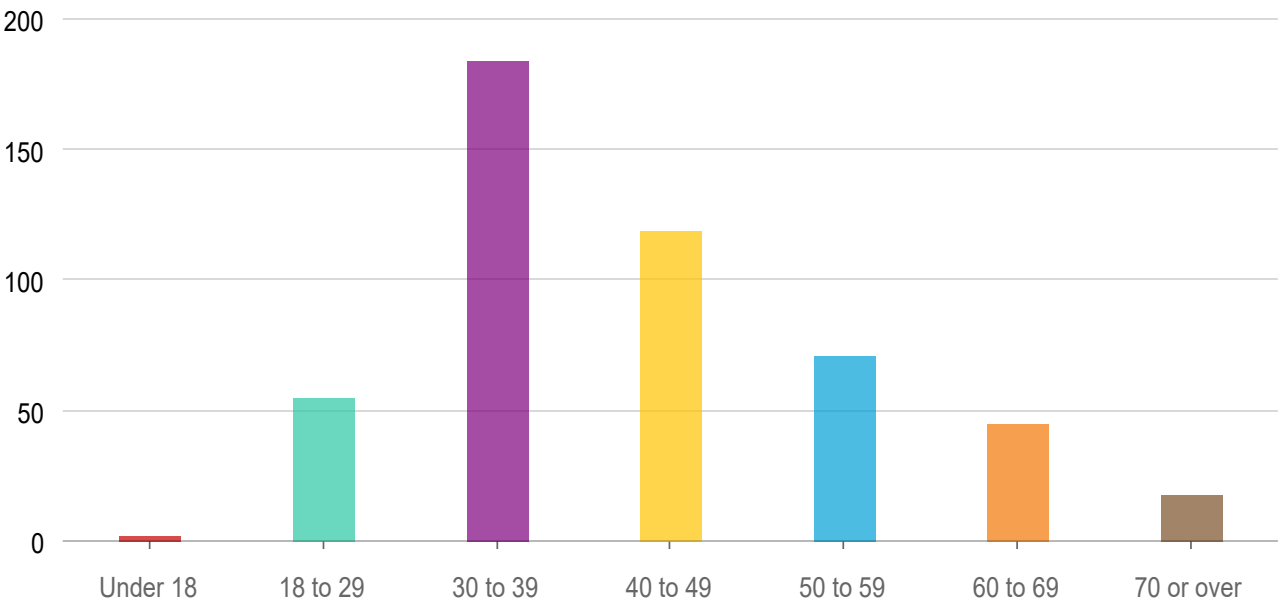
What is your connection to Horace?



Answers	Count	Percentage
I live in Horace	484	96.41%
I work in Horace	35	6.97%
I only own property in Horace and do not live or work there	6	1.2%
I visit Horace Regularly	21	4.18%
I visit Horace occasionally	8	1.59%

Answered: 500 Skipped: 2

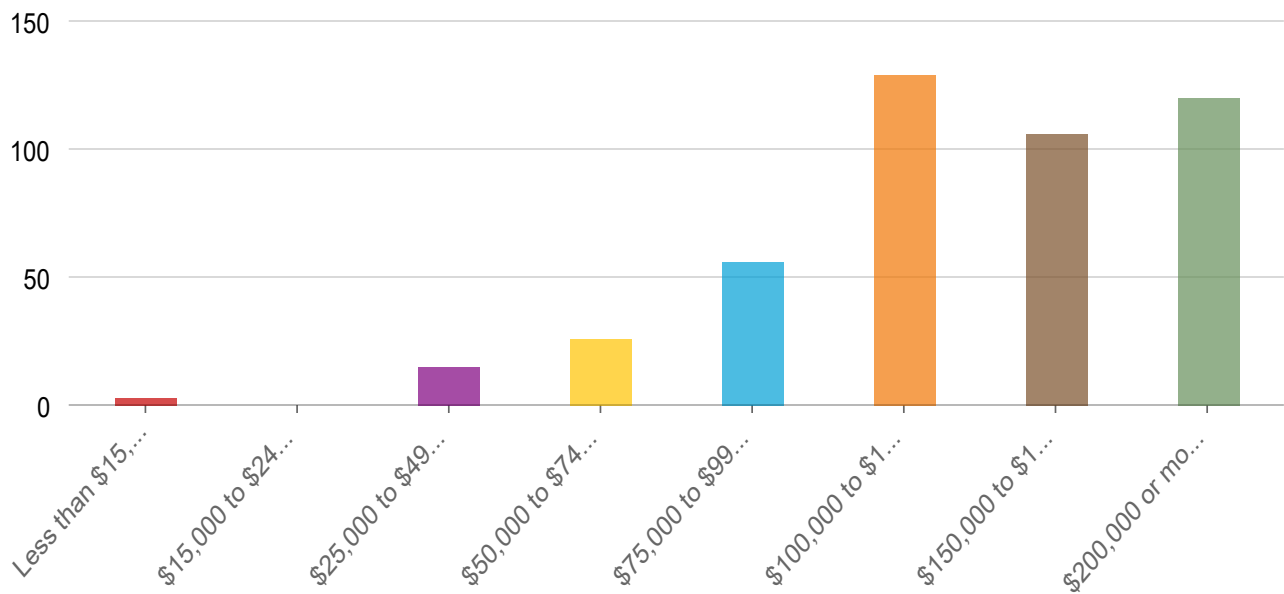
What is your age?



Answers	Count	Percentage
Under 18	2	0.4%
18 to 29	55	10.96%
30 to 39	184	36.65%
40 to 49	119	23.71%
50 to 59	71	14.14%
60 to 69	45	8.96%
70 or over	18	3.59%

Answered: 494 Skipped: 8

What is the total income of all wage earners in your household?



Answered: 455 Skipped: 47

Would you like to stay up to date on the Horace Downtown Neighborhood...



Answered: 471 Skipped: 31

[illegible]

Response	Count
Danielle	4
Matt	3
Sharon Arnold	2
Nicole	2
Mike	2
Maria	2
Kristin	2
Jen	2
Erin Sarbaum	2
Zachary	1
Zach Kubas	1
Wade Frank	1
Vanita Vik-Ohlgren	1
Tyler Schumann	1
Trish Sorenson	1
Trent	1
Tracy Beecher	1
Tracy	1
Traci	1
Toni Brossart	1
TJ	1
Timothy Russell	1

Tim Kreft	1
Tiffany	1
Terry Heiden	1
Taylor Wolf	1
Tasha	1
Tara	1
Tammy Helweg	1
T	1
Susan Condry	1
Steve Becher	1
Steve	1
Stefanie Berger	1
Stefanie Anderson	1
Sherri Smith	1
Shelby Helgoe	1
Shaun	1
Shannon Kist	1
Shannon	1
Seth Ehlert	1
SD	1
Scott Jacobson	1
Scott Bauer	1
Sarah Veit	1

Sarah Pederson	1
Sarah	1
Sara McKay	1
Sara	1
Samuel Ihrke	1
Samantha Brown	1
Samantha	1
Ryan	1
Ross Renner	1
Roshan	1
Robert Goerts	1
Rhea Block	1
Rebecca fee	1
Rachel Gierszewski	1
RachealAnn Johnson	1
Rachael Danielson	1
Philip	1
Pete	1
Peggy Hill	1
Pam Gaulke	1
Orson Opheim	1
Nicole Bredahl	1
Nathan Kramm	1

Nathan	1
Nate Bertelsen	1
Nancy Johnson	1
Mycarey217@hotmail.com	1
Morgan Remer	1
Mike McNamara	1
Michael Frank	1
Michael Collins	1
Melissa Torres	1
Melanie Zander	1
Megan Zahradka	1
Max	1
Mattie Merritt	1
Matthew walker	1
Matthew Kittelson	1
Marshall Sonsteby	1
Mark Woodrow	1
Mark Quenette	1
Mark Gusaas	1
Mark Benolken	1
Mark and Nancy Hansen	1
Mark	1
Marj Meester	1

Marissa	1
Marie Tanner	1
Marie Laufenberg	1
Maria Corneillie	1
Lynn Larson	1
Lyndsay Coulombe	1
Lukas Zahradka	1
Lisa	1
Lindsay Darrah	1
Lexy	1
Laura Holzwarth	1
Laura Bucholz	1
Larry Geffre	1
Krystal Anderson	1
Kristina	1
Kristie Thone	1
Kristi Erickson	1
Kristen	1
Kory Brown	1
Korey Stamp	1
Kirsten Day	1
Kirk Carlson	1
Kimberly Gaalswyk	1

Kimberly Bertelsen	1
Kim Soderholm	1
Kim Hurley	1
Kiersten Henderson	1
Kevin Hanson	1
Kenny Scott	1
Kenneth Tappy	1
Kendra Lange	1
Kelsey Mehus	1
Kelly Blank	1
Kayla Powell	1
Kayla Estepp	1
Katy Dobberstein	1
Katina	1
Katie Siegert	1
Katie	1
Kathy Schroeder	1
Kathy Haakenson	1
Katelyn Daul	1
Kasey mccann	1
Karissa Hansen	1
Karen Kahler	1
Kait Dahl	1

Julia Mahoney	1
Josh Remer	1
Jordan	1
Jon Carlson	1
John Evans	1
Jody French	1
Jodi Buzick	1
Jo Anne Liston	1
Jim Kasper	1
Jessina Mohl	1
Jesse Koenig	1
Jess	1
Jenna Elgin	1
Jena	1
Jen Quenette	1
Jayson Metzger	1
Jay Cournia	1
Jay	1
Jason Trainer	1
Jason	1
Jafrah	1
Jack Dwyer	1
Jaci Fritz	1

Holly	1
Heather Kranitz	1
Heather Grandstrand	1
Heather	1
Gwen. Dahlen	1
Gus	1
Grave	1
Erin	1
Ericka Urness	1
Erica Johnson	1
Eric Dahl	1
Eric	1
Emma Bernath	1
Elliot okland	1
Elizabeth	1
Elisa	1
Dylan Henderson	1
Dwight Spear	1
Diane Hahn	1
DIANA STECKLER	1
Dee	1
Deb Matzke	1
Dawn	1

Danielle Nelson	1
Daniel	1
Dan	1
Curtis Wetzstein	1
Crystal Baukol	1
Creighton	1
Craig Blumenshine	1
Cory	1
Corin Bragstad	1
Connie	1
Conner Reitmeier	1
Cole Mercier	1
Cody Nelson	1
Cindy Peterson	1
Cindy Norberg	1
Christopher Bellmore	1
Christine	1
Chris Halvorson	1
Chris	1
Charles	1
Chantelle	1
Cathy Holmen	1
Carrie Speer	1

Carla Janecky	1
Cami	1
Cam Bastian	1
Bryce Peltier	1
Brittany Pavek	1
Brittany Mark	1
Brittani Ash	1
Brian Splichal	1
Brent Borge	1
Brandy Dockter	1
Brandon Ringsaker	1
Brandon Ottt	1
Beth	1
Benjamin Holland	1
Becky	1
Barry and Terri Olson	1
Barb	1
Bailey Fletcher	1
Autumn Hareland	1
Austin	1
Aspen Iverson	1
April Vetsch	1
Anne	1

Anna Rossow	1
Ann Baumeister	1
Angela matzke	1
Angela Cross	1
Angela	1
Amy	1
Amie Chang	1
Amanda Kueber	1
Amanda Koplin	1
Amanda	1
Alyssa	1
Alissa	1
Alicia winmill	1
Ajalie savageau	1
Addie Wohler	1
Abbie Koenig	1
Aaron Stinton	1
A	1
Charlie	1

Answered: 260 Skipped: 242

Stakeholder Meeting – Al Aamodt

Attendees: Jace Hellman (City of Horace), Al Aamodt (Resident, Special Assessment Commission), Luke Champa (SRF)

AA: State special assessment case from Grand Forks

- Special assessments impacting Lost River mortgage/property tax payments significantly
- Reestablishing cultural center in Horace is a huge opportunity

AA: Moorhead redeveloped and tore out their historic buildings downtown, Fargo preserved, Horace's downtown feels sort of laughable

JH: Want to redevelop in a way that fits. See downtown as Main Street because Horace doesn't have historic buildings

AA: what are we going to do for Bean Days next year. Car show was moved to school, sense of community is eroding downtown

- It's one mile from house to the post office
- Grand daughters like to walk to Dairy Queen
- Walking paths on CR 17 are great
- Trailer Court houses south of Fire Hall
 - Is there a plan for them?

JH: 5-year schedule – not to push people out – piece things together slowly over time – Walking paths, no facilities in older parts of town, provide connectivity across town for everyone.

AA: concern about strip malls

- Some on 45th St not that far away which are open but nothing in there, no tenants
- Vacancy rates of commercial spaces
- Elevators are ok, cool. But what are you going to do with them. Fire hazard of converted agricultural buildings – is it even safe to convert
- Comstock Elevator burnt
- Horace elevator burnt a while back –
- Young people may not even know or understand what the buildings are
- Safety concerns and large properties
- Metro Flood Diversion Authority owned

AA: short end of the stick with the MFDA

AA: moved to Horace in 1979 (July)

- Friend built custom designs, he was a craftsman
- Moved after the major flood in 1975
 - Population was 469
 - Lived in Norway for some time
 - Participated in the All City Band and Choir, concerts and performance
 - Worked in the News business for 50 years
- Was a News director
- Had opportunity to move to Minneapolis about 4 years ago
- Worked at WDAY for 35-years
- Taught at Moorhead State

AA: started civic involvement when he moved out to Horace

- The 'dry river' was a mess behind his house
- Considered a drainage easement
- Worked with neighbor to clean it up
 - Pea gravel, drain tile, sump pump, drain a lot of the neighboring properties between their setup.
 - Cleaned it up and was treated terribly by city council at the time
 - Stretched thin, did the best they could
 - Pump gets replaced every year

AA: special assessment commission now

- Has learned a lot
- A lot more to learn
- Personal goal to get rid of specials
- Problem is the city is encouraging land speculation, developers have no skin in the game.
- Remembers when Fargo got special assessments too.
- Not anti-development but thinks eliminating special assessments for residential development is a good idea
- Understands why businesses might need special assessments for special infrastructure etc.
- Need to get serious about special assessment burden on property owners – where are people going to go?

JH: developers seem to believe that the market would simply stop. Challenges that notion the market couldn't support eliminating special assessments.

AA: Southwood Addition lot frontage = 80'

- Modest homes, nice lots, a lot of work to keep up, beautiful.
- New homes east of CR 17 have 40' lot frontage – looks like one step above the trailer park.

- Narrow street
- Brooks Harbor skinny streets can only park on one side of the road
- Up north homes have 2 cars and small garage, toys end up in driveways. Sheyenne Drive looks dumpy.
- Concerned with the look of town – trailer park and southdale

AA: Moorhead used to have a housing program for people to get into their first home, 2nd home, etc.

JH: there is definitely an affordability issue – which makes it harder to get the product people want and also construct the infrastructure associated with development.

AA: knows a friend who listed home recently for \$430,00. Sat for a while and realtor assessed people didn't want the dated kitchen appliances and non-trendy cabinet color. Ended up selling for \$350,000.

JH: southdale lessons learned – connectivity – better products

AA: Arrowwood layout street network

- Interest rates are only an excuse but certainly make things stagnant. It is a challenge
- Things are costing more than the drawings
- Would love to see more businesses that benefit the community
- Show what people get out of it, how does it benefit the community?
- Sanford, dentist, essential services
- Would love to even see an oil/lube business or car maintenance services

JH: used to have a few backyard businesses but none exist anymore

AA: thought there was a shop by Taco Revolution

- Need more convenience services nearby
- Used to be a grocery store business that used to deliver to Horace and other small towns south of Fargo
- Turnover of grocery store ended that operation
- Convenience stores are nice but Casey's might not last much longer – not enough help
- Grocery store would be great but would have to be a smaller store setup more for convenience items – inventory challenges with larger stores
- There was talk of a Super Target near 45th St and 52nd Avenue S. area
- People are easily commuting into S Fargo for groceries

AA: DT Neighborhood plan needs to sell it show what's in it for people – show the community benefit

- Benefit cost ratio?
- Would love to see a community pool or something – but what can we support?
- Fargo Park District facility proximity might be sell to the community
- Lakeview Bandshell
- Serious marketing – older residents are on a fixed income

AA: would love to see a pool or something to unite the community around, etc.

- Who builds a pool
- Who builds a grocery store
- Strong churches in Horace
- Back in the day black topped the street by petitioning neighbors and then took to city council – got street black topped
- People from around town would come drive on the street afterwards
- Not too long after City paved all city streets. Did something for themselves, grassroots efforts
- Getting people involved is hard, responsibility is hard to communicate

AA: sees Horace becoming the haves and havenots

- Older people and population almost feel expendable based on political climate

JH: no intent to leave anyone out to dry

AA: lifespans are changing – people want to be at home long-term

- Property taxes, special assessments could make it harder to stay in their home long-term

JH: recommendations need to fit into areas, for example no specials in older parts of town, south and northwood neighborhoods have very limited traffic. Creative solutions to pay for infrastructure.

AA: some areas may need sidewalks, some may not

AA: what would you do with Elevators?

JH: no personal attachment – if were to keep them, mitigation would be expensive

- Only landmark in town
- Community art space for mural or park space
- Benefit in that regard
- Cost to keep may not be worth it
- Some value to them, gathering space but leaning toward unrealistic to preserve or reuse

AA: question elevators being a major part of the community identity, question their value – community focused Horace skyline

- Turkeys everywhere
- Disappeared for a while but now back
- Someone brought them here
- Nuisance
- Geese everywhere in Cub Creek
- Other examples – Warroad, New York Mills
 - Marvin windows working hard to bolster Warroad's downtown
 - Harrisburg, SD

1. From engineering and public works perspective, what are the pros and cons of newer neighborhoods and older neighborhoods? From an engineering/public works perspective.
2. What are the most expensive components of new infrastructure? (i.e. for new neighborhoods)
3. What does your ideal downtown street scape and cross section look like?
4. What opportunities do you foresee for the Downtown Neighborhood Plan?
5. What challenges do you foresee for the Downtown Neighborhood Plan?
6. What big ideas do you have for Horace?

7. What would make this plan a success, in your opinion?

Stakeholder Meeting – Chelsey J. & Brandon H.

Attendees: Chelsey Johnson (Prairie Design Studio/Property Owner), Brandon Huseby (Property Owner), Jace Hellman (City of Horace), Luke Champa (SRF), Arsen Martyshchuk (SRF), Mary Stokes (Property Owner), Leah Petersen (Prairie Design Studio)

1. Which properties you own downtown?

CJ: Mary Stokes, mother owns property just north of Center Ave on the east side of Main Street. Brandon owns property just to the north.

- Looking to develop 2-story max. mixed use buildings
 - Some with apartments on second floor
 - Office space & commercial
- Existing shop on northern property is Brandon's and used for his landscaping company.
- Parking situation is important
- Would like to utilize alley for parking there isn't necessarily an easy solution to parking challenges.

2. Any plans for properties downtown? Timeline? Etc.

CJ: Want to create old town traditional development

- Currently working on the Rail District in Dilworth
- Building Frontage is 27' minimum, 37' max.
- Alleyway concept
- Phase 1 is four condo units on Mary Stokes' property
- 1500-2000 sq.ft. units
- Looking at modern & brick elements

3. What are the biggest opportunities for Downtown Horace?

CJ: potential for traffic/visibility

- Near Lakeview/Sparks
- Destination for 'commercial experience'

CJ: super interested in the Elevators

- Visible landmark
- 6.5 stories tall
- Minot did a mural on their old massive Ag building – made a public park space
- Key point in the community (Horace)
- Could also disassemble
- Parking in front of shops

- ROW improvements
 - Like Broadway – less tables
 - Planters
 - As little furniture as possible
 - Trees
 - Charming aesthetic
 - Natural elements

4. What are the biggest challenges for Downtown Horace?

CJ: Don't have a nice 'old' downtown like other communities across the state.

- Even Broadway (Fargo) has challenges with traditional downtown elements
- Business is hard to sustain in town
- Not sure what it would take to reuse or preserve the Grain Elevator structures
 - Dust from grain, constructability, wood construction, building code, etc. challenges
 - Likely expensive
 - Even preservation cost
- Residential properties nearest to Wall Ave.
- Hopewells Automotive
- Parking, don't want to lose Main Street on street parking
 - Will need to coordinate with other property owners on alley concept
- ROW width is 80' on Main St between Wall and Park Drive
- Changes are hard, especially with existing homeowners
- Freed Park reconstruction and property owner acquisition discussion – no interest to sell to the Park District at the time
- Mobile Home Park south of downtown
 - Lots of hoops to jump through, relocation is very difficult, funding, legal protections, etc.
- Cass-Clay Community Land Trust owns home(s) near trailer park
- City doesn't qualify for CBGP funds unless looking at Block Group data – had to apply more funding alternatives for CCCLT
- Bad soils – houses sinking – sand vane – geo grids successful
- Concerns about blight
- Some property lines go to centerline of Main Street
- People are unrealistic about changes
 - Market is likely going to drive development
- Change might start with them and radiate out
- Existing parking lot options for additional parking
- Single-family lots north of their properties to a community square

- Community parking area on abandoned rail corridor or elevator property?
- Fire hall expansion – parking expansion

5. Anything else you'd like to share regarding the future of Horace?

Stakeholder Meeting – Dwyer

Attendees: Jack Dwyer (Dwyer Law Office), Luke Champa (SRF), David Sweeney (SRF), Jace Hellman (City of Horace), Chelsea Levorsen (Metro COG), Michael Maddox (Metro COG)

JD: Dwyer Law Office – works with landowners in Horace on development projects

- e.g. Lost River, Terra Gardens, Lakeview, Sparks, Lost River West, Jackson Alley, Energy Park (south side of 100th Avenue S.)
- Wife is Rachel Dwyer, on City Council
- Center Ave Redone in 2023 & CR 17 overlay

JD: Masterplan for lakeview

- Has hardcopies somewhere
- Concept was to create a town center for Horace
- Started with public amenity (central open space/park/lake)
- Diocese of Fargo original owner of land. Looked to sell to developers considered not only price of sale but for plan for what it could look like
- Idea of recreational lake was from Edina's Centennial Park (water feature)
- Family oriented – Lakeview Drive as Downtown
- Diocese Liked it even with 3rd highest price point
- 1st phase – single-family housing.
- 2nd Phase – working with Kilbourne Group on funding mechanisms to build the rest. Investment group for building vertical construction.
- Considering TIF revenue for recreational lake components, performance area, and playground
- Not on the same page with investors on look/aesthetic
- Currently locking in a realistic design, something everyone would be excited about
 - Developing revenue model (pro forma)
- Were meeting regularly with Horace (City) and Kilbourne, some movement but likely to get picked up again this Winter
- Option to remove comm on 'lakeside' of lakeview dr

DS: Visioning for Downtown Neighborhood Plan – some concepts in a couple of months. To what extent do we approach Lakeview? Responding to public feedback. Connections to the area. Multi-modal connectivity.

JD: several rounds of masterplan and redesigns so far.

- To elaborate, one concern that pond/lake feature is hidden.

- Considered opening up corridor from the intersection of Wall Avenue & Mainstreet – corridor to Lake feature to the northeast. Have developed 4 or 5 different concepts for the area.
 - Have been working on the masterplan since 2019
 - Dedicated 8.5 acres to the Horace Park District – not necessarily locked in forever though

DS: Opportunity to build upon public input. Opportunities. Qualms. Hesitations?

JD: Tweaks. Any suggestions would listen to but otherwise

- 90% happy with masterplan as shown
- Goals and principals
 - Higher density, parking behind structures, impressions of downtown

CL: Lakeview is not necessarily a secret, many residents seem excited about it.

JD: Sparks Addition

- Tough plan – have piece of land, property borders older part of Horace
- Was slated for mixed use but may shift to more commercial-only adjacent to CR 17
- Close to downtown – looked at density
- Single-family homes – land owned by family not professional developers – didn't like cash in lieu park payment so including a nontraditional park or open space near CR 17
- Green lots on map are public – public square – thought about trails, hedge maze, community gardens
- Platted – as mentioned may be moving away from large mixed use buildings
- Kelly Visto and Big Erv's have some interest in a larger commercial property here
- Commercial near CR 17 – smaller lot options
- Urban design standards for streets
- Rural cross sections are certainly economical – no one wants to pay for improvements.
- Older neighborhoods with rural cross sections are fun places for trick or treating

JD: Likes the yield street – peds have priority, people seem to forget that. Love european roads where ped priority is very apparent.

- Pursues projects in town because cares about the future of Horace and its Identify
- Criticized comp plan Future Land Use and consultant
- Worried about strip commercial development on CR 17 – those are long walks up and down CR 17.
- Large blocks on CR 17 – way cheaper land than compared to bigger cities

- Concerned about CR17 development
- Preserving Downtown feel was not priority in Lakeview
- Wants to preserve parks, family-orientation i.e. more family amenities like a hockey arena, etc.

1. General Timeline – Development Plans for Lakeview and Sparks Additions

2. What are the biggest opportunities for connecting new developments to Downtown?

3. What are the biggest challenges for connecting new developments to Downtown?

4. Have you heard anything about the city owned property on the east side of the Sheyenne River and a possible bike/ped connection from Lost River to Independence Park?

Stakeholder Meeting – Engineering & Public Works

Attendees: Jace Hellman (City of Horace), Adam Carpenter (City of Horace), Jim Dahlman (Interstate Engineering/City of Horace), Luke Champa (SRF), Michael Maddox (Metro COG)

LC: Status of Utilities

JD: Sitting pretty good.

- Need abandoned railroad corridor
- Lakeview Development is good
- South of Park Drive is challenging
- Lift station 1 from low ground
- Fire is not happy about yield street
- Don't see problem on low volume streets to just walk on the road

AC: Park Drive could use a sidewalk maybe but others no

- Sheyenne Drive

JD: complete streets – delineate, break out – Center – CR 17

- Was just in St. Paul – showed example of Kellogg/Wabasha would like to see something like that
- Pros and cons of rural vs urbanized
 - Con of urbanized is cost
 - Rural not always wide enough for parking, even garbage day can be challenging
 - Parking on rural hard, either one side or the other would be better situation
 - Stern opposition expected if rural cross sections were changed to urban
 - Not necessary
 - Sometimes people complain of cattails, ditch being wet, can get hard to explain to people that's the way it should be
- Rural character and how things develop in the future, speed shifts,
- Depending upon long-term vision of CR 17/Main Street, pedestrian infrastructure is biggest concern moving forward
- Yield St
- Some resistance with bulb-outs on CR 17
- Bus stops at water tower for Kindred school students
- Parking and dodging
- Parking
- Bulbouts

JH: subdivision ord. currently requires curb/gutter/sidewalk

JD: ord. should change sidewalk requirements

AC: seems like people are more concerned about the use of land vs. what buildings look like

JH: throw the book out the window

JD: 3rd and 4th street cost would be crazy to urbanize for those properties

MM: not a one size fits all solution here – need to be more strategic

JD: Downtown has more residential than commercial currently, newer developments are attracting commercial businesses. Lakeview development incorporates more commercial too.

JD: Sheyenne River crossing independence park to Lost River

JD: Crossings of CR 17 – considerations – what's appropriate?

MM: expect that out of infrastructure – sidewalk ending, etc. – zig-zags

JD: light preferred at Wall Ave and Main St intersection

- Doesn't see a roundabout fitting there
- Open to asking the public about what configuration they'd prefer

AC: CR 17 & 81st Avenue

MM: people may not like signalized intersection (none have ever existed in Horace)

JD: 28 second left turn on CR 17 – compared to other areas that's very fast during peak hour traffic – sometimes the fastest left is right.

MM: hard for the public to understand

- Hwy on diversion alignment will help
- Discussions about 64th vs. 76th Ave S interchanges

JH: 1776 comp plan area needs to be revisited

- 76th planned as major but may end up being 64th now

MM: Circuity/connectivity challenges, people have to walk along CR 17?

JD: People still walk along CR 17, even with heavy traffic and faster speeds.

MM: Veterans Blvd extension politically gaining more support

JH: Re-evaluating major corridors

MM: Retrofit collector distributor

1. From engineering and public works perspective, what are the pros and cons of newer neighborhoods and older neighborhoods? From an engineering/public works perspective.
2. What are the most expensive components of new infrastructure? (i.e. for new neighborhoods)
3. What does your ideal downtown street scape and cross section look like?

JD: Hopkins, MN – includes walkability and landscaping

- Zero setbacks – wider pedestrian realm – planters
- Don't haul snow yet out of downtown but may have to consider depending on development
- Oaks snow median
- Old style neighborhoods (traditional)
- Indianapolis mall style – how tall of buildings

JH: market for density is 2-3 stories max. depends on developer

4. What opportunities do you foresee for the Downtown Neighborhood Plan?
5. What challenges do you foresee for the Downtown Neighborhood Plan?

General discussion about railroad corridor & private sales.

6. What big ideas do you have for Horace?

7. What would make this plan a success, in your opinion?

NOTES Stakeholder Meeting – Metro Flood Diversion Authority (MFDA)

Attendees: Luke Champa, Jace Hellman, Michael Maddox, Jodi Smith, Chelsea Levorsen, Dave Sweeney, and Eric Dodds.

1. Are there policies in place surrounding the sale of remnant parcels currently owned by the Metro Flood Diversion Authority (MFDA)?

JS: Shared Map – rail corridor purchased from western portion of the channel to wall avenue. Excess land policy, selling to adjoining property owners. Same thing to occur on segment east of. Property owners purchasing to centerline. Anything in road ROW will be deeded to Horace (eric working on). A few properties were sold directly by BNSF to property owners. Going through the excess lands policy. One section is working on, 4 subsections – A and B are authorized to be sold – deemed acception to the excess lands policy.

JS: acception near Park Dr – previous property owner has refused their right to purchase A and B are directly to public sale, C and D will be approved for sale at Oct. meeting. Sell as policy outlines. Interested in sanitary sewer, possible trail on C and D, northern potions.

Land management – land sales – excess lands policy is being following.

Map land management – maps – interactive map (7234)

200'-wide and 100' wide

City has expressed interest in B –

C and D have received some phone calls – notice of sale hasn't been sent to neighbors yet. 30 -days to review and offer, can ask for additional time. What is the interest from the City? If City is interested – will want to know before board considerations. Can delay, property owners are getting anxious. City needs to let board know before October,

JS: Rail corridor – obligated to do some environmental cleanup from the railroad. Minimal effort – wherever they had to stop before crossing roads there are some environmental concerns. Working with DEQ to do some voluntary cleanup – cleaning up ballast, if city wants portion we would want to know that as soon as possible. Contract for bridge removal over the Sheyenne river. Meeting with City once a month, they asked how much. Portions in C and D – limited value \$50/sq.ft. (66,000) A and B

\$2.50-\$12/sq.ft. Need sell off A and B. Low value in triangle (.25 acre). Value sits in A – depends on developer value of that.

MM: Elevator property not shown?

JS: needs to be updated. CCJWRD owns some, MFDA owns elevator parcel.

MM: not planning for what will go on those parcels, but making a nod to infrastructure and future land use integration.

JS: senior center has expressed interest, fire hall, there is interest in purchasing elevator. Has offered to sell to the city – would need to make an exception to the policy. Only has made two exceptions – A and B will be straight to public sale – would be hesitant to do that – property owner interest. Technical folks would have to check on sewer DEQ additional requirements. When purchased, rail corridor has a deed restriction for 99 years, no essential development can occur on that property including water. Limited use. Pipeline is unsure – MFDA is researching.

MM: do allow paths to go over with 99-year deed restriction.

DS: if trail connectivity would need to require as public ROW? Is already getting parceled out.

JD: Liberty lane south is sold, just need to plat.

Liberty Lane to Wall is for sale – City wouldn't agree to pay MFDA 12-18 months ago. Deal went into Executive Session. Didn't agree to purchase at any price, liability, costs to own it, insurance. MFDA doesn't need it for the project, selling it at a loss. Powerful. Board doesn't feel the same about A and B, likely wouldn't sell at a loss.

DS: is there an option to purchase just a skinny portion of the corridor.

JD: need to know, lands committee on Oct. 23rd, CCJWD 24th, and MFDA 24th

JS: would rails to trails require ballast to be removed? MM: yes it's too big. Ballast is heavy stuff.

JS: some property owners have asked to remove it for landscaping.

MM: it is very stable, interlocks, 3 or 4 facets. Not enjoyable to walk on.

DS: City's perspective. If someone wanted to put something on it – subdivision process to make it buildable, does City have any authority in that review? Minor subdivision – no authority.

ED: city prefers if they are connected. Likely a plat and replat. Want to make sure easement restrictions from rail are still in place for original lot. Asked us to go ahead

and combine them. Lot setback issues, portions of rail corridor aren't zoned, will be zoned to existing lost.

MM: not getting into that but looking at options. Seem to have enough information on options. Not up to us, up to city to acquire property.

JS: shared where environmental concerns are located on the Rail corridor. Starting south, will work north to segments. Remove ballast and about 6-inches of soil. Contract with Braun

MM: How much remediation are you required to do?

JS: have gone through all that. Worst DEQ near grain elevators. DEQ is a bit of a process. Challenging process. DEQ is going through major changes, more time to get through. Other consideration with sales. A and B intentionally slow-rolled because of Environmental mitigation.

MM: envisioning DT and how it's served with infrastructure. Good to know turned over to private development.

- a. Do policies differ between dry/wet side of the diversion channel or in urban areas?
- b. Is there a document we may reference to fully understand the process?

2. As of today, where is the MFDA in the process of selling remnant parcels in Downtown Horace/Horace City Limits?

- a. Can MFDA describe the process/schedule as understood today?

3. Is the MFDA pursuing or forwarding any recreation opportunities near Horace? (FM Greenway Recreation Master Plan)

- a. What recreation opportunities are still on the table and/or feasible near Horace?
- b. Any updates to share on the recreational component of the Red River Diversion Project? (e.g. Drain 27 wetland mitigation area, Sheyenne River Aqueduct, others?)

JS: No, bypass project, rec components comes up as a challenge with ASN. Walking path or bypass, which takes priority. Haven't focused much on Rec Features. Contract and change orders are excessive. Costs how much? Super expensive.

MM: cant bring federal funds to bear because of open procurement for construction.

JS: when things calm down. There may be more openness to those conversations. Complexity is not welcome at this point. Just in Dallas, something similar, meant for water to run through, paths near feature, sidewalks are concrete. Huge project, huge ROW acquisitions, home purchases, etc. Problem was that no one was on the path, near downtown. Katy Trail, like San Antonio River walk.

JS: Corps. 404 permit. Issues with corps. Have heard of that but havent handed Drain 27 over and will be another 4 years before handed over to MFDA. Forrest mitigation sites, some further south, some abut the channel, planting trees were to start this spring, but starting to do ground work, walking path. Easements similar to FEMA.

JS: open to email.

JS: conflicting interest in property owners. Some have expressed frustrations and intentions of the City. Reading into that owner does not want trail behind their house. All property owners snatched it up, did not want trail back there. Some property owners further south have been maintaining portions of the property for a very long time. Some Accessory structures in rail ROW. A lot of ownership already, some possession. If city were to come in and purchase – property owners may challenge there.

MM: wanted to reactivate corridor in Illinois – RR came and demolished structures.

JS: finding that people don't know where the property line is located. Tree lines in corridors.

DS: people are using this already?

JS: snowmobiles really make people upset about it. There are people okay with walkers and people not okay with walkers.

DS: going to be challenging as private landowners.

JS: all they can do is call the Sheriff and report trespassing. South of town more issues. Fencing? Once its handed off.

DS: Messy.

4. If there is strong community support to preserve railroad right-of-way and/or the grain elevators, would MFDA be willing to meet with the city again about this Plan?
 - a. How would you recommend the City of Horace/Metro COG proceed with future visionary recommendations that may pertain to the grain elevators and railroad right-of-way?

Stakeholder Meeting – ND Department of Commerce

Maria Effertz -Commerce – community services – host and work with federal funds that come through from energy, community block grants, weatherization, slum and blight, state oriented not federal oriented. Housing, retail, grocery stores, all the important things. Help lead to some solutions. Lean funding but high in terms of resources. What another community is doing, looking for studies, community development. How to support implementation. Lincoln does not have a ZIP – sales tax goes to Bismarck. Getting their own zip code.

Nicolette Daschendorf – Commerce – fairly new, taking it all in. Find resources for this plan.

Arsen Martyshchuk – planner 1

Tanya Olson –

Michael Maddox –

Chelsea Levorsen -

1. Could you tell us a little bit more about the Department of Commerce?

Commerce is made up of 4 divisions

Workforce and development council – planning and development – high need

Economic development and finance – primary sector- recruitment and retention – hosts some development fund dollars and innovative fund dollars can support entrepreneurs and primary sector growth – ideas for growth

Tourism and marketing – first visit to ND how do we market – find the good life campaign – visit and retention of workforce live and grow

Community services – formula funds and programs to help communities with growth, some low-income services. Touch on almost everyone in the state.

2. Does the Department of Commerce administer or provide funding to local jurisdictions for projects or other activities?

Stakeholder Meeting – NDDOT

Attendees: Wayne Z., Stacey Hanson, Chealsea Levorsen, Michael,

1. What major changes to funding can Horace expect with a population over 5,000?

Programs –

SH: main program on the urban side – urban grant program once reach 5,000 with decennial census – generally multimodal improvements to downtown. Program for governor's main street initiative. Geared toward downtown areas and eligible for. Another thing would be eligible for different funding through metro cog- more FFC roads and opportunities through Metro COG. In the past the City would've been eligible for Urban roads program – but because in UZA – would fall into the TMA program. Keep in mind federal aid programs regardless of size, city qualifies for and would qualify for. TA for example, funds that could be gone after. TMA has their own TA suballocation, would have to apply through TA. HSIP program for known safety issues, can currently apply for HSIP – no pop. Size limits, annual solicitation. Federal on HSIP is 90%, helps with safety issues. Nice thing about it is doesn't need to be on federal aid route, just on public road. Can't be private, any public street. HSIP typically tied to data, if there is perceived need, visit with Justin Schlosser who can give some good ideas to apply for those types of things. One more is the flexible transportation funds, City is currently eligible for that. No pop. Restriction on that, can apply for project at 100% funding or to match other funding. Original thought was to match federal grants (discretionary) or to match regular federal aid. But now agencies can apply for flex funds, total or if there are local dollars they can put in, just need to put on application. Brand new with last legislative session, solicitation on bi-ennium basis, hopefully get another pot of money to move forward with another solicitation, maybe summer.

2. What can you tell us about Prairie Dog Funds for communities?

Does the distribution change with pop.?

Prairie dog funds – stacey would have to check on. Can check on that. Stacey to send info.

3. Any other legacy funds that Horace is eligible for?

-strictly, oil producing counties.

4. Programs for streetscape/Main St. improvements? Anything from other State Departments?

ND parks and rec – use NDDOT funding – also another program for pedestrian and bicycle type improvements.

5. Do you anticipate any major change to the Main Street Initiative and Urban Grant Program with an upcoming new Governor?

-anything is possible. Did develop the UGP when Burgum was on board with his initiatives. At this time no thoughts on getting rid of the program. If new Governor didn't like, could be a possibility. Funding for the program comes out of the overall urban program for the state – taking a portion and allocating towards that program. Downtowns get forgotten, anticipate would get reallocated through regular urban program.

Eligibility, plantings that come back every year (perennials). Grasses, daylilies, have done planter areas, benches, trash cans, decorative lighting, stamped concrete, paver areas, some other monuments and other types of things – show tie to transportation. Up to FHWA to approve that portion, in general pretty flexible. Sheyenne st in west fargo was a UGP – stained conc. Benches, lighting, trees, plantings, through UGP.

Monuments that are transportation related – like Devils Lake example. Photo of monument with street names and information about downtown – historical photos from the past. Patterned concrete was done – called monument, picnic table, decorative lighting, bulb out, did all with project. Any line of sight issues? Inside the parking area, curb extension, setback. Devils Lake 4th St. sight distance at the intersections – if they didn't, haven't received any complaints. Project was built in 2019-2020. \$4.6M .

Highway Distribution Fund – Horace currently receives Highway Tax Distribution Fund – figure out how that would change for them. Stacey can ask about how that changes things.

Tanya: thank you good information.

Stacey: knows every

Wayne: follow stacey's instruction

Monuments on google earth – 4th st.

CL: how is it transportation-related? SH: has the street names, believes FHWA was okay because of historical reference for the City and downtown. Transportation-related, can come up with 'transportation-related'. Will take to FHWA and see if they agree, some things like advertizing arent eligible, side of a bench or trash can and rules against ads in public ROW. Have been able to get Jamestown 'buffalo' on side of trash cans because they're called the buffalo city.

WZ: demers some were eligible and some were not i.e. planters not removable. GF light monuments at intersections not eligible because didn't necessarily provide lighting (as a utility to transportation) those were not federal-aid, majority of other things were. A lot of projects have cool features, some eligible and some not (city can pay the ineligible). Work closely with the cities and FHWA on all of the items.

Stakeholder Meeting – Horace Park District

Attendees: Luke Champa (SRF), Tanya, Maddox, Paige Shockman (park district), Wade Frank (park district, president), Dave Sweeney

1. Could you tell us a little bit more about the Horace Park District? (staff, roles, existing properties)

Staff – not a lot. All part-time, maintenance (2), seasonal staff (mowing, rec programs, small staff). Clerk (Jason) for park district has day job at the WF Park District. Has worked out some arrangements with WFPD, worked out agreement to have staff from WFPD to share. Sharing full-time employee for the 2 Park District. Full-time hire is exciting – to transition between part-time staff to hiring full-time staff. Shared position will bridge gap.

PS: seasonal staff don't stick around. Hoping to have some continuity and transitioning to a full-time director. Help bridge that gap.

2. Does the Park District have a strategic and/or long-range plan?

WF: 2020 initial strategic plan. Interstate did a reboot over the summer, recently adopted. 8-10 future plans to undertake, long-term planning, priority of needs assessment. Starting that shortly. Involves looking at growth and 8 or 10 parcels, get feedback of what the public wants to see, what do they want. Knocking stuff off moving forward.

PS: strategic plan became obsolete in 2.5-years.

WF: will get it to us. (email ps)

TO: With respect to what we're doing, would Park staff do maintenance in all public spaces?

WF: if it's on park property, we do it. Do snow removal, don't do anything in the neighborhood. HOA properties are paid through HOA.

Park property is their responsibility

TO: ANY CONCERNS ABOUT DOWNTOWN AND TRANSPORTATION?

WF: Connectivity is a priority. Other interest is as concepts grow, if there are public spaces to make sure they understand those concepts. One was the Lakeview Development. We anticipated that to be part of it, mixed-use space.

PS: Wade did you take the downtown neighborhood survey. Which of these do you most resonate with? Was curious about how those responses fit in. If the survey responses point a different direction.

DS: LV has been in the works for a while – public engagement could be beneficial. Left it there to support each other, can certainly talk about, public space. Haven't looked at the results yet. One thing Jack mentioned was

TO: propose such a great vision – that developers want to be a part of it. Encourage creativity. Maybe vision for downtown will create some vision and design language – at least you have your compelling vision for downtown. Safe crossings, the heart of the community wants to walk and bike, etc. Small town means to community of Horace. Walking, biking, and golf carts, using trails.

WF: LV – fully inclusive playground. ADA and accessible – make sure that it's not so closed off that it doesn't feel like public space. If Kilbourne analyzes it's not feasible, Park District will have a park regardless of moving forward.

3. Any future projects, properties, or big ideas that we should know about?

4. Does the Park District have any maintenance responsibilities of shared use paths/trails?

5. What opportunities do you foresee for the Downtown Neighborhood Plan?

6. What challenges do you foresee for the Downtown Neighborhood Plan?

TO: lightning fast development – fee-in-lieu (yes), lots of parcels already, developer brings draft plat,

WF: proposal for 100 ac. Parcel, dedication is 10%. They do want to give full percent, Park District sometimes takes straight cash, smaller chunk with FIL, it is different everytime to work with developers and district. PD pays special assessments just like any other property owner. Value in green space but want productive green space as well. Have tried to propose swamp-type land. Interstate might have a map. Will check with interstate, where all the parks are etc. Wade can mark something up. Any parcel they own will show up, some under consideration not accepted or formalized.

WF: 14 years at Park District – pick away over time. Shannon and WF on the board at the same time. First project was playground in neighborhood (\$30,000). Current doing \$1.5-2M

Planning Commission Meeting Notes

What is Downtown and Downtown Neighborhood Plan?

- Started the process, waiting for the green light from the state and Metro COG.
- 10% of the community have participated in the survey. 475 responses from the survey and over 200 people who participated at Bean Days. Survey will be open by October.
- It is pretty obvious, Wall Avenue mostly indicated by the citizens.
- The plan is transportation focused, but land use plays a big role.

Biggest opportunities in Downtown Horace?

- Rideshare programs.
 - o People just expect it to be in Horace.
 - o Examples from the City of Hawley, Fargo-Moorhead.
 - o Maybe demand-responsive microtransit (uber-like, vans).
 - o E-bikes as an option. E-bikes and scooters – policy nightmare.
 - o The city needs to talk about it.
 - o Great Falls, Montana and Bismarck have micromobility services.
- Elevators.
 - o Aren't owned by the city.
 - o What needs to be done to work with them?
 - o Needed engaging interest and guidance.
 - o The city needs to decide whether to buy them or not.
- Commercial businesses.
 - o The city needs something walkable/green.
 - o Maybe small shops (corner stores), they might stand out.
 - o Examples from other small towns needed.
 - o Horace needs a unique identity; it will be attractive.
 - o We need to dig bigger than the elevators.
- Automated vehicles and drones.
 - o Regulated by FAA.
 - o Will be in use maybe in 10 years (that's what planners think).
 - o Should stay on Horace's radar.

- Gathering space.
 - o Something to accommodate food trucks and/or farmers' market.
 - o Bean Days – a good example.
 - o Driving and walking from Meadowlark should be obvious.
 - o Plaza type may be used for Bean Days in the future.

Biggest challenges in Downtown Horace?

- Residential properties.
 - o If they are close to commercial properties, we don't know about the reaction.
 - o Market drives turnover.
- Elevators.
 - o Takes up a lot of land and cost.
- Drive-thru.
 - o May be very noisy.
 - o It can bother the residents if commercial businesses are close to residential areas.
 - o Maybe there is a way to bring the speaker volume down.
- Serving the community.
 - o Lack of restaurants. The chains are watching Horace.
 - o Need for a grocery store.
 - o Horace is a bedroom community. Are there any ways to change that?
 - o Parking is an issue.
 - o Downtowns are about walkability, not parking (Naomi).
 - o Need for more sidewalks.
- Snowmobiles, UTVs, golf carts.
 - o Maybe implementing a policy – no motor vehicles where people use those modes.
 - o **More research** on the mode use in other cities: success and failures.
 - o Kids can't drive golf carts (law), only with adults beside and within a speed limit.

- o People using these modes sometimes don't know they pass private property since it has been bought recently. They need to be informed about it and be more respectful.
- Communication.
 - o There is a lack of engagement.
 - o There is a need to find more ways from more effective communication and better informing of residents besides mail and the website.

Biggest successes in Horace? Are there any other downtowns you think are successful? What do you think is success in Horace?

- Transition: density is needed, it's better when people are together.
- Bars: there might be a lot of "not in my backyard" issues.
- Woodbury, MN: good example of mixed use in a single-family neighborhood (corner store and restaurant).
- Boise: when there's people, the property volumes are higher.
- Developers don't rule, if you give them everything, there won't be any uniqueness. Form-based zoning is great (Naomi).
- Smaller buildings with high density.
- Comp Plan: create areas with different needs.

Stakeholder Meeting – Southern Valley Fire & Rescue

Attendees: Volunteer Firefighters and staff of Southern Valley Fire & Rescue

1. From the Fire Department's point of view, what are the pros and cons of newer neighborhoods and older neighborhoods?

- Street widths in older neighborhoods are too small if they allow on-street parking.
- Construction equipment obstructions
- Street dance/Bean days poses a big obstacle for emergency response
- New neighborhoods are chopped up – no grid system
- Loops are a pain
- Some streets too narrow – 20' unobstructed for code
- New hydrants have low water pressure

2. What does your ideal downtown street scape and cross section look like?

- No parking within 40-feet of Fire Hall - hard to see when pulling onto Main Street
 - No parking 75' from fire hall pad
- Good off-street parking
- Wide enough for parking on either side
- Parking front and back – Big Erv's doesn't seem to have enough
-

3. What opportunities do you foresee for the Downtown Neighborhood Plan?

- Somewhere for venue off the street (i.e. street dance/bean days)
- Growth has happened, community sentiment has changed

4. What challenges do you foresee for the Downtown Neighborhood Plan?

- Older residents have seen a lot of change in a very short period of time
- Special assessments – need to identify grants, other resources for the City
- Mitigation of grain elevators – no ladder truck tall enough to fight grain elevator fire
 - High risk of fire

- Cost to preserve will be high
 - Collapse zones pose danger to nearby neighborhoods/residents
- Council has seemed to be unclear on what businesses should be allowed on Main Street (example just north of big erv's)
- 2045 plan FLU disaster
- No faith in the planning effort – waste of money – seems like the plan is figured out already and Council does what they want
- Water lines impact flow to hydrants
- 4th street truck traffic? Plans for truck traffic?
 - City working on collector
- Political environment isn't great – a work in progress
- School district pressure
- Residential growth issues
- Slowing growth is going to be hard to do
- Fire district funding is behind – budget hasn't grown with the City

5. What big ideas do you have for Southern Valley Fire & Rescue?

What about big ideas for Horace as a community?

- Be strategic
- Would be desirable to have fire training tower downtown
 - Could have training facility elsewhere in town
- 81st St paved, more north/south connections, 57th St/Veterans
- Want 40' ladder truck
 - 5-year lead time to get one, cost \$2.1 million
- Would like green space in front of building
- Mission to increase fire safety
 - Would have concern with raised crosswalks and small roads in nearby neighborhoods
- Horace needs more business development
- Chance to redirect focus

6. What type of call volume does the Southern Valley Fire & Rescue see annually? Do you have a breakdown of call types (e.g. EMS/Fire/Traffic Incident/Etc.)?

- 1 to 2 calls per day – need 24/7 staff
- Looking for expansion and EMS
 - Has EMS needs
- Will need 9-10 people 24/7 based on growth
- Have 2 existing dorms

- 295 calls in last year
 - 60% of calls are in horace
 - Agreements to respond and help other local fire departments
 - 14 fires last year, 3 were structure fires
 - Respond to more with west fargo fire
- Currently 2 FTE and 5PTE with numerous volunteers – their Hickson, ND station struggles, provide support there
- Have talked about a new station on 64th Ave S
 - Tenders/grass rigs
 - Do fight both rural and urban
- Would like to use whole railroad ROW for fire training/facility expansion.



WORK CONTINUES ON THE HORACE DOWNTOWN NEIGHBORHOOD PLAN

Throughout September, there was a push to gather ideas from Horace residents about the Horace Downtown Neighborhood Plan (Plan). The feedback received was focused on opportunities and challenges that Horace may face moving forward, to gather initial thoughts on what Downtown Horace means to the community, and to determine how people get around and how they would like to get around town.

Thank you to those who participated in the community survey, and to those who engaged with the planning team at Bean Days. An estimated 700 residents and visitors have participated in the planning process so far! Everyone who participated brought valid ideas, comments, and inspirations for the future of our community. The planning team is currently summarizing engagement event results, which will be posted on the Plan's webpage here under the 'Project Resources' section: <http://fmmetrocog.org/projects-rfps/downtown-horace>

The feedback received so far will guide the Plan through upcoming phases to help answer two critical questions:

WHAT COULD DOWNTOWN LOOK LIKE?

The planning team will use feedback as a guide to conceptualize what the future of Downtown Horace could look like. Multiple concepts will be developed to identify types of land uses, types of transportation infrastructure, and different downtown scenarios.

WHAT DOES THAT MEAN FOR OUR COMMUNITY?

Broader connections to and from downtown will be analyzed. Gaps in the bicycle and pedestrian network will be identified, roadway cross sections will be examined, and strategies will be developed for a more cohesive transportation network to support our community's central business district or downtown.

This Fall and Winter, the planning team will develop concepts and strategies based on public input received so far. Feedback from residents and community members will be used to refine developed concepts. A final round of community engagement will occur in the spring of 2025 to confirm Plan goals and objectives and to provide community input on any of the recommendations and strategies identified.



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Horace Downtown Neighborhood Plan

Join us for a public workshop on
January 22nd or January 23rd!



Let's roll up our sleeves and dive into options. Come talk to the project team about which street, intersection, and land use development options you think are best. Show us which routes connecting to downtown are the highest priority and provide your ideas to guide future strategic investments in Horace's core neighborhood. We are hosting two opportunities in January at the Fire Hall Event Center, open to everyone.

We'll have activities for all ages so please visit anytime during one of the days/time-frames shown below.



Image showing one of the potential future options for downtown

Downtown Neighborhood Plan Workshop Open Houses

Fire Hall Event Center, 413 Main Street, Horace, ND

January 22 and 23, same information at both meetings.

Activities for all ages. Refreshments will be provided. Stop by anytime!



2:30 - 8:00 p.m.

&



4:00 - 8:00 p.m.



Can't make it? Workshop materials and an online survey will be added to the project website Wednesday, January 22nd, for those who cannot stop by in-person. To learn more about the Horace Downtown Neighborhood Plan and to subscribe to plan updates please scan the QR code or visit:

www.fmmetrocog.org/projects-rfps/downtown-horace



METROCOG
FARGO-MOORHEAD METROPOLITAN COUNCIL OF GOVERNMENTS

FOR IMMEDIATE RELEASE

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HORACE DOWNTOWN NEIGHBORHOOD PLAN – WORKSHOP OPEN HOUSES

The Fargo-Moorhead Metropolitan Council of Governments (Metro COG) and the City of Horace are hosting two workshop open houses for the Horace Downtown Neighborhood Plan. The workshops will be held at the Fire Hall Event Center, 413 Main Street, Horace, ND. People may stop by anytime on **Wednesday, January 22nd between 2:30p.m. and 8:00p.m. or Thursday, January 23rd between 4:00p.m. and 8:00p.m.** Refreshments will be provided!

Anyone interested is encouraged to come see what downtown Horace could look like in the future! The workshop will allow people to provide thoughts about which street, intersection, and land use development options they think are best for Horace's downtown. The purpose of the workshops is to allow people to participate directly in the development of the Downtown Neighborhood Plan by responding to potential future downtown options and visiting with the project team. There will be feedback opportunities and activities for all ages, and people can visit anytime during one of the days/timeframes shown above. Feedback will be used to put together recommendations and guidance included in the final plan report, anticipated to be completed this Spring.

If someone cannot make it to one of the workshop open houses, all workshop materials and an online survey will be posted on the project webpage by 2:30p.m. Wednesday, January 22nd. To take the survey, learn more about the Horace Downtown Neighborhood Plan, and to subscribe to plan updates, you may scan the QR code or visit <http://fmmetrocog.org/projects-rfps/downtown-horace>. The survey will be closed after Wednesday, February 12th.

Written comments about the Horace Downtown Neighborhood Plan workshop materials can be submitted by mail or email to Michael Maddox (contact information listed below) before Wednesday, February 12th. Comments will also be accepted through the project webpage at the address listed above.

For questions regarding the Horace Downtown Neighborhood Plan, please contact Michael Maddox, Metro COG Senior Transportation Planner, at maddox@fmmetrocog.org / (701) 532-5104. Alternative participation options will be accommodated upon request.

###

Workshop Open House 2025 Engagement Summary

- When:** Wednesday, January 22, 2025, 2:30-8:00p.m. and Thursday, January 23, 2025, 4:00-8:00p.m.
- Where:** Horace, ND – Fire Hall Event Center, 413 Main Street.
- Why:** Public & Stakeholder involvement is a critical component of the Horace Neighborhood Downtown Plan and will be used to guide plan strategies and recommendations.

Event Background

As part of the Horace Downtown Neighborhood Plan, the Fargo-Moorhead Metropolitan Council of Governments (Metro COG) and the City of Horace hosted two workshop open houses to engage the community in shaping the future of downtown Horace. These workshops were facilitated to provide residents, business owners, and stakeholders with an opportunity to actively participate in the planning process by reviewing and providing feedback on potential street, intersection, and land use development options.

The open house workshops were held at the Fire Hall Event Center in Horace, ND, on January 22nd and 23rd, 2025. Participants were encouraged to stop by at their convenience to review plan materials, interact with the project team, and share perspectives on the future of Horace's downtown. The workshops featured interactive activities suitable for all ages.

Figure 1. Workshop Open House Announcement on Project Webpage

Workshop Open House 01/22/2025 & 01/23/2025

Wednesday, January 22, 2025

2:30 PM - 8:00 PM

The Fargo-Moorhead Metropolitan Council of Governments (Metro COG) and the City of Horace are hosting two workshop open houses for the Horace Downtown Neighborhood Plan (plan). The workshops will be held at the Fire Hall Event Center, 413 Main Street, Horace, ND. Please come anytime on Wednesday, January 22nd between 2:30p.m. and 8:00p.m. or Thursday, January 23rd between 4:00p.m. and 8:00p.m. Refreshments will be provided.

Come see what downtown Horace could look like in the future! Everyone is welcome to stop by the workshop and provide thoughts about which street, intersection, and land use development options you think are best for Horace's downtown. The purpose of the workshops is to allow people to participate in the development of the Downtown Neighborhood Plan by responding to potential future downtown options and visiting with the project team. There will be feedback opportunities and activities for all ages, to please visit anytime during one of the days/times shown above. Your feedback will be used to put together recommendations and guidance included in the final plan report, anticipated to be completed this spring.

For more information please visit: <https://ftrmetrocog.org/projects/https://downtownhorace>

Horace downtown neighborhood plan - workshop open houses

Downtown Neighborhood Plan Workshop Open Houses

Fire Hall Event Center, 413 Main Street, Horace, ND

January

Refreshments will be provided!

22

Wednesday

2:30 - 8:00p.m.

&

Stop by anytime!

23

Thursday

4:00 - 8:00p.m.

The Fargo-Moorhead Metropolitan Council of Governments (Metro COG) and the City of Horace are hosting two workshop open houses for the Horace Downtown Neighborhood Plan.

Other News

Horace downtown neighborhood plan - workshop open houses

Jan 16, 2025

2050 Metropolitan Transportation Plan now taking public input

Dec 19, 2024

Metro COG seeking Bicycle & Pedestrian Committee Citizen Rep

Sep 4, 2024

The feedback gathered during the workshops plays a critical role in shaping recommendations and guidance for the final Horace Downtown Neighborhood Plan report, expected to be completed in the summer of 2025. For those unable to attend in person, workshop materials and an online survey were made available on the project

webpage. Written comments were also accepted via mail and email through February 12th.

Workshop Advertising

Robust efforts to advertise the open houses were undertaken through the following channels:

- Horace Happenings publication
- Email Subscribers
 - 296 people subscribed via email during the first round of engagement
- Legal advertisement
 - Published in the Forum of Fargo-Moorhead (official newspaper)
- City Utility Bills
- Postcards mailed to adjacent property owners
 - Adjacent to the downtown focus area

Workshop Open House Engagement Activities and Feedback

The Horace Downtown Neighborhood Plan workshop open houses featured several interactive activities to gather community input on potential downtown development options, infrastructure priorities, and the overall vision of downtown Horace.

Figure 2. Engagement Activities



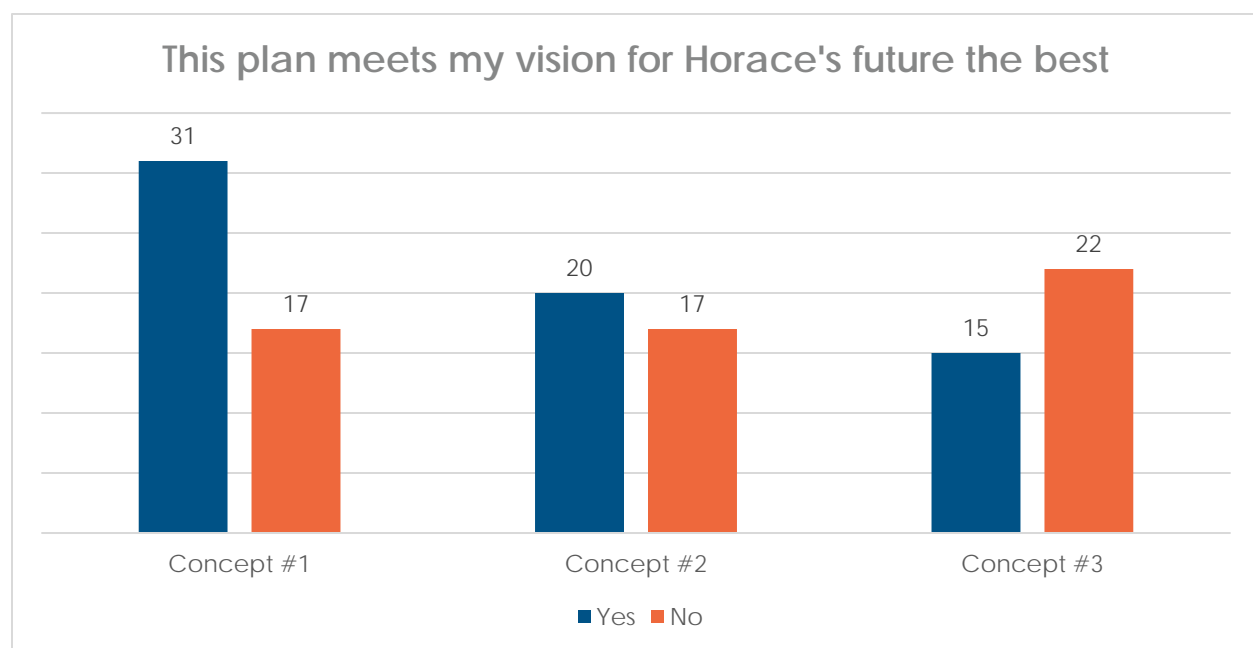
Downtown Concept Comments

Description

Participants reviewed three potential downtown concepts, each presenting different visions for the future of Horace. They were encouraged to provide feedback by communicating directly with the project team and by placing written comments on the boards. Additionally, attendees used stickers to express agreement or disagreement with statements about the overall vision and how each concept aligned with Horace's identity.

Feedback Received

Table 1. Downtown Concept Feedback



The community support for the three proposed concepts indicates varying levels. Concept #1 received the most positive feedback and had 31 participants agreeing that it aligns best with their vision for Horace's future, compared to 17 who disagreed. Concept #2 had a more mixed response, with 20 in favor and 17 opposed. Concept #3 had the least support, with only 15 agreeing that it meets their vision, while 22 disagreed.

Concept #1

Figure 3. Concept #1 Board

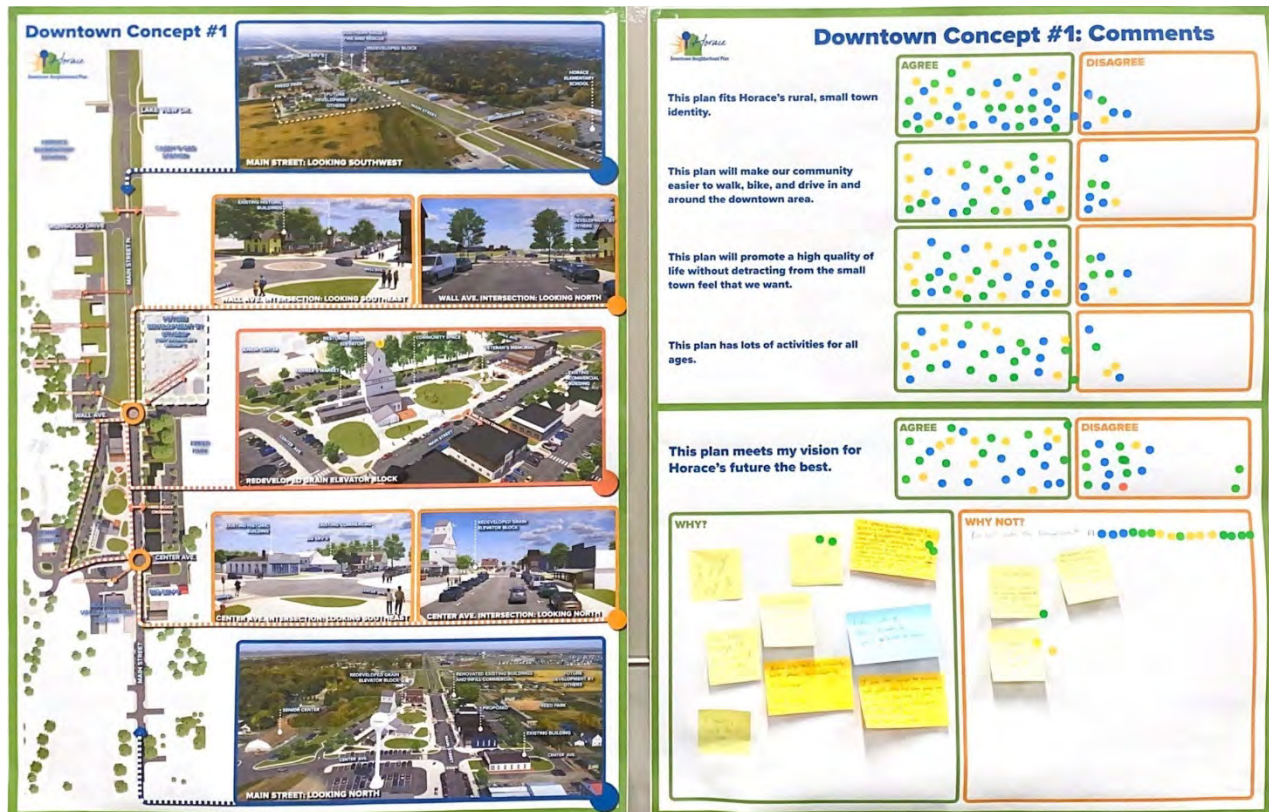
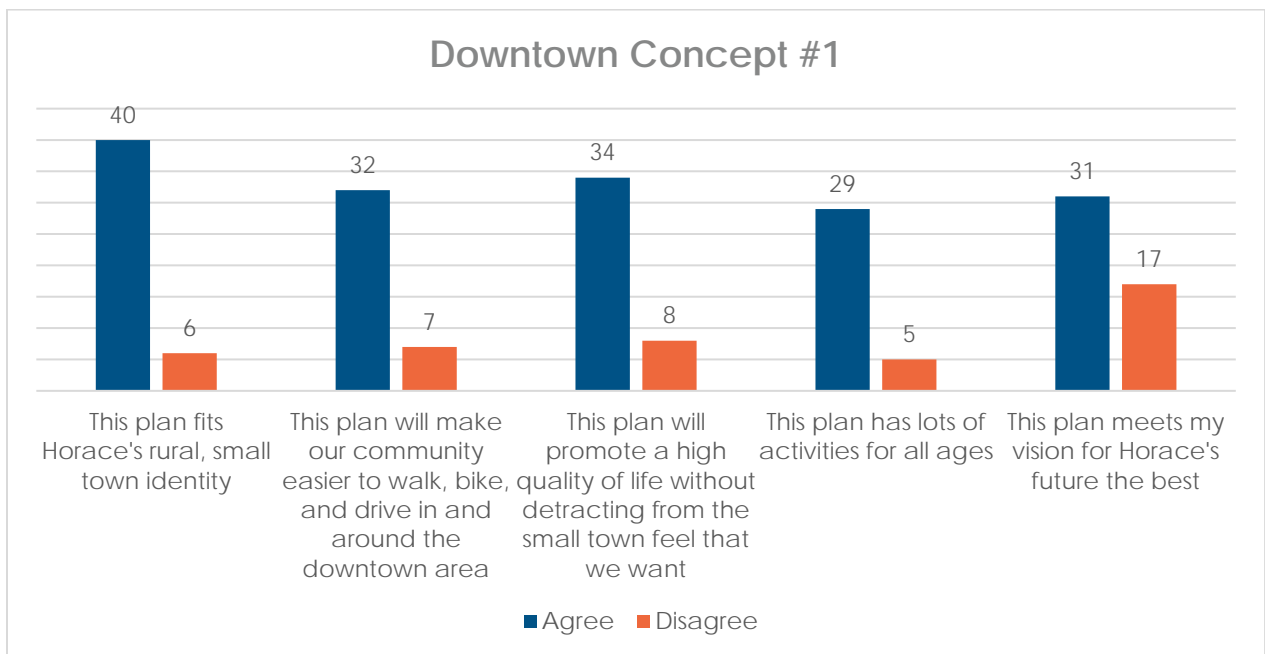


Table 2. Concept #1 Feedback



Positive Feedback:

- Many participants felt that the concept would successfully maintain the small-town feel of Horace and integrate improvements for walking, biking, and driving.
- A significant number of respondents emphasized that the grain elevator is a key landmark that should be preserved, as it gives Horace a unique identity.
- A gathering green space around the grain elevator, along with a designated area for a farmers' market, was highly praised as a way to improve community interaction.
- Several participants appreciated that this concept left the frontage road to Ironwood Drive intact.
- Respondents felt that this concept promoted a high quality of life without compromising the small-town atmosphere, and they appreciated the variety of activities included for all age groups.

Suggested Improvements & Concerns:

- The most frequently voiced concern was a strong dislike for the roundabout, with many believing it would negatively impact traffic flow.
- Some participants felt that the concept lacked adequate commercial areas and suggested incorporating elements from Concept #3, such as a redone frontage road and a downtown connection trail (off-street), to better support business development.
- A few respondents expressed concerns that there were too many parking spaces, which could be better utilized for businesses or family-friendly gathering areas.
- A minor but notable concern was that the elevator could pose a fire hazard, with a few participants questioning its long-term safety.

Concept #2

Figure 4. Concept #2 Board

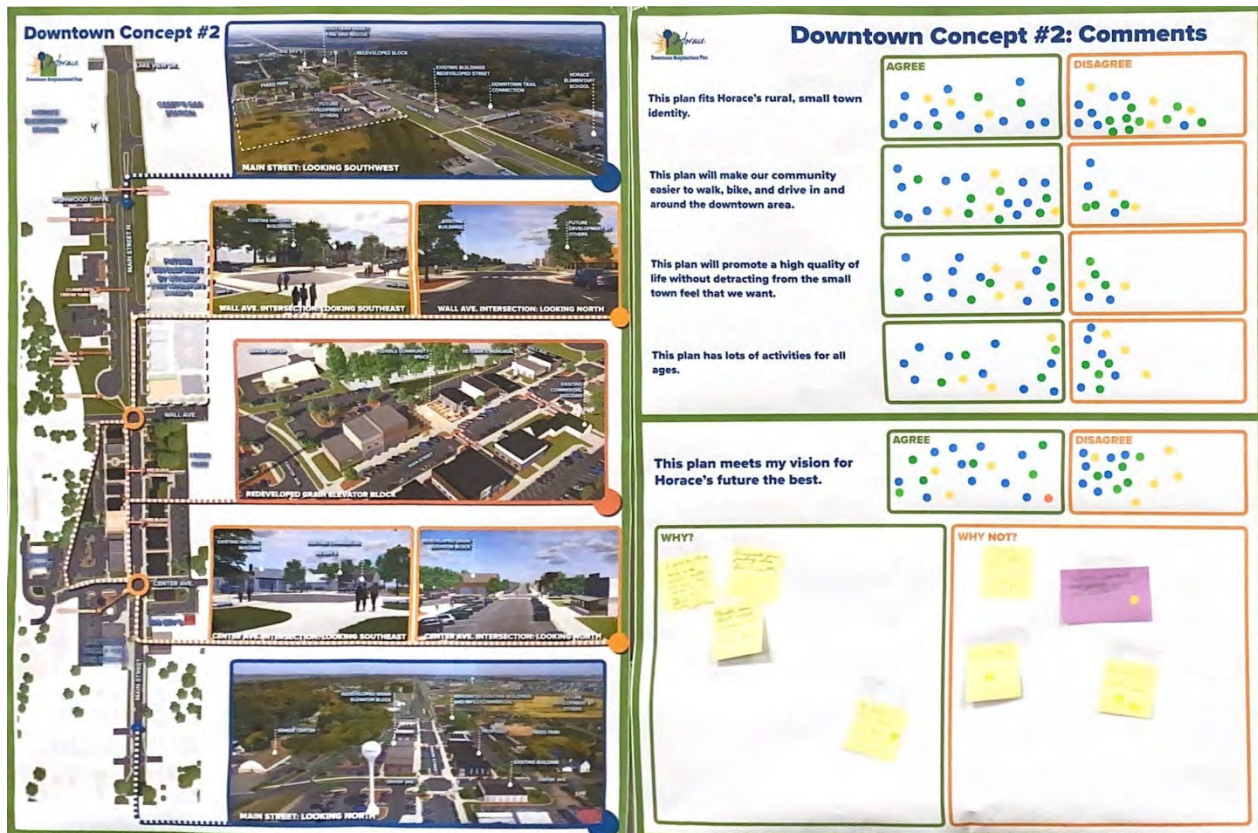
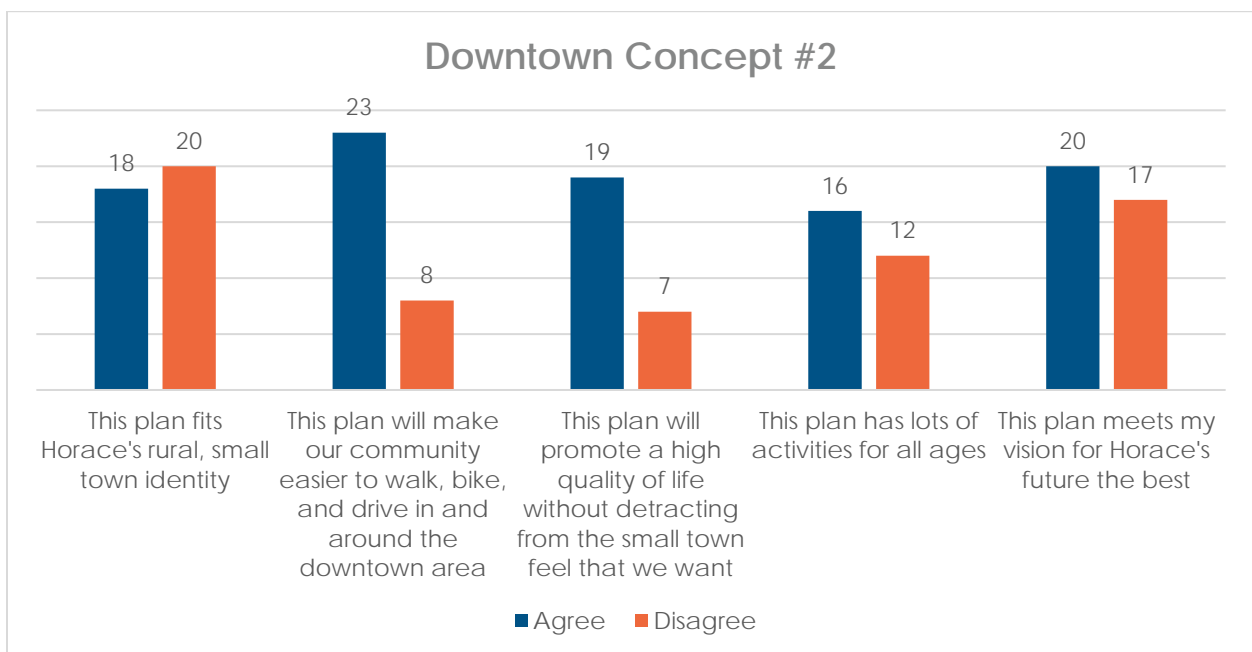


Table 3. Concept #2 Feedback



Positive Feedback:

- The majority agreed that this plan would make it easier to walk, bike, and drive around downtown.
- Many believed the plan would enhance the quality of life while maintaining Horace's small-town charm.
- Several participants appreciated the classic downtown atmosphere and preferred this option over Concept #1, which they felt duplicated existing public spaces.
- Some respondents suggested enhancements, such as a community center with additional amenities.

Suggested Improvements & Concerns:

- One of the biggest criticisms of the concept was the absence of a preserved grain elevator or designated green space.
- Many felt that there was too much downtown street parking, particularly along Main Street.
- Some worried that excessive brick-and-mortar development would detract from Horace's rural character.

Concept #3

Figure 5. Concept #3 Board

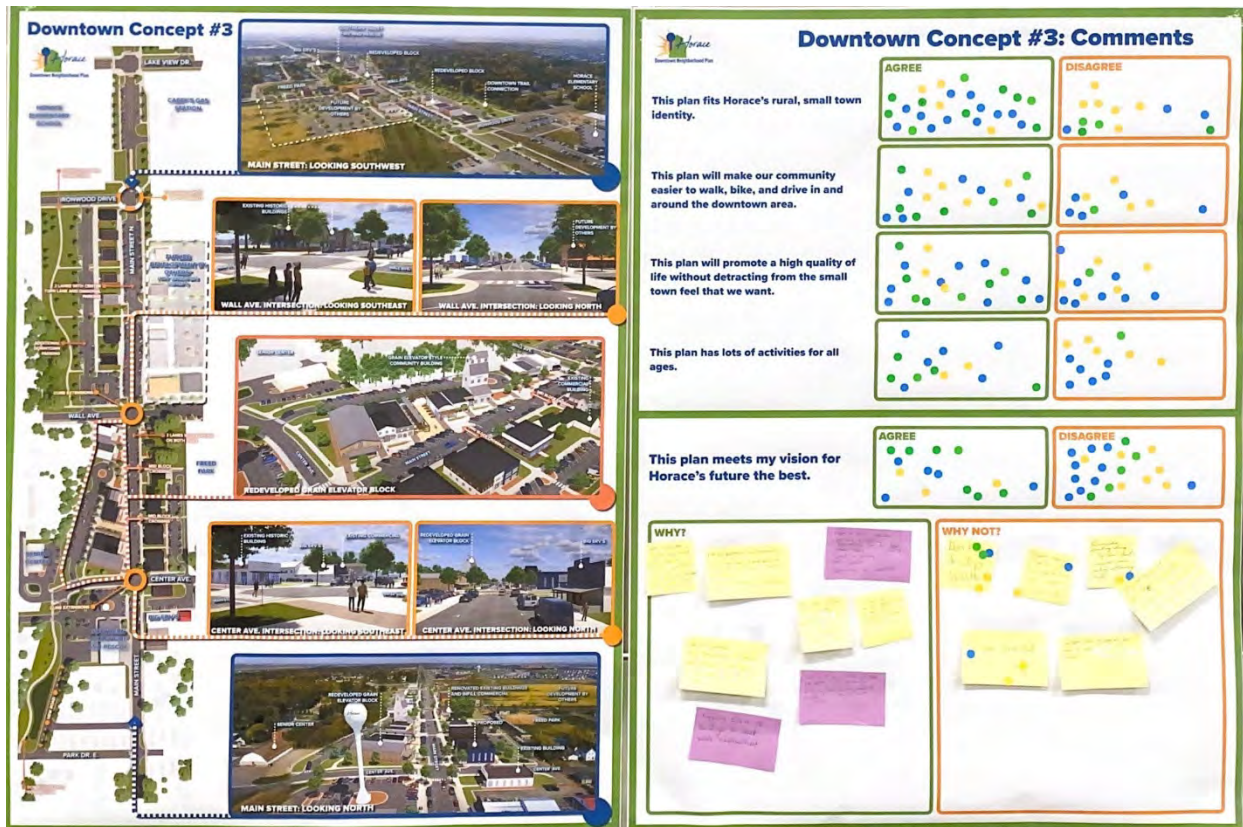
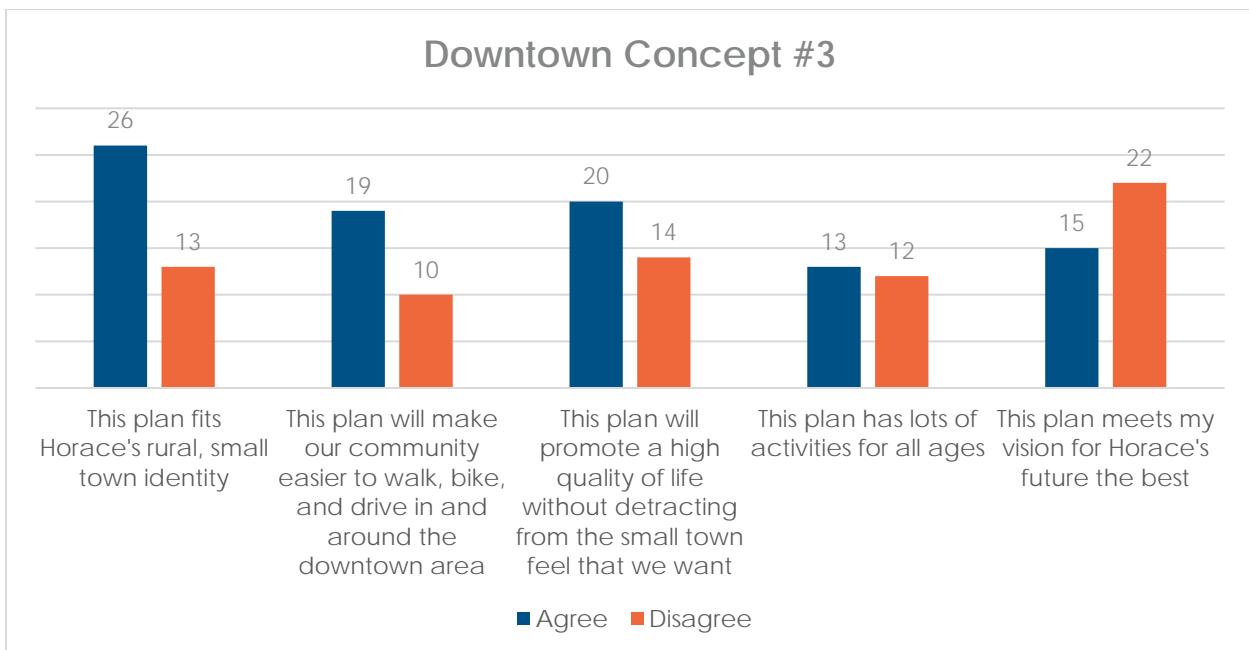


Table 4. Concept #3 Feedback



Positive Feedback:

- This concept received public agreement in maintaining the rural character of the community.
- Many participants appreciated the off-street downtown trail connection and suggested it could be enhanced with additional parks or gathering spaces.
- The inclusion of a market space in the grain elevator block was well received, and the respondents noted it has potential as a vibrant community hub.
- The layout of shops, event spaces, and available parking was seen as a good mix of opportunities for businesses and space for all age activities.
- Several participants expressed interest in a dedicated green community space for events, with comparisons to open-air music venues in other downtown settings.

Suggested Improvements & Concerns:

- Some respondents worried that certain design elements could create congestion issues.
- A notable concern was opposition to the development south of downtown.
- The respondents appreciated the trail connection, but some felt the concept lacked adequate green space and suggested incorporating more.
- There was a desire to see stronger connections between Maple Lake Addition and the downtown core to enhance accessibility.

"Scale Your Preference" Exercise

Description

This activity explored what the "small town feel" means to residents. Participants were presented with six categories: Commercial, Retail & Services; Major Intersections; Commercial Corridors; Pedestrian & Bicycle Connections; Pedestrian & Bicycle Crossings; and Streetscaping. Each category included three (3) images with a scale-bar below with 11 levels, ranging from the least (1) to the most intense (11) development or infrastructure options. Attendees placed a sticker anywhere on each scale to indicate the level of intensity they believed best suited Horace. The scale rating equates to the following:

1-3 = left-most, less intensity

4 = in-between or mix of left-most and middle images

5-8 = middle image, moderate intensity

9 = in-between or mix of middle and right-most images

10-11 = right-most, most intensity

Feedback Received

Figure 6. Scale Your Preference

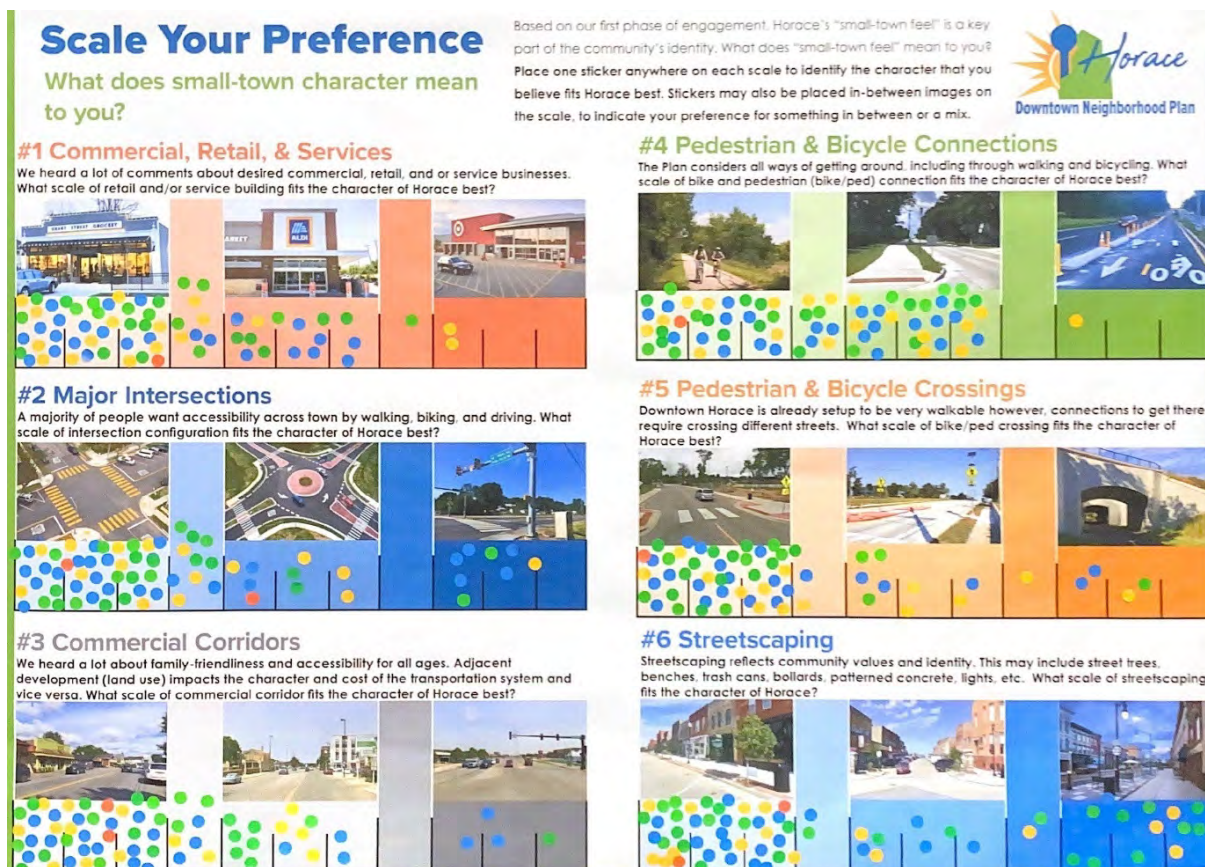
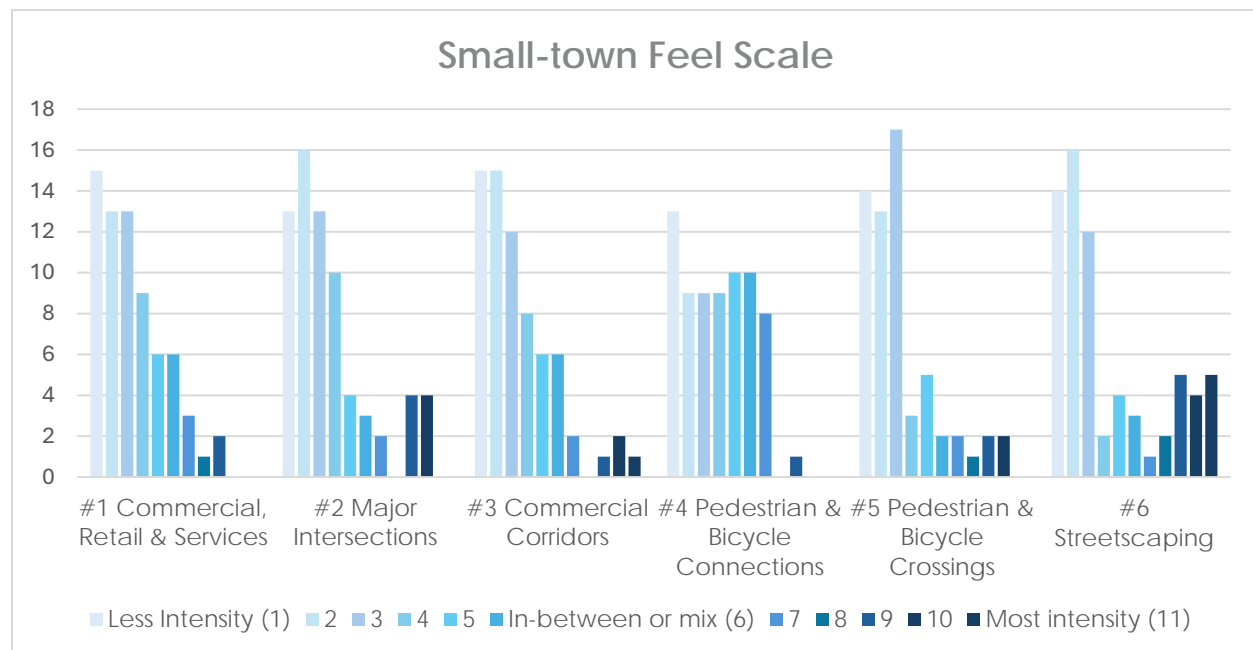


Table 5. Small-town Feel Scale



Key insights:

1. **Commercial, Retail & Services** – Preferences leaned toward lower to moderate-intensity, with the majority selecting levels 1-4 (50 out of 68 responses) with participants favoring smaller-scale businesses over larger commercial or ‘big box’ development.
2. **Major Intersections** – Responses were somewhat varied, though most participants favored lower-intensity traffic control, with the highest concentration in levels 1-4 (52 out of 69 responses). A small group supported more engineered intersections (levels 9-11) including roundabouts or traffic signals.
3. **Commercial Corridors** – Similar to retail preferences, most participants preferred lower-intensity corridors, with a majority selecting levels 1-4 (50 out of 68 responses). Results indicate a desire to maintain a small-scale, traditional commercial corridor rather than an auto-oriented commercial corridor.
4. **Pedestrian & Bicycle Connections** – Responses were more balanced, with preferences spread across levels 1-6 (60 out of 69 responses). Results showed broad support for pedestrian and bicycle infrastructure at a moderate scale including off-street trails and shared use paths along major corridors.
5. **Pedestrian & Bicycle Crossings** – Most responses clustered in levels 1-5 (52 out of 61 responses) with a preference for simpler, less intense crossing

infrastructure rather than highly engineered or complex pedestrian infrastructure.

6. **Streetscaping** – This category had the most diverse responses, with some participants supporting more intensive streetscaping. While the majority preferred **levels 1-5** (48 out of 68 responses), a notable minority selected higher levels (6-11) and showed the interest in a variety of streetscaping elements.

Intersection Prioritization Exercise

Description

To assess intersection improvements, participants were shown a map with 10 major intersections identified as locations which may need bicycle and pedestrian safety enhancements. People were asked to place three (3) stickers in any of the boxes associated with the intersections which they believed required the most urgent improvements or were their top priority. Additionally, attendees could suggest other intersections they felt needed attention, beyond those identified by the project team. Two (2) other intersections were identified by the public: The intersection of 81st Avenue S. and CR 17; and the intersection of Chestnut Drive and CR 17.

Feedback Received

Figure 7. Intersection Improvements

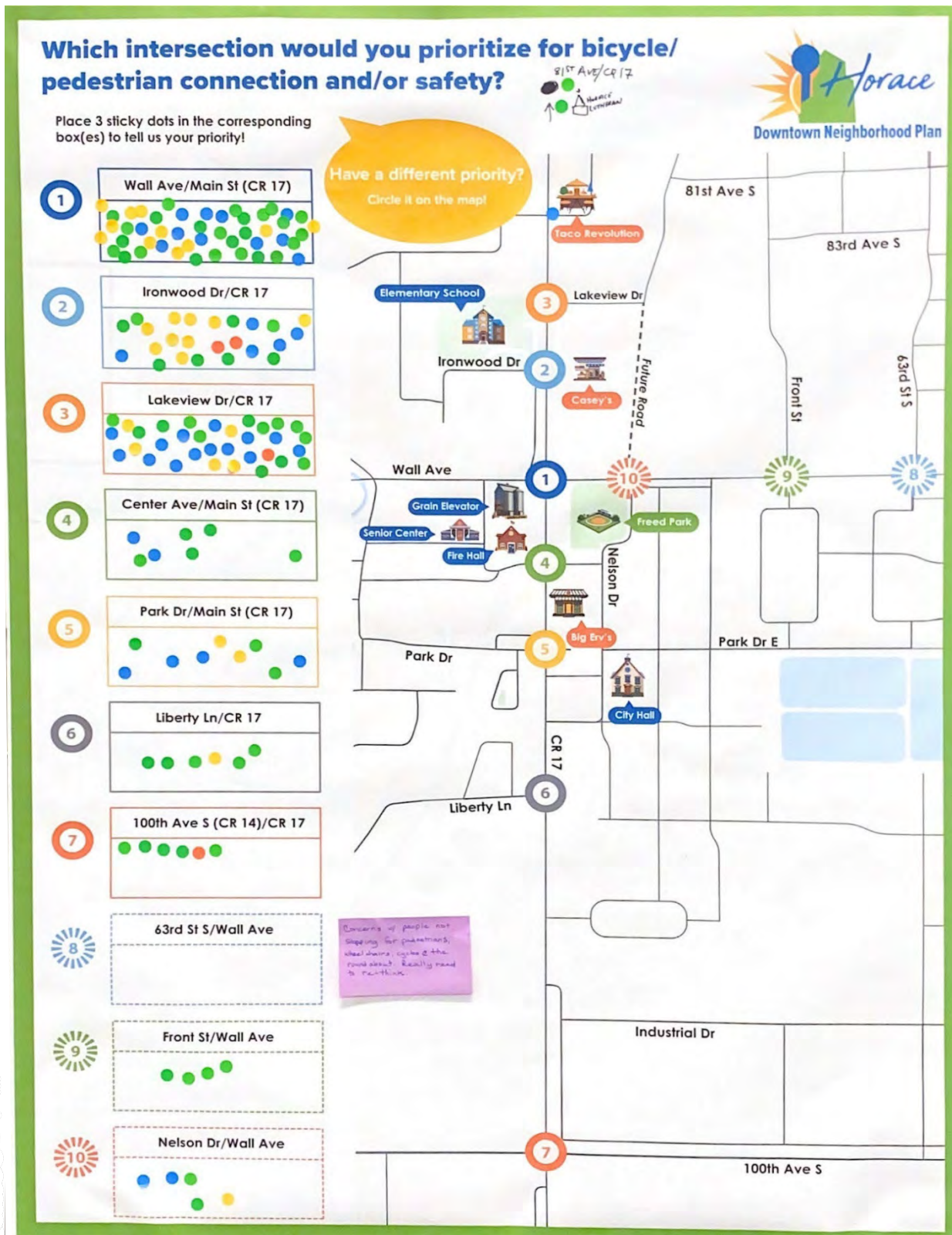
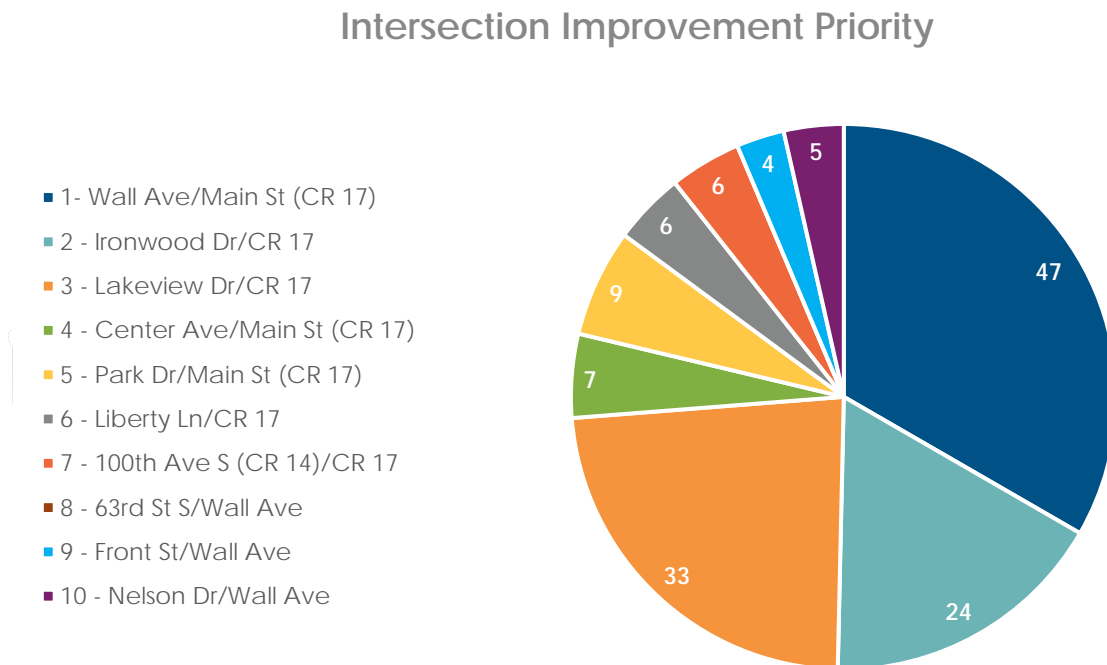


Figure 8. Intersection Improvement Priority



Key insights:

Top priority intersections:

1. [ID 1] Wall Ave/Main St (CR 17) – The most critical intersection identified, likely due to high traffic volume and pedestrian safety concerns.
2. [ID 3] Lakeview Dr/CR 17 – Another high-priority area with strong demand for safer crossings and infrastructure.
3. [ID 2] Ironwood Dr/CR 17– Ranked third in priority, showing the need for safety improvements due to its proximity to Elementary School.

Moderate priority intersections:

4. [ID 5] Park Dr/Main St (CR 17)
5. [ID 4] Center Ave/Main St (CR 17)
6. [ID 6] Liberty Ln/CR 17
6. [ID 7] 100th Ave S (CR 14)/CR 17

Low Priority Intersections:

6. [ID 10] Nelson Dr/Wall Ave
7. [ID 9] Front St/Wall Ave
8. [ID 8] 63rd St S/Wall Ave – No participants identified this location as a priority.

Additional intersections identified:

- 81st Ave/CR 17
- Chestnut Dr/CR 17

Intersection Comments Received:

- "Concerns w/ people not stopping for pedestrians, wheelchairs, cycles @ the roundabout. Really need to re-think."

Bicycle/Pedestrian Connectivity Exercise

Description

This activity focused on key routes that could improve biking and walking connectivity in Horace. Participants were provided with a map highlighting seven major routes identified as critical for bicycle and pedestrian connectivity. They placed three stickers in any order in the boxes corresponding to the connections they believed should be prioritized. Similar to the intersection exercise, attendees could also suggest additional routes that were not included in the project team's initial list. Several other routes were identified by the public: East side of CR 17 between 76th Avenue S and 64th Avenue S; along 64th Avenue S; along 76th Avenue S, west of CR 17; from Sunset Drive, across the Sheyenne River to Meadowlark Park; from Sunset Drive to the east/northeast.

Feedback Received

Figure 9. Bicycle/Pedestrian Connectivity

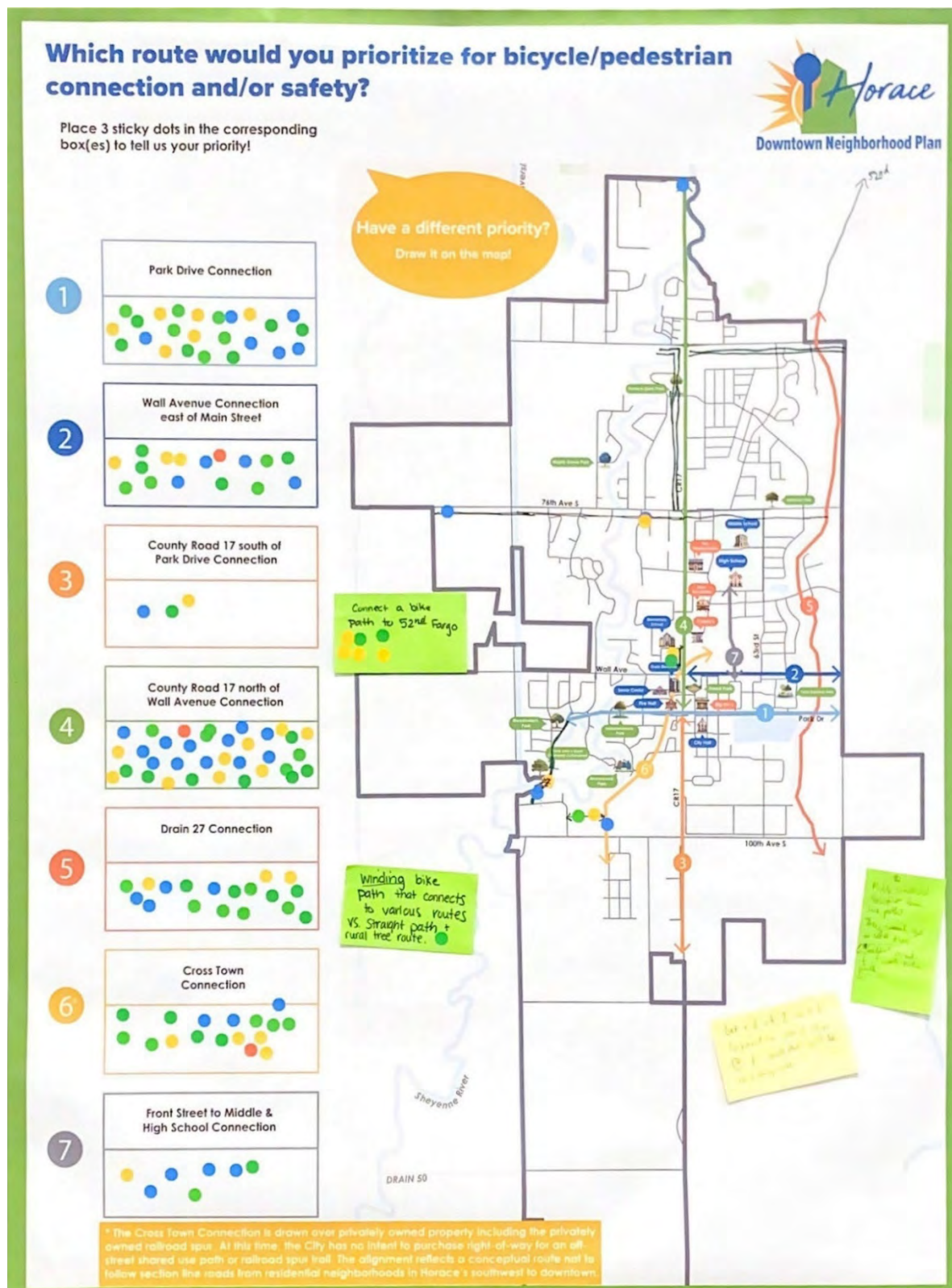
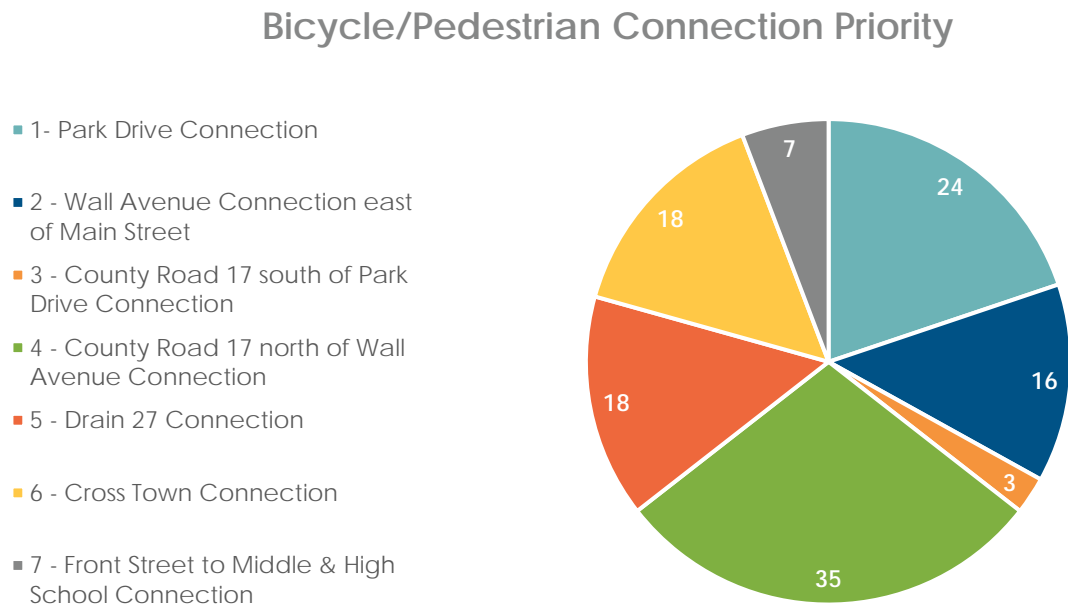


Figure 10. Bicycle/Pedestrian Connection Priority



Key Insights:

Top priority routes:

1. [ID 4] County Road 17 north of Wall Avenue Connection – The most prioritized route with strong demand for improved pedestrian and bicycle infrastructure along this corridor.
2. [ID 1] Park Drive Connection – A significant priority, due to its potential to enhance accessibility.

Moderate priority routes:

3. [ID 5] Drain 27 Connection – A well-supported route, due to its potential for scenic and safe biking/walking connections off of major roads.
3. [ID 6] Cross Town Connection – Another notable priority with east-west connectivity potential.
4. [ID 2] Wall Avenue Connection east of Main Street – Some support, though concerns were raised about high traffic volumes in this area.

Lower priority routes:

5. [ID 7] Front Street to Middle & High School Connection – Not a top priority but still could provide benefits for students and families.
6. [ID 3] County Road 17 south of Park Drive Connection – The least prioritized connection.

Additional routes identified:

- East side of CR 17 between 76th Avenue S and 64th Avenue S
- Along 64th Avenue S

- Along 76th Avenue S, west of CR 17
- From Sunset Drive, across the Sheyenne River to Meadowlark Park
- From Sunset Drive to the east/northeast.

Community Concerns & Suggestions:

- Strong support for more bike paths to enhance the rural aesthetic.
- Suggestions for adding a kids' splash pad connected to bike paths/parks.
- Concerns that Wall Avenue will be too busy for a pedestrian-focused route, with some recommendations to remove the Wall Ave and Cross Town connections.
- One comment from homeowner along the rail corridor for creating a pedestrian/walking connection (non-motorized only).

Connection Comments Received:

- "It would be great to have a kids splash pad that connects to bike path/park."
- "Connect a bike path to 52nd Fargo" [6 additional priority votes]
- "Winding bike path that connects to various routes vs. straight path + rural tree route." [1 additional priority vote]
- "#2. Kids would benefit from the path. They go to 100th Ave"
- "#6. Wouldn't mind if was a bike path"
- "Get rid of 2 and 6 connection would stop @ #1 Wall Ave. Will be very busy area"

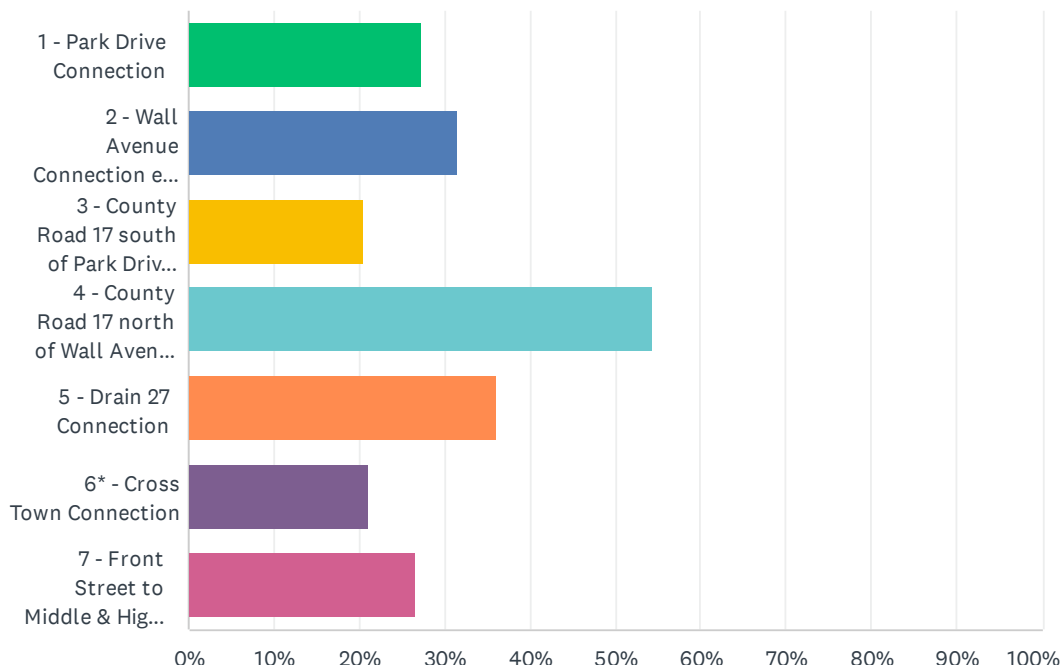
Additional Comments

By completing additional comment forms, respondents expressed a strong interest in enhancing public spaces, improving pedestrian safety, and expanding transportation options in Horace. Key comments include:

- Desire for a larger community center to accommodate events and support the town's growing population.
- Support for repurposing the grain elevator into a functional space, incorporating green areas and pedestrian-friendly features.
- Concerns about safety at the CR 17 and CR 14 intersection.
- Support for a walking/biking path along the old railroad tracks, providing a route into town from the north and south.
- Emphasis on the need for better walking path connections along Main Street.
- Calls for more green space, especially south of town.
- Demand for paratransit services, especially for homebound residents who currently rely on family for transportation.
- Concerns about 100th Avenue S safety for pedestrians and cyclists, with opposition to further commercial development in the area.

Q1 Which route(s) would you prioritize for bicycle/pedestrian connection and/or safety? Please select your top three priorities

Answered: 314 Skipped: 13



ANSWER CHOICES	RESPONSES	
1 - Park Drive Connection	27.39%	86
2 - Wall Avenue Connection east of Main Street	31.53%	99
3 - County Road 17 south of Park Drive Connection	20.38%	64
4 - County Road 17 north of Wall Avenue Connection	54.46%	171
5 - Drain 27 Connection	35.99%	113
6* - Cross Town Connection	21.02%	66
7 - Front Street to Middle & High School Connection	26.75%	84
Total Respondents: 314		

#	DO YOU HAVE ANY ADDITIONAL COMMENTS TO SHARE?	DATE
1	I like all the bike trails we are adding and we should keep it up! Moorhead plows their bike lanes in the winter so that they can be used as cross country ski trails. Maybe that is something we can consider here? I also really like the trail lighting added to some of the communities around the high school, but I would prioritize more trails over lighted trails. I want to make sure we are being fiscally responsible and considering long-term maintenance, but at the same time working within a growth mindset that creates a better quality of living for those in Horace. Frolf course maybe? Lol	2/12/2025 9:20 AM
2	We need a safe way to get kids further into town (who bike or walk in groups) without having to cross highways (we are south of 100)	2/11/2025 9:46 PM

Horace Downtown Neighborhood Plan Community Survey #2, Future Options

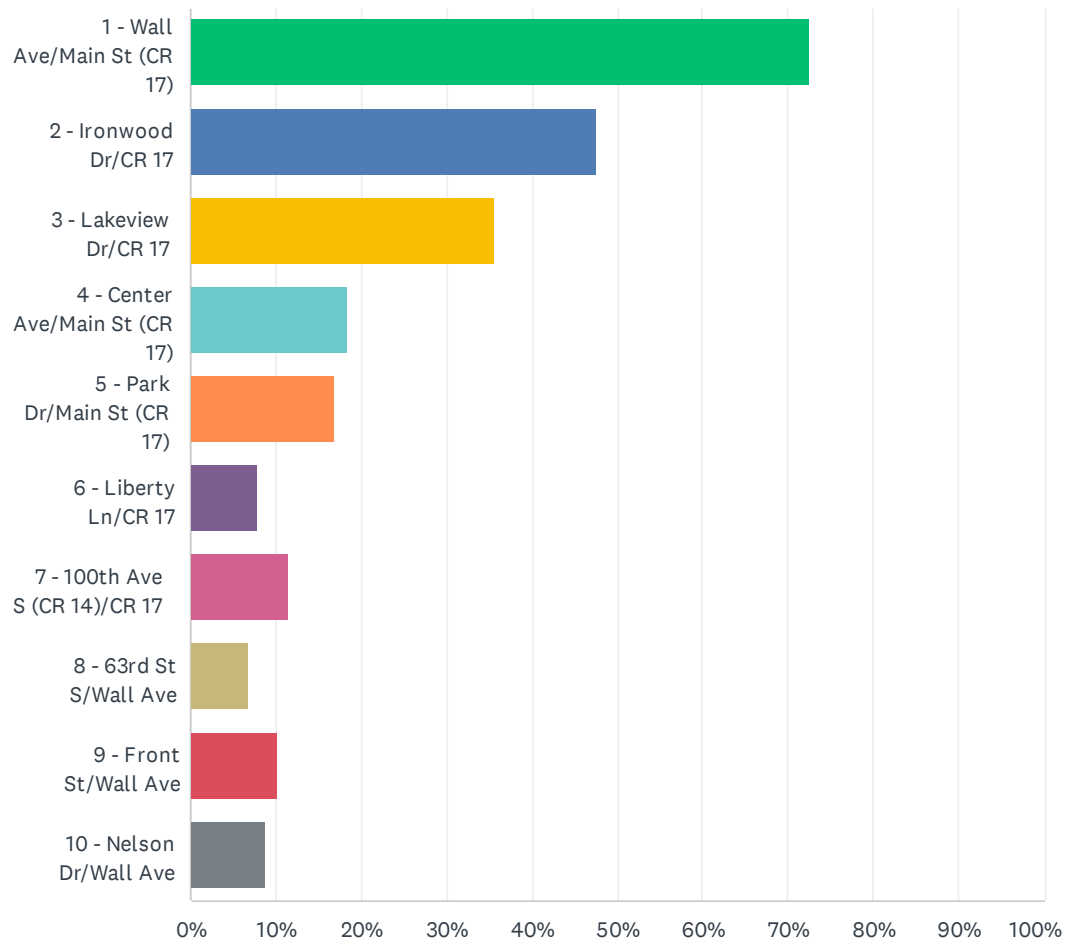
3	We only asked multiple times for something to be done about the cub creek development, 79th and 63rd should be a school zone	2/11/2025 9:04 PM
4	Would be nice to have a safe way to get from East to West across Sheyenne. Where will grocery store go? Bicycle and pedestrian routes should be able to access this.	2/11/2025 8:19 PM
5	Prioritize youth going to local places and school safely on bike paths.	2/11/2025 8:10 PM
6	Any of these 3 is a good option, but #1 seems to be far and away the best at this time	2/11/2025 7:25 PM
7	Why are you trying to make this town part of west fargo, specials are through the roof and now your gonna add more by doing this, no wonder families only stay in there new house for 4-5 years	2/11/2025 6:52 PM
8	The diversion will have trails and roads that should be connected to, the city should follow up with ASN Constructors for details	2/11/2025 4:49 AM
9	There is no reason the city of horace should not own the cross town connection path property... it was a mistake they didn't take advantage of it when the railroad property was annexed by the diversion project. Dumb!	2/11/2025 12:26 AM
10	Kids riding from the Terra Gardens neighborhood Do not have a good way to get to the elementary school!	2/10/2025 8:17 PM
11	I feel like #4 is already being done with the sidewalks, but pedestrian safety such as cars following 25mph is crucial and more crosswalk paintings on the main road with flashers too. Decreased speed to 25 until the first roundabout as well. I said #4, but also 5 would be a very neat scenic route.	2/10/2025 6:59 PM
12	It would be nice to have a safe bike trail from the southern developments (Maple Lake Estates) into town	2/10/2025 5:25 PM
13	Keep people & bikes off County 17	2/10/2025 2:25 PM
14	Is there a potential for a bike path parallel with 76th ave west of the round a bout? I see a lot of people walking there and speeds are much higher on that road	2/10/2025 12:55 PM
15	More trees	2/10/2025 10:42 AM
16	Keep the bike lanes off the roads, they are already narrow enough	2/10/2025 10:26 AM
17	As Horace builds farther south, keep sidewalks and bike paths in mind. Residential and industrial are separated by main the farther south you go, but depending on what gets built up that way, there's no clear path for any walking/biking. Once again, it depends on what gets built up there but houses are coming from Liberty Lane on out and there should be some trails/walkways/green spaces outside of town.	2/10/2025 5:52 AM
18	Just be smart with the monies. Make sure the impact from a route is designed with the required maintenance involved.	2/9/2025 11:04 PM
19	Safe for kids to walk to school if needed.	2/9/2025 10:41 PM
20	Don't believe routes will provide any real benefit to Horace residents. And find it convenient that the city is attempting to bypass MFDA policy to purchase the rail corridor over the rights to adjacent landowners. Yet this form specifically says the city has no intention of purchasing. If it wasn't for irresponsible growth, the city would have no use for this rail corridor.	2/9/2025 10:31 PM
21	Any way yy guys will keep the tornadobsirens	2/6/2025 5:49 PM
22	The most direct routes N-S should be prioritized. Not only will this give the Horace community the most use, but it will help encourage others from Fargo, West Fargo to come out and visit Horace and stimulate tourism.	1/29/2025 8:33 AM
23	Mostly 4	1/28/2025 9:18 PM
24	I would be nice to connect County Road 17 West on 76th Ave to 81st St S. I see so many people walking, biking on there and it can be super dangerous with cars, trucks and heavy machinery flying by.	1/28/2025 8:53 PM
25	I don't see the point of including options that run across private land.	1/28/2025 8:50 PM

Horace Downtown Neighborhood Plan Community Survey #2, Future Options

26	#6 would create access for the southern most development to have access to town allowing safer access for kids to travel to and from.	1/28/2025 7:09 PM
27	Connecting to the greater part of West Fargo should be the priority. Families and people should have the opportunity to bike to and from places like the Wilds, Deer Creek, Eagle Run, etc to downtown Horace.	1/28/2025 7:08 PM
28	Personally, I like all of the route options proposed. If these conceptual routes could all be stitched together over time I think that would be beneficial for the community	1/24/2025 11:46 AM
29	In order of priority, 6, 5, 1. Even though the cross town connection is currently a rail spur, there is a successful precedent within the area of conversion to a pedestrian/bikeway path. Connecting this path to the drain 27 connection will enable alternative transportation methods throughout Horace/Fargo as development continues, and connecting the park drive connection throughout the current center of Horace provides the best use path connecting the previous routes while focusing on Horace. Alternative methods of transportation instead of driving are key characteristics of a small town/rural environment. Heavy car traffic is better left to arterial routes and detract from the small town feel. Converting the rail spur to engage with downtown Horace and its surrounding neighborhoods is crucial to the wellbeing of this development plan.	1/24/2025 11:16 AM
30	Walking routes for kids to walk to school.	1/23/2025 9:54 PM

Q2 Which intersection(s) would you prioritize for bicycle/pedestrian connection and/or safety? Please select your top three priorities

Answered: 295 Skipped: 32



Horace Downtown Neighborhood Plan Community Survey #2, Future Options

ANSWER CHOICES	RESPONSES	
1 - Wall Ave/Main St (CR 17)	72.54%	214
2 - Ironwood Dr/CR 17	47.46%	140
3 - Lakeview Dr/CR 17	35.59%	105
4 - Center Ave/Main St (CR 17)	18.31%	54
5 - Park Dr/Main St (CR 17)	16.95%	50
6 - Liberty Ln/CR 17	7.80%	23
7 - 100th Ave S (CR 14)/CR 17	11.53%	34
8 - 63rd St S/Wall Ave	6.78%	20
9 - Front St/Wall Ave	10.17%	30
10 - Nelson Dr/Wall Ave	8.81%	26
Total Respondents: 295		

#	DO YOU HAVE ANY ADDITIONAL COMMENTS TO SHARE?	DATE
1	63rd and 79th	2/11/2025 9:05 PM
2	This intersection is a nightmare for cars in the morning, let alone anyone on a bike path and deserves high priority connect school age children the opportunity to cross safely.	2/11/2025 8:12 PM
3	Na. Leave alone	2/11/2025 3:38 PM
4	63rd st s and 79th ave s!!!!!!!!!!!!!!!!!!!!p!!	2/10/2025 10:43 AM
5	Either 3 or 2. It is a mad house when I drop my child off at Kid Ventures and there's no light protected turns to slow traffic. Not sure if it warrants anything but just an observation I've seen.	2/10/2025 5:54 AM
6	Don't believe routes will provide any real benefit to Horace residents. And find it convenient that the city is attempting to bypass MFDA policy to purchase the rail corridor over the rights to adjacent landowners. Yet this form specifically says the city has no intention of purchasing. If it wasn't for irresponsible growth, the city would have no use for this rail corridor.	2/9/2025 10:32 PM
7	The crossing between the middle/high schools and elementary should absolutely be priority. I teach at Horace and crossing that road is so dangerous.	1/28/2025 7:09 PM
8	I believe improvements close to the elementary school is a priority. Additionally, the intersection of CR-17 and CR-14 could be a traffic and pedestrian safety hazard in the future. Would like to see improvements at that intersection	1/24/2025 11:51 AM
9	Why are all these options confined to Main street? If the development plan to use this as an arterial or collector connection across Horace, that is in direct conflict with pedestrian/bicycle safety. It is my belief that from the water tower to the elementary school, this segment of road should focus heavily on modes of transportation other than cars. Vehicles are loud and given the straight alignment of Main street, travel speeds will likely be at speeds that contrast the interest of pedestrian travel. What is the city's priority, people or cars? Main street cannot and should not be both.	1/24/2025 11:21 AM
10	Crosswalks for kids going to the Kindred bus and students going to school.	1/23/2025 9:58 PM
11	The round about on 76th. Why is only a portion of the city shown? A true study would encompass the entire town.	1/23/2025 2:21 PM

Q3 Commercial, Retail, & Services: We heard a lot of comments about desired commercial, retail, and service businesses. Click anywhere on the image below to indicate the style and scale of retail that you prefer.

Answered: 245 Skipped: 82



Q4 Major Intersections: A majority of people want accessibility across town by walking, biking, and driving. Click anywhere on the image to indicate the intersection configuration that fits the character of Horace best.

Answered: 240 Skipped: 87



Q5 Commercial Corridors: We heard a lot about family-friendliness and accessibility for all ages. Adjacent development (land use) impacts the character and cost of the transportation system and vice versa. Click anywhere on the image to indicate the scale of commercial corridor that fits the character of Horace best.

Answered: 240 Skipped: 87



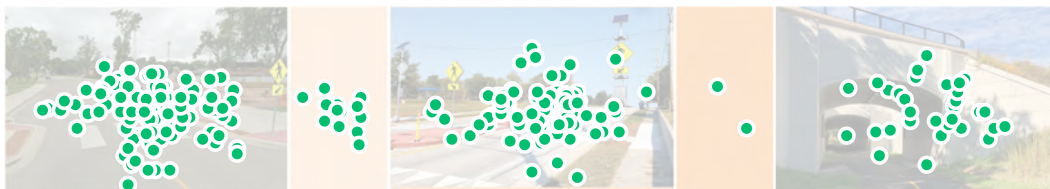
Q6 Pedestrian & Bicycle Connections: The Plan considers all ways of getting around, including through walking and bicycling. Click anywhere on the image to indicate the scale of bike and pedestrian connection that fits the character of Horace best.

Answered: 249 Skipped: 78



Q7 Pedestrian & Bicycle Crossings: Downtown Horace is already set up to be very walkable however, connections to get there require crossing different streets. Click anywhere on the image to indicate the scale of bike and pedestrian crossing that fits the character of Horace best.

Answered: 245 Skipped: 82



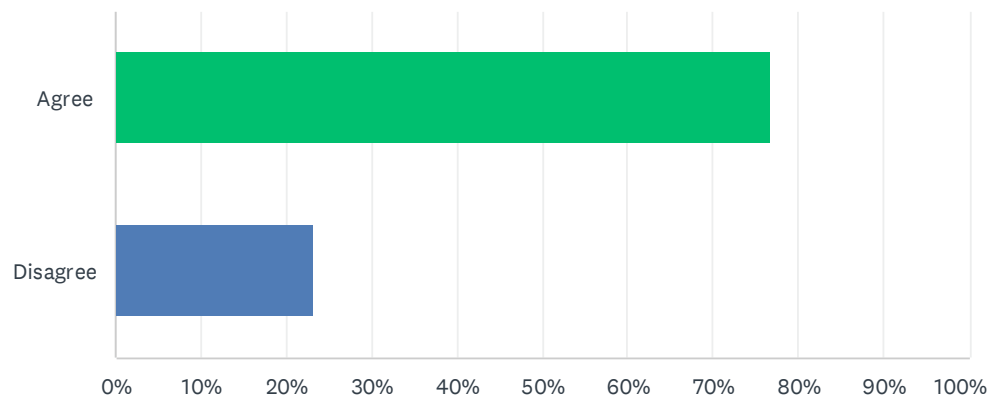
Q8 Streetscaping: Streetscaping reflects community values and identity. This may include street trees, benches, trash cans, bollards, patterned concrete, lights, etc. Click anywhere on the image to indicate the scale of streetscaping that fits the character of Horace best.

Answered: 247 Skipped: 80



Q9 This plan fits Horace’s rural, small-town identity.

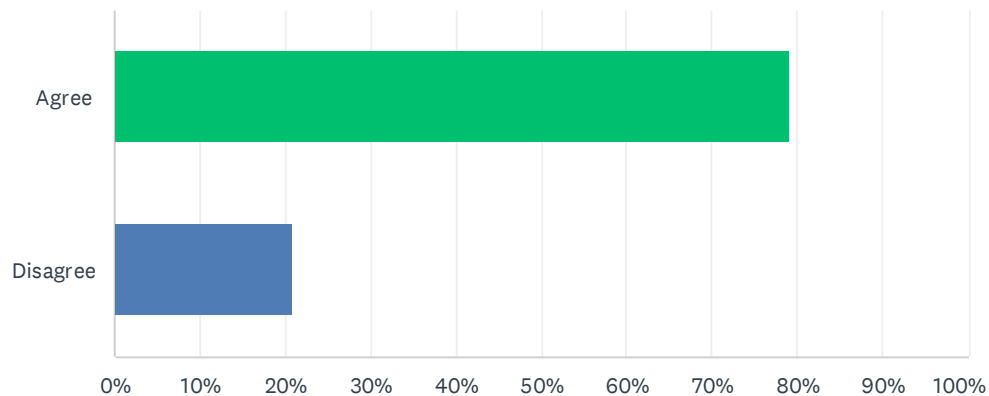
Answered: 229 Skipped: 98



ANSWER CHOICES	RESPONSES	
Agree	76.86%	176
Disagree	23.14%	53
TOTAL		229

Q10 This plan will make it easier to walk, bike, and drive in and around the downtown area.

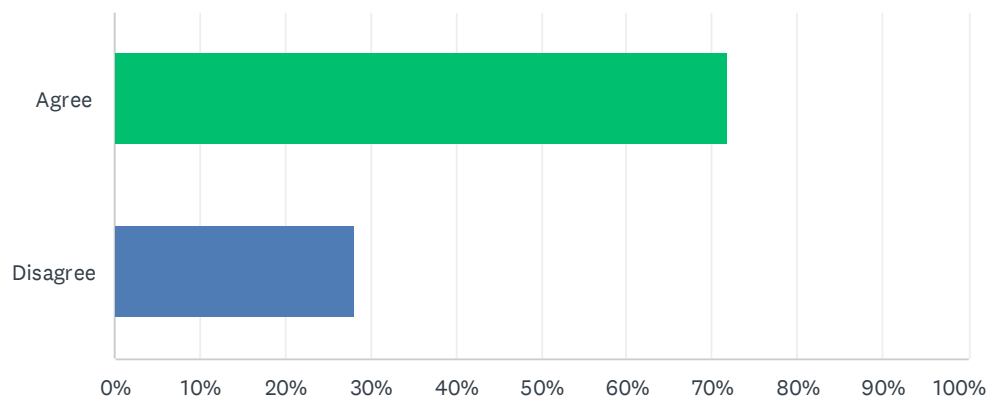
Answered: 229 Skipped: 98



ANSWER CHOICES	RESPONSES	
Agree	79.04%	181
Disagree	20.96%	48
TOTAL		229

Q11 This plan will promote a high quality of life without detracting from the small-town feel that we want.

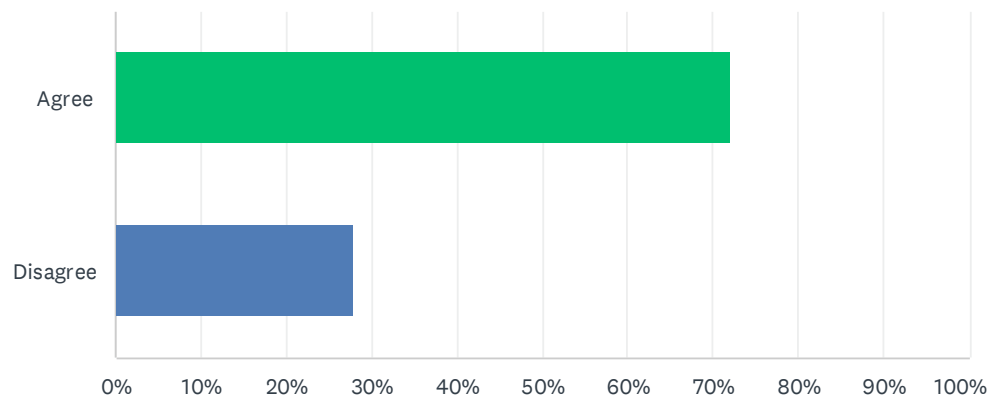
Answered: 228 Skipped: 99



ANSWER CHOICES	RESPONSES	
Agree	71.93%	164
Disagree	28.07%	64
TOTAL		228

Q12 This plan has lots of activities for all ages.

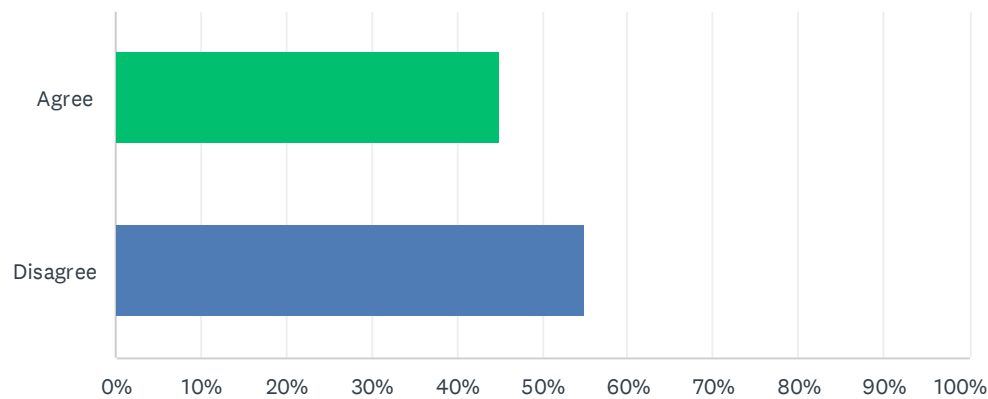
Answered: 225 Skipped: 102



ANSWER CHOICES		RESPONSES	
Agree		72.00%	162
Disagree		28.00%	63
TOTAL			225

Q13 This plan fits Horace’s rural, small-town identity.

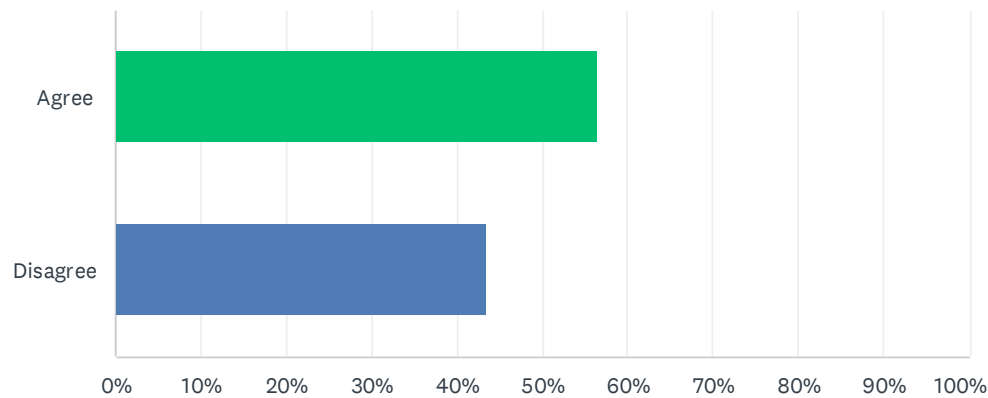
Answered: 227 Skipped: 100



ANSWER CHOICES	RESPONSES	
Agree	44.93%	102
Disagree	55.07%	125
TOTAL		227

Q14 This plan will make it easier to walk, bike, and drive in and around the downtown area.

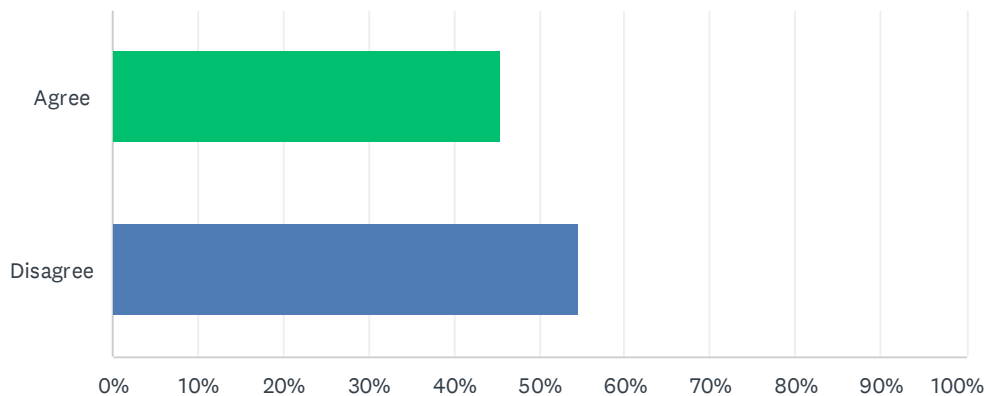
Answered: 225 Skipped: 102



ANSWER CHOICES		RESPONSES	
Agree		56.44%	127
Disagree		43.56%	98
TOTAL			225

Q15 This plan will promote a high quality of life without detracting from the small-town feel that we want.

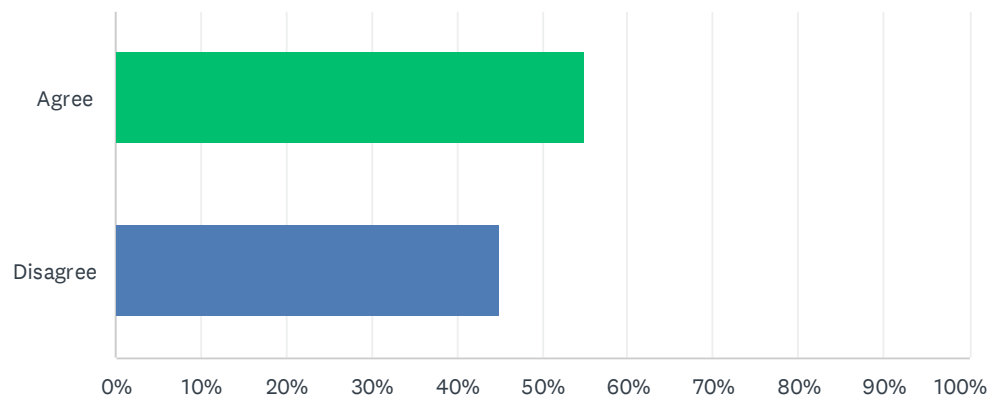
Answered: 222 Skipped: 105



ANSWER CHOICES	RESPONSES	
Agree	45.50%	101
Disagree	54.50%	121
TOTAL		222

Q16 This plan has lots of activities for all ages.

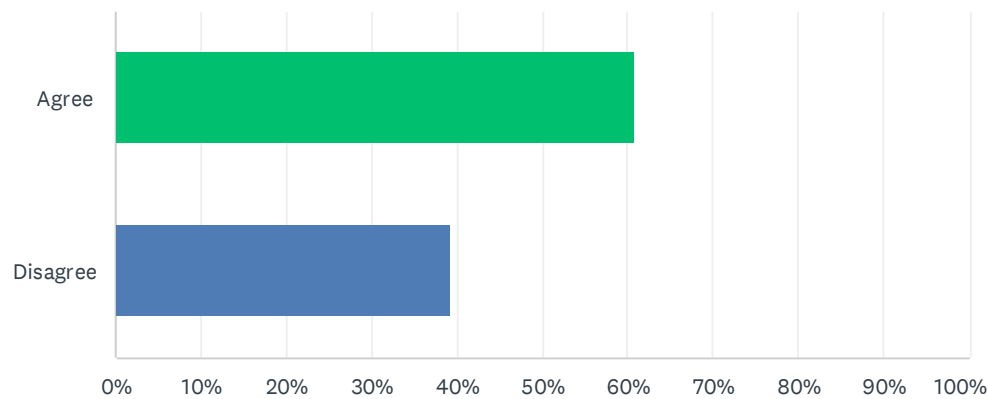
Answered: 220 Skipped: 107



ANSWER CHOICES		RESPONSES	
Agree		55.00%	121
Disagree		45.00%	99
TOTAL			220

Q17 This plan fits Horace’s rural, small-town identity.

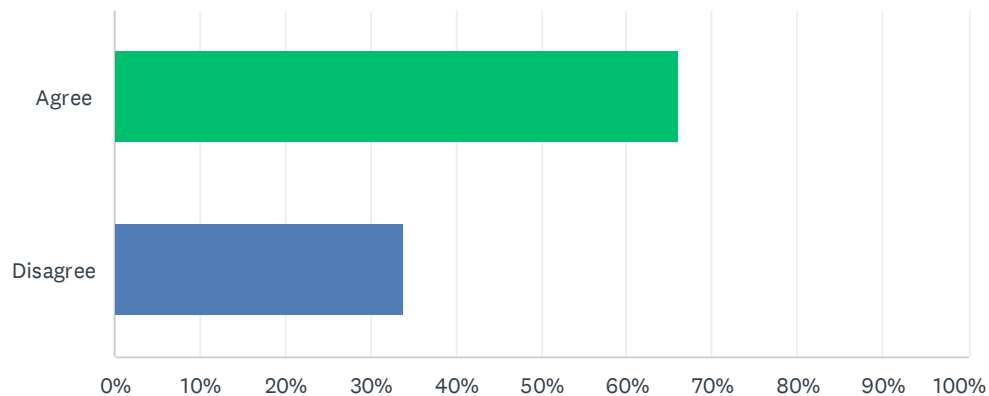
Answered: 229 Skipped: 98



ANSWER CHOICES	RESPONSES	
Agree	60.70%	139
Disagree	39.30%	90
TOTAL		229

Q18 This plan will make it easier to walk, bike, and drive in and around the downtown area.

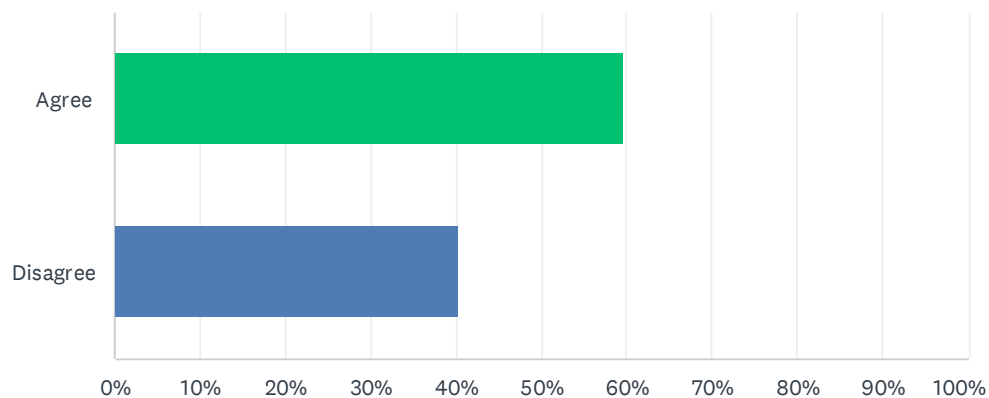
Answered: 227 Skipped: 100



ANSWER CHOICES	RESPONSES	
Agree	66.08%	150
Disagree	33.92%	77
TOTAL		227

Q19 This plan will promote a high quality of life without detracting from the small-town feel that we want.

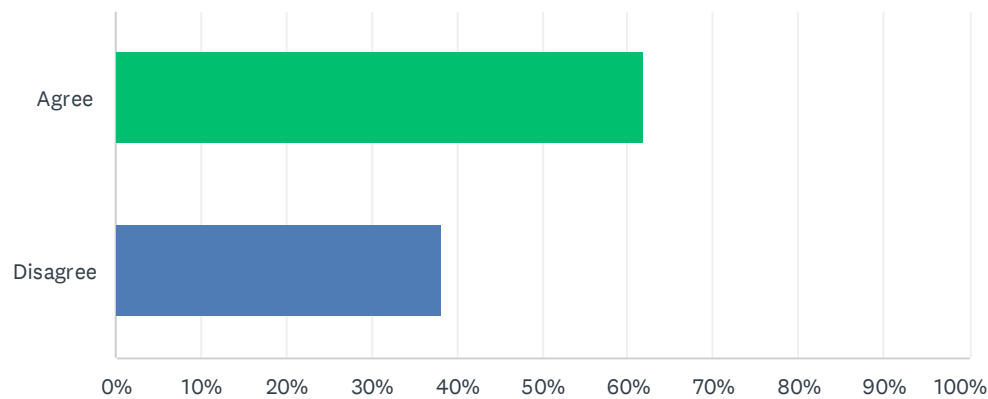
Answered: 228 Skipped: 99



ANSWER CHOICES	RESPONSES	
Agree	59.65%	136
Disagree	40.35%	92
TOTAL		228

Q20 This plan has lots of activities for all ages.

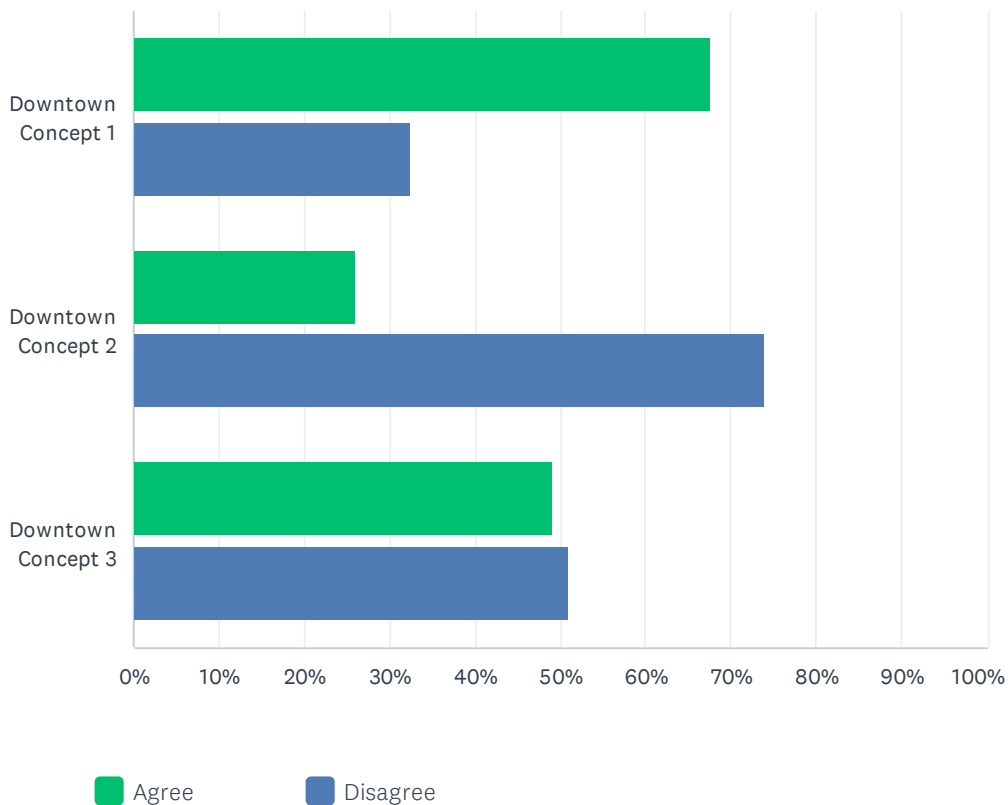
Answered: 225 Skipped: 102



ANSWER CHOICES		RESPONSES	
Agree		61.78%	139
Disagree		38.22%	86
TOTAL			225

Q21 After reviewing all three downtown concepts, we'd like to know which plan(s) meet your vision for Horace's future.

Answered: 237 Skipped: 90



	AGREE	DISAGREE	TOTAL	WEIGHTED AVERAGE
Downtown Concept 1	67.54% 154	32.46% 74	228	1.32
Downtown Concept 2	25.99% 59	74.01% 168	227	1.74
Downtown Concept 3	49.13% 113	50.87% 117	230	1.51

Q22 Do you have any additional comments or suggestions for the Horace Downtown Neighborhood Plan?

Answered: 75 Skipped: 252

#	RESPONSES	DATE
1	I think we should do our best to keep the grain elevator as it has become an icon of the town. I remember growing up here and thinking how cool it was as a kid. Maybe it can be repurposed into a museum or learning center? A farmers market area would be great to attract the locals and it would fit our character well. I also think we need to prioritize a central park with lots of walkability, but we need to provide flex-zone areas for both commercial and housing to coexist. This is a great option for aging populations, but also those who want to live in the area next to all the happenings, but don't want to own a large home. Maybe we can consider co-op condos or another form of housing attached to commercial storefronts.	2/12/2025 9:35 AM
2	Keeping the grain elevator structures seems imperative to keeping the face of Horace alive and recognizable.	2/12/2025 8:23 AM
3	No, but I know it has been brought up at least 100 times. New development people are always going way too fast! Something need to be figured out!	2/11/2025 10:50 PM
4	Love the idea of farmers market! Good to get a little bigger, but not too crazy. Clean up some of the run down looking areas would be nice too. What population is needed for our own police station, that would be nice as well. Need a grocery store.	2/11/2025 9:33 PM
5	Combine plan #1 and Plan #3. We NEED to keep our grain elevators period! Add a downtown trail to plan #1 and keep all additional sidewalks.	2/11/2025 9:03 PM
6	Increase speed limits or add more lanes. It is way too long of time driving to get out of Horace since there is too many people driving at once during busy hours.	2/11/2025 8:06 PM
7	I don't really care with keeping the elevator look, but #3 is my favorite	2/11/2025 7:45 PM
8	I really like the idea of having a farmers market in the downtown plan. For all concepts I feel that should be included. That gives a huge community small town feel.	2/11/2025 7:32 PM
9	Stop coordinating with metrocoog	2/11/2025 3:39 PM
10	Fix sheyenne dr and dakota ave first. Can't drive down either over 15mph after poor repair work and devolving potholes it's ridiculous that every new neighborhood i. Town gets perfect streets but the older neighborhoods drive on this trash. Both of these streets have been redone in the last 5 yrs and the quality of work from the repairs is horrendous	2/11/2025 2:54 PM
11	Grocery store	2/11/2025 1:51 PM
12	I moved out here for small town feel and cheap taxes. Not for taxes to go up for it to feel like dargo	2/11/2025 12:21 PM
13	I think a combination of 2 and 3, the access for the fire station needs to be reviewed as that needs to remain for calls and the growth as the city gets larger. There is more need for restaurants, gym, and a grocery store as well which will need a bigger lot. The grain elevator will cost the tax payers to much money to restore for the minimal use of space that would be inside.	2/11/2025 4:55 AM
14	Don't put in any more round-a-bouts...	2/11/2025 12:32 AM
15	Tear down the elevators.	2/10/2025 10:27 PM
16	I grew up in Horace and moved back here with my husband and children. As much as I love to see the city grow, I do not want to see Horace change from the small town I love.	2/10/2025 10:03 PM
17	Highly suggest no more roundabouts (even the mini one), they are so slippery in the winters and I truly feel it's unsafe for pedestrians, especially near the elementary school where hopefully more kids will bike or walk from school when able. One stop sign town is the	2/10/2025 7:37 PM

Horace Downtown Neighborhood Plan Community Survey #2, Future Options

definition of "small town". I love the look of #1, but incorporating the downtown trail connection from park dr to senior citizen center from #3 & trail connection by elementary school on #2 would make the "perfect small town downtown" in my eyes. All I see in the pictures is a lot of planning for parking. More parking=more cars=less safe for kids and pedestrians. Less buildings, more open downtown community space, more small town feel. I highly suggest thinking long on the intersection of wall Avenue to what will be the safest for families crossing that area and heading to school. Also those adjacent neighborhoods near south end of town and safety of those cars pulling out with all the parking planned. Makes hard for blind spots. Keep speed limit to \$25!!!

18	The elevator in town is a must to keep, as it's a staple of downtown horace. Also can we please go away from roundabouts? To most that I've talked to we cannot stand them. Also could we widen the road from Kaseys heading north into west fargo to help with all the traffic heading north in the morning and south in the evening.	2/10/2025 7:18 PM
19	Think elevator needs to be taken down.	2/10/2025 6:20 PM
20	Get rid of the old vermin infested elevators.	2/10/2025 4:56 PM
21	Less is better, Concept 2 and 3 seem to cram a lot of buildings into the redeveloped grain elevator space. Concept 1 has some buildings and more green space which gives it the more small town feel that Horace should be.	2/10/2025 2:49 PM
22	Horace needs a small grocery store and to develop one of the areas in to a pool for the community or into a golf course with a pool and clubhouse for the community. Not many want more businesses other than a grocery store. Everything else is available in Fargo. We only want community assets, not more businesses. More businesses and apt building bring more problems. Mark my words that within the next two years you start to see lots of burglaries and thefts from motor vehicle. Almost unheard of until Horace began pushing for more businesses and twin homes and apts. Now those words, burglary and theft from motor vehicle are becoming more common. We will lose our sense of community and decrease public safety....	2/10/2025 1:41 PM
23	I feel roundabouts are the most unsafe thing with pedestrians and bikers. Neither the car nor the pedestrian feel they need to stop and the car is going at a higher speed than they would at a stop sign or crosswalk with blinking lights	2/10/2025 1:17 PM
24	Please repurpose the elevators!! And do improvements to the senior center. They are such staples here.	2/10/2025 11:33 AM
25	Clean up the roads and sidewalks (intersections) keep it how it is	2/10/2025 11:06 AM
26	Make the older residential roads as wide as the newer residential roads. Make sure you do something with the stench off the poop pond before more people are added to Horace-MOST important.	2/10/2025 10:41 AM
27	Thanks for making survey!	2/10/2025 5:59 AM
28	Leave it the way it is and quit trying to make us Fargo. Stop with the apartments and keep us a house community.	2/9/2025 11:08 PM
29	The only thing would be nice to have a community poop and maybe a small grocery store but besides that we don't need a downtown feel.	2/9/2025 10:47 PM
30	Don't believe routes will provide any real benefit to Horace residents. And find it convenient that the city is attempting to bypass MFDA policy to purchase the rail corridor over the rights to adjacent landowners. Yet this form specifically says the city has no intention of purchasing. If it wasn't for irresponsible growth, the city would have no use for this rail corridor.	2/9/2025 10:35 PM
31	Na	2/9/2025 10:20 PM
32	Putting more downtown makes it feel like a part of Fargo.	2/9/2025 9:41 PM
33	Stop spending so many tax payers dollars. Just because you're a city doesn't mean you should spend money like it's going out of style.	2/9/2025 8:56 PM
34	Keep businesses downtown. Place for all business to be conducted.	2/9/2025 1:54 PM
35	Please keep and plant trees. The small town space is welcoming. Do not cram buildings on top of each other. Would rather have less with more space. Stop putting apartments up. Keep some "greenspace" to help avoid the urban jungle feeling. I love coming home after being	2/8/2025 4:43 PM

Horace Downtown Neighborhood Plan Community Survey #2, Future Options

stuck in Fargo/West Fargo, the space and trees are therapeutic. Also, please please please stop with the cookie cutter homes that have zero yard space, what happened to all the rules about apartments and row homes?

36	Downtown Concept 3 seems to strike the perfect balance of added downtown commercial spaces, historic preservations, and community spaces.	2/8/2025 10:39 AM
37	Planning for retail expansion will be key for sucesess	2/8/2025 9:55 AM
38	Please tell me you will keep the sirens and keep the 12 am test 6!pm test and 9pm test thanks	2/6/2025 5:54 PM
39	Need a grocery store outside of downtown area. Need to balance safety with traffic flow. A downtown "destination" area for events would be nice with a park nearby and restaurants and bars.	2/4/2025 8:23 AM
40	I think all three of these plans are fantastic but #3 is my favorite!	1/30/2025 5:42 PM
41	I prefer design 2 over the other options. I really don't see the need for the grain elevator for numerous reasons: it's old, regular maintenance has not been done to it and it just doesn't have any historical significance from what I can tell. It would be nice to add in a variety of new buildings in its place and could be built based on the communities vision and what we want for the future of Horace.	1/30/2025 9:14 AM
42	More trees!	1/29/2025 8:06 PM
43	Please quit trying to make horace west fargo or fargo people live here for a reason	1/29/2025 2:43 PM
44	Consider bringing Seniors and Children together with family friendly park with childrens playground equipment as bridge. Seniors and Children are a great natural fit. Kids have lots of energy Senior have lots of wisdom and patience. By placing a park between the grain elevator and the senior center it could be what ties all generations together and invites everyone to crossover. Working parents may arrange to meet a loved one at the senior center following a day of activies, they all go to park to watch kids having fun, then everyone walks downtown to grab a bite beer or root beer for the kids. Parents with older kids may leave them at the park with some sense of security knowing there will be watchful eyes nearby when people are at the senior center. I like the canted buildings that are all skewed and not parallel to the street in Option 3 - Its unique and place based (assuming the angle shown comes from something existing -cross street / building). It provides additional spaces that each business can program and activate street side without encroaching into sidewalk area. Concept 1 is by far the best and hope to see the elevators remain with additional community programming -- only reason I didn't mark agree to all is the programming for seniors (existing) and the kiddos felt disconnected and tucked far away from the redeveloped Main Street.	1/29/2025 1:08 PM
45	I like concept 1 best	1/29/2025 11:07 AM
46	I don't think a lot of green space is necessary downtown given the proximity to Freed Park and the potential park and public gathering spaces at Lakeview. Roundabout at Wall Ave makes sense but prefer standard intersections elsewhere. Would like to see a public library incorporated into downtown or the new city hall. Perhaps the Fargo or West Fargo libraries could open a branch library.	1/29/2025 10:06 AM
47	I feel 2 or 3 are the best options, but err on 3.	1/29/2025 8:42 AM
48	Too many buildings are proposed. Leaving more open area gives way to a small town feel.	1/28/2025 10:19 PM
49	Prioritize small town feel focusing on local businesses. Focus on safety of bike/walking paths for children traveling to school.	1/28/2025 10:02 PM
50	Less big chains more small businesses. Prioritize less gas stations and hideous car washes and more accessible visually pleasing small businesses	1/28/2025 9:58 PM
51	Would love to preserve the elevator in some fashion....iconic part of Horace that I feel will be the beginning of the end of the small town feel. It seems to make more sense to utilize small businesses to promote money in the community as opposed to a large public space, plenty of opportunities in future developments to do this elsewhere.	1/28/2025 9:34 PM
52	I like concept one the most but without the round about!	1/28/2025 8:58 PM
53	Create a plan they requires adequate drainage and mowed/trimmed spaces.	1/28/2025 8:09 PM

Horace Downtown Neighborhood Plan Community Survey #2, Future Options

54	Businesses are important but I do not view strip malls as small town. The ones we currently have take away from the charm or small town rural. In the last decade since we moved here, the appeal of rural feel has started to diminish. Our town has grown far too big too fast.	1/28/2025 8:08 PM
55	Please stop allowing builders to throw cheaply built homes in town while charging a fortune. Taxes and specials are out of control, no one is going to be able to afford to live here any longer. Horace has completely lost its small town feel.	1/28/2025 7:54 PM
56	Leave it alone. We want to keep our homes.	1/28/2025 7:40 PM
57	The option one has a huge potential to give Horace a specific identity through utilization of the grain elevator for a central location that the city is focused around	1/28/2025 7:35 PM
58	I really love the recreation of the grain elevator designs found in concept 1 and 3.	1/28/2025 7:28 PM
59	Horace does not need a "downtown". Nobody wants expensive downtown stores. We need more fast food restaurants and a grocery store.	1/28/2025 7:12 PM
60	Whomever is doing this market research, they are doing an amazing job!	1/28/2025 7:12 PM
61	The downtown sector should be North of 88th/Wall Ave. You are wasting tax dollars trying to redevelopment a neighborhood into commercial property.	1/28/2025 7:12 PM
62	All Horace needs is a grocery store. So many people love to live in Horace because of the small town feel. All of the city-like amenities are minutes away. No more specials.	1/28/2025 7:09 PM
63	Worry about it all the other growth problems Horace is having before making a "downtown" that NO ONE WANTS!!! I think it's very sad that you have to boot people out of their homes to make any of these plans work. I have lived here my entire life. Horace used to be an amazing place to live now it is somewhere I dread being.	1/26/2025 7:13 AM
64	No more roundabouts please	1/24/2025 3:58 PM
65	I think the grain elevator is a great identity feature of Horace. Repurposing it and the area around it would be beneficial to the community, keeping with the small town feel.	1/24/2025 11:57 AM
66	There are likes and dislikes to each concept. They are all okay, none are great. There are severe missed opportunities that can be addressed by a concept that is blend of the most successful characteristics of each concept. First, the rail spur between Center Avenue and Wall Avenue appears to be a parking alley or full parking lot. Why not a single side dedicated to parking, while the other focuses on a bike path and pedestrian connections to squares and stores? Second, each concept provides a different approach to engaging or repurposing the water tower and grain elevator. Concept 1 best addresses an adaptive reuse scenario for the grain elevator and this NEEDS to happen! The grain elevator is one of the best elements that expresses this is the center of Horace, preserve and reuse this structure instead of building a new facility that imitates what is already there and can be used. As for the water tower, why are all three options best use scenario around this space a parking lot? Would a public square between the water tower and grain elevator not be better utilized here? There obviously needs to be some parking around what is to be developed here but it absolutely should not be in this space. Finally, concepts two and three provide best use scenarios to develop what could be recognized as a downtown area. The alignment of the buildings on the west side to the current rail spur creates an engaging use of space. Combine this with the slightly more urban feel of building designs shown in concept two would tie together a proposal that is distinct to Horace and retains the small town and rural feel. The building heights feel appropriate and within proportion. A mix of revival style brick buildings similar to what Dilworth is creating with their rail district on the East edge of town, and one or two story pitched roof structures of various finishes has the potential to establish a unique look to Horace. Not as dense and urban as West Fargo, or as uniform as Dilworth. Absolutely not as tall as Fargo. Combining these aspects into a single concept is the most successful approach.	1/24/2025 11:44 AM
67	No round abouts. Tear roundabouts out and use traffic lights. One level buildings downtown. Don't make it look like your pathetic cookie cutter houses.	1/23/2025 10:03 PM
68	It seems like all the concepts are hemming in the fire station with parking that is not related to the fire hall. If the city grows as indicated the city will soon need a full time fire department. Where do they park? How do they move equipment around if all their surrounding space is filled by people at the bar across the street. It is already a problem today, this seems to make it worse. When I think of small town downtown, I think of small, quaint buildings. Yes, I want Horace to have a grocery store, a Target, Etc. but not downtown! I also like necessary	1/23/2025 6:56 PM

Horace Downtown Neighborhood Plan Community Survey #2, Future Options

services grouped together, such as Post Office, City Hall, Library, etc. It seems like all the plans still require people to drive to them unless you are young and can walk long distances. Also it seems like senior center parking is even more cramped and that is already a problem. How about a plan that addresses that issue?

69	Would like to see the elevator(s) rehabilitated in some way. Other buildings 2 stories or less. Do not like mini round-about in option 1. Similar round-about in Detroit Lakes is awful.	1/23/2025 4:37 PM
70	Downtown should be closer to the schools.	1/23/2025 4:17 PM
71	Do not add any more taxes. So whatever plan that is. We moved here in 2015 to be in a small town. And huge bonus of being only 5 minutes from west Fargo. Do not change this for everyone that was already here. Putting in a bunch of house etc does not support small town feel. Or a bunch on unnecessary downtown plans. We can go to Fargo for everything we need. Now we don't have a fast way out of town anymore. Stop building houses. Stop spending our money on things the original community members don't want. I can't wait to move from here. I have a sophomore and senior and as soon as they are finished we are out. Unless you stop spending our money.	1/23/2025 2:33 PM
72	Why are you requesting input when everything is already decided. It is obvious that the city and their buddies have already decided that the elevator is "Horace Downtown Neighborhood". Way to make everyone mad that lives here for many years and put their quiet homes into your new "Downtown".	1/23/2025 2:26 PM
73	Keep the major commercial development downtown - we do not need major commercial buildings on the land purchased by Enclave between 68th and 74th Ave along CR17. I envision a collection of office condos, similar to Oak Ridge in West Fargo - small office buildings that encourage Horace residents to keep their businesses in Horace - things like hair salons, a bakery, coffee shop, insurance/lawyer/financial offices, etc. would be a great thing to go into those buildings - plus they're mostly only open from 9am-5pm Monday through Friday, keeping traffic and interruptions to a minimum.	1/23/2025 10:53 AM
74	A combination of concept #1 and #3. I love the idea of repurposing the grain elevator. From concept #1, I love all the open space as community space, a place for the farmers market and the veterans memorial but there is a lot of space this is for outdoor use only which doesn't work for all seasons. From concept #3, I like the idea of more business/building opportunities, but would like to see more open outdoor space.	1/22/2025 6:11 PM
75	It would be really nice to have an Aldi in town and some type of indoor play space for children ages from 0-6 and a separate area for 7+. I think a few small necessity businesses would be great for the town however, would not be interested in something similar to Fargo. We enjoy the small town feel here in Horace and it would be disappointing to see it turned into Fargo. There's also going to be a point where there are just too many residential homes and the schools are going to be overfilled. I enjoy seeing the fields and grassy areas around town. Would REALLY hate to see those spaces turned into more homes.	1/22/2025 1:23 PM

Horace Public Survey #2; Open-Ended Responses

Unless noted otherwise, the following summary and analysis utilized Google Gemini (Artificial Intelligence) to assist with word processing and identification of themes. The Downtown Neighborhood Plan consultant team has reviewed the results and analysis to confirm accuracy. Details have been added and/or verified as applicable.

Question #1: Which routes(s) would you prioritize for bicycle/pedestrian connection and/or safety?

Focus on Safe and Connected Bike/Pedestrian Paths: 29 people commented

- **Priority on Youth Safety:**
 - Many comments emphasize the need for safe routes for children to walk or bike to school and local destinations, avoiding highways and busy roads.
 - Concerns about children from specific neighborhoods (Terra Gardens, Maple Lake Estates) lacking safe routes.
- **Connectivity:**
 - Desire for connections between different parts of Horace, including southern developments, the east and west sides of Sheyenne, and connections to West Fargo.
 - Interest in connecting to planned trails and roads from the diversion project.
 - Specific requests for paths along 76th Ave west of the roundabout and parallel to 76th ave west of the roundabout, and connecting County Road 17 West on 76th Ave to 81st St S.
- **Bike Path Design:**
 - Preference for bike paths separated from roads, due to concerns about narrow roads and safety
 - Requests for trail lighting, with a prioritization of more trails over lighted trails.
 - Consideration of winter use of bike paths for cross-country skiing, similar to Moorhead.
- **Safety Measures:**
 - Requests for increased pedestrian safety measures, such as crosswalk paintings with flashers and reduced speed limits (25 mph) in certain areas.
 - Keep people and bikes off county 17 (1 comment)

Community Survey #2 Open-Ended Response Summary

- Long-Term Planning:
 - Emphasis on fiscal responsibility and considering long-term maintenance costs.
 - Importance of planning for future growth and ensuring adequate walking/biking infrastructure in developing areas.

Other Community Concerns:

- School Zones:
 - Requests for school zones at Cub Creek development (79th and 63rd).
- Development Concerns:
 - Concerns about the impact of development on existing residents, including rising specials and changes to the town's character.
 - Questions about the location of a future grocery store and ensuring pedestrian/bike access.
- Rail Corridor:
 - Disappointment that the city did not acquire the cross-town connection path property when the railroad property was annexed.
 - Concerns that the city is attempting to bypass MFDA policy regarding the rail corridor.
- Amenities:
 - Suggestions for additional amenities, such as a "frolf" (disc golf) course and more trees.
 - Ensuring the tornado sirens are maintained.
- Route Prioritization:
 - Prioritizing North-South routes.
 - Prioritizing routes that allow access from the southern-most developments.
 - Prioritizing connections to West Fargo (to get people to ride bikes to Horace from West Fargo).
- Private Land:
 - Concerns about routes that run across private land (former railroad).

Community Survey #2 Open-Ended Response Summary

Question #22: Do you have any additional comments or suggestions for the Horace Downtown Neighborhood Plan? (75 answered 252 skipped – 29% answered)

- Preservation of Small-Town Feel:
 - A resounding sentiment against Horace becoming like Fargo or West Fargo, with residents valuing its current small town atmosphere and expressing concerns about overdevelopment, loss of rural character, and resisting the encroachment of a “city” feel.
 - Emphasis on preserving green spaces and trees, and avoiding dense development like apartments and "cookie-cutter" homes.
 - Concerns about overdevelopment, strip malls, and cheaply built homes that detract from the town's charm.
 - Desire to maintain open spaces and avoid excessive residential development.
 - Concern about overfilled schools due to residential growth.
- Grain Elevator Preservation:
 - Mixed opinions on the grain elevator, with some wanting it preserved and repurposed, and others wanting it removed.
 - Many strongly desire to preserve the grain elevator as an iconic symbol of the town, with suggestions for repurposing it into a museum, learning center, or farmers market.
 - Suggestion to include water tower property in public space
 - However, some individuals believe the elevator should be torn down due to cost, personal preference, or nuisance such as fire hazard or vermin.
 - Emphasis on how the grain elevators help to create a unique identity for Horace, distinct from Fargo and West Fargo.
- Downtown Development:
 - Concept preference
 - Preference for concept 1 and 3 in the downtown development plan.
 - Preference for concept 1 and 3, which incorporate the grain elevator, though some prefer concept 2 or 3.
 - Preference for concepts 2 and 3, with a focus on creating an engaging and distinct downtown area.
 - Retail / commercial

Community Survey #2 Open-Ended Response Summary

- Retail expansion is seen as key for success.
- Support for a farmers market to enhance the "small-town feel."
- Mixed opinions on the necessity of a traditional "downtown" area, with some preferring a focus on practical needs like a grocery store and fast-food restaurants.
- Requests for a grocery store either downtown or in a separate location (Aldi specifically mentioned) and other essential businesses.
- Emphasis on prioritizing local businesses over big chains.
- Concerns about the current strip mall style businesses.
- Desire for smaller office condos to encourage local businesses.
- Concerns about commercial development between 68th and 74th ave.
- What should downtown be?
 - Interest in a balanced downtown development that includes commercial spaces, community areas, and historical preservation.
 - A strong desire for a central park with walkability and flex-zone areas for commercial and housing coexistence.
 - Desire for unique place based building designs.
 - Concern about the location of the downtown area.
 - Concerns about overdevelopment and maintaining the town's character.
 - Concerns about displacing residents to create a downtown area.
 - Desire for a mix of building styles that reflect a small-town, rural feel.
 - Concerns about the impact of development on the fire station's accessibility.
 - Strong desire for one level buildings downtown.
 - Some commentors want the downtown area closer to the schools.
- Infrastructure and Traffic and practical needs:
 - Roads / traffic
 - Significant concerns about existing road conditions, particularly Sheyenne Dr. and Dakota Ave.
 - Strong opposition to roundabouts / mini-roundabouts, citing safety concerns for pedestrians and cyclists, especially near the school.
 - Traffic congestion issues, with multiple requests to widen roads or increase speed limits.

Community Survey #2 Open-Ended Response Summary

- Requests for better sidewalk maintenance.
 - Concerns about balancing traffic flow with pedestrian safety.
 - Emphasis on the safety of bike and walking paths for children traveling to school.
 - Requests to prioritize fixing existing infrastructure.
 -
- Community Services and Amenities
 - Desire for a town police station.
 - Interest in a public library.
 - Desire for an indoor play area for children.
 -
 - Requests for community assets like a pool or golf course.
 - The desire to keep the towns sirens and testing schedule.
 - Urgent need to address the "poop pond" stench.
 - Requests for adequate drainage and well-maintained public spaces.
 - Desire for necessary services like a Post office, city hall, and library to be grouped together.
 - Support for improvements to the senior center / Need for better senior center parking.
- Taxes
 - Concern about the city's spending of tax payer dollars.
 - Concerns about high taxes, increased taxes, and special assessments.
- Community and Safety:
 - Concerns about increased crime rates respondents believe are associated with more businesses and apartments.
 - Interest in creating community spaces that bring together different generations, particularly seniors and children.
- Planning and Coordination:
 - Criticism of coordinating with Metrocog.
 - Skepticism about the benefits of planned routes and concerns about the city's approach to acquiring the rail corridor.
 - Concerns about the city bypassing MFDA policy.

Community Survey #2 Open-Ended Response Summary

- Concerns about rapid growth and its impact on the town's character.
- Criticism of poorly built homes and the resulting high costs.
- Concerns about wasting tax dollars on unneeded projects.
- Skepticism about the city's transparency and concerns that decisions are already made.

Question #22: - Do you have any additional comments or suggestions for the Horace Downtown Neighborhood Plan? **(not AI summarized)**

- 5 of 75 said tear the grain elevators down (6%)
- 13 of 75 said keep the grain elevators (17%)
- 6 of 75 said “nobody wants a downtown” (8%)
- 7 of 75 mentioned wanting a grocery store (9%)
- 7 of 75 said no to roundabouts (9%)

Residents don't understand the value of businesses to help relieve special assessments:
»we.only.want.community.assets.not.more.businesses«

Residents have a different idea of what is rural:

»Horace.does.not.need.a»downtown«;Nobody.wants.expensive.downtown.stores;We.need.more.fast.food.restaurants.and.a.grocery.store«

Some residents feel excited and have a vision for the future of Horace:

fConsider.bringing.Seniors.and.Children.together.with.family.friendly.park.with.childrens.playground.equipment.as.bridge;Seniors.and.Children.are.a.great.natural.fit;Kids.have.lots.of.energy.Senior.have.lots.of.wisdom.and.patience;By.placing.a.park.between.the.grain.elevator.and.the.senior.center.it.could.be.what.ties.all.generations.together.and.invites.everyone.to.crossover;Working.parents.may.arrange.to.meet.a.loved.one.at.the..senior.center.following.a.day.of.activities?they.all.go.to.park.to.watch.kids.having.fun?then.everyone.walks.downtown.to.grab.a.bite.beer.or.root.beer.for.the.kids;Parents.with.older.kids.may.leave.them.at.the.park.with.some.sense.of.security.knowing.there.will.be.watchful.eyes.nearby.when.people.are.at.the.senior.center«

»I.think.we.should.do.our.best.to.keep.the.grain.elevator.as.it.has.become.an.icon.of.the.town;I.remember.growing.up.here.and.thinking.how.cool.it.was.as.a.kid;Maybe.it.can.be.repurposed.into.a.museum.or.learning.center?A.farmers.market.area.would.be.great.to.

Community Survey #2 Open-Ended Response Summary

attract.the.locals.and.it.would.fit.our.character.well;.I.also.think.we.need.to.prioritize.a.
central.park.with.lots.of.walkability?but.we.need.to.provide.flex_zone.areas.for.both.
commercial.and.housing.to.coexist;.This.is.a.great.option.for.aging.populations?but.also.
those.who.want.to.live.in.the.area.next.to.all.the.happenings?but.don't.want.to.own.a.large.
home;.Maybe.we.can.consider.co_op.condos.or.another.form.of.housing.attached.to.
commercial.storefronts; .

APPENDIX C: COMPLETE STREETS POLICY EXAMPLE

HORACE DOWNTOWN NEIGHBORHOOD PLAN





TOWN OF MADISON, CT

COMPLETE STREETS POLICY

Adopted by the Board of Selectmen, May 29, 2018

INTRODUCTION

The term "Complete Streets" describes a comprehensive, integrated transportation network with infrastructure and design that allows safe and convenient travel along and across streets for all users, including pedestrians, bicyclists, persons with disabilities, motorists, emergency responders, users and operators of public transportation, seniors, youth and families.

Complete Streets is a national movement that fundamentally changes how we view our communities and how we design, build, and use our streets in order to further the health, safety, welfare, economic vitality, and environmental well-being of our town. Complete Streets are supported by the Institute of Transportation Engineers, American Planning Association, US Conference of Mayors, and other planning and public health professionals.

The Town of Madison recognizes the numerous benefits associated with the adoption of a Complete Streets policy to enhance mobility, improve safety, expand transportation access/choice and related contributions toward building a healthy community. A Complete Streets approach to design contributes to an enhanced quality of life and economic vitality for residents and businesses. Benefits include reducing vehicle miles traveled and increasing transportation by walking, bicycling and public transportation, which can help address a wide variety of challenges, including pollution, climate change, traffic congestion, social isolation, obesity and physical inactivity.

Complete Streets are an important component of transportation design, and economic and community development, by helping create walkable and vibrant communities, which attract and retain businesses and residents, and allow for the option to safely walk or bicycle to school, beaches, work, parks, or other community destinations.

The residents of Madison have long expressed the need for walking and bicycling options that are safer, more accessible, connected, and convenient. In fact, the 2013 Plan of Conservation Development (pages 26 & 28) specifically mentions Complete Streets and suggests many of the elements contained in this policy. By adopting this Complete Streets policy, the Town of Madison will meet those needs through the development of a user friendly transportation network that accommodates, encourages, benefits and welcomes all users and transportation modes.

1. VISION AND INTENT

This Complete Streets policy shall direct the design, construction, use, and maintenance of town roadways, pathways, and sidewalks creating a comprehensive, integrated transportation network that is safe, accessible, comfortable, accommodating, and welcoming to all users. This shall include people of all ages, race, ethnicity, income, and physical ability, as well as all modes of transportation including pedestrians, people with mobility challenges, cyclists, motorists, commercial vehicles, and emergency vehicles.

A Complete Streets approach will encourage non-motorized modes of transportation and a Complete Streets culture that promotes an inter-connected community, healthy living, and a thriving town.

The overarching intent of this policy is to gradually transform Madison from a community that disproportionately encourages automobile travel to one that invests in transportation infrastructure equitably across all modes to the benefit of all members of the community and its visitors, while maintaining the charm and appeal of a small town.

Specific benefits include the following:

- Makes Madison roads safer and more inviting by increasing the capacity for various uses therefore decreasing the potential of vehicular, bicycle, and pedestrian-related accidents;
- Enables healthier lifestyle choices by providing an expanding variety of safer and convenient walking and bicycling options;
- Promotes a Complete Streets culture in Madison through education and events;
- Encourages private sector economic development that will benefit from, and contribute to, a more livable and vibrant community;
- Expands the network of safe walking and bicycling routes to schools, beaches, parks, or other community destinations;
- Connects with other town/citizen's action groups in Madison that have goals related to Complete Streets;
- Helps address a wide variety of challenges, including pollution, climate change, traffic congestion, social isolation, obesity and physical inactivity.

The following elements shall guide the planning and implementation of all Complete Street improvements.

2. DIVERSE USERS

All users and all transportation modes shall benefit from Complete Streets improvements.

The Town of Madison benefits from having two modes of public transit located in proximity to Madison Center - bus stops serving both 9 Town Transit and CT Transit, and the Shoreline East Train Station. These public transit options are relied upon by portions of Madison's population that may not have access to private transportation options. This policy shall endeavor to facilitate utilization of public transit options by encouraging Complete Streets improvements that enhance access to these facilities, such as sidewalks and bike lanes, as well as improve the comfort and convenience of the facilities, such as shelters.

In addition to public transit users, Madison's elderly, low-income, and disabled populations can benefit from Complete Streets improvements by enabling access to community destinations. Some examples of vulnerable users that could benefit from Complete Streets improvements include:

- a) Residents of Concord Meadows, a low income housing development off Woodland Road, would benefit from improved access to Madison Center.
- b) Vista Life Innovations, a local service organization for individuals with disabilities, requires safe walking options for its clients. Currently, safe access across Bradley Road is a concern for the organization.
- c) Visitors to the Madison Senior Center would benefit from improved access to Madison Center and the Town Green.

3. COMMITMENT IN ALL PROJECTS AND PHASES

The Town of Madison commits to applying Complete Streets principles at the outset of all transportation and roadway projects. Each project shall be approached as an opportunity to improve the safety and accessibility of the transportation network for all users.

Preventive maintenance activities, such as mowing, cleaning, sweeping, spot repair, concrete joint repair, pothole filling, water, sewer and drainage or other utility installation or repairs taking place without any other tangible improvements, do not mandate Complete Streets Improvements. To the maximum extent possible, however, provisions for safe access shall be made for all modes of transportation during maintenance activities.

Periodic maintenance activities, such as re-striping, re-surfacing and re-milling, which allow for no or low cost CS improvements, e.g. "road diets" that narrow the vehicle travel lanes and provide space for the addition, or widening, of bike and pedestrian lanes, shall be considered. This means that small and routine tasks, not just the larger construction and reconstruction projects, provide opportunities to implement Complete Streets. Many small, low-cost improvements will, when thoughtfully implemented over time, create a much friendlier and safer environment for everyone.

Transportation and roadway improvement projects, such as road widening and intersection improvements, shall be considered most thoroughly for Complete Streets improvements and are subject to the exception criteria outlined in Element 4.

4. CLEAR, ACCOUNTABLE EXCEPTIONS

The Town of Madison commits to applying Complete Streets principles at the outset of all transportation and roadway improvement projects. Each project shall be approached as an opportunity to improve the safety and accessibility of the street/right of way for all users.

However, exceptions shall be made if any of the following criteria render Complete Streets improvements unworkable:

- a) Where specific users are prohibited by law from using the ROW (e.g. pedestrians and bicyclists within a limited access highway). However, exclusion of certain users on particular ROW's shall not exempt projects from accommodating other permitted users;
- b) Cost is disproportionate to the current need or projected future need for Complete Streets improvements, or unusual circumstances, such as where natural features (e.g. steep hills, ledge, shorelines) make it very costly or impossible to accommodate all modes, or funding is not available;
- c) There is an absence of current and future need (e.g. a rural road that carries low Average Daily Traffic (ADT) and is remote from neighborhoods, schools, or points of interest);

Requested exceptions shall be evaluated initially by the Director of Public Works in consultation with a member of the Bicycle & Pedestrian Advisory Committee (BPAC), who shall be elected by the committee to serve as the "Complete Streets Liaison", and then presented at a regularly scheduled Board of Selectmen meeting for action. A determination of exception will conform to one or more of the three allowable exceptions listed above.

5. JURISDICTION

This Complete Streets Policy shall apply to all Town owned streets and land within public ROWs. All Complete Streets improvements must be coordinated between all relevant Town of Madison departments such as Engineering, Public Works, Planning, Police, Emergency Management, Public Health, and Senior Services.

All new private development projects, which propose improvements within the public ROW, shall comply with this Complete Streets policy. Approval of necessary permits is contingent upon meeting the Complete Streets requirements. Private development projects proposing privately owned right-of-ways shall be reviewed for consistency with the Complete Street policy as part of the approval process.

Owners of private streets and ways shall also be encouraged to adhere to this policy.

The State of Connecticut controls several principal transportation corridors that traverse Madison. The Town shall work cooperatively with the Connecticut Department of Transportation to plan and implement Complete Streets improvements within these ROWs.

Public Act PA 09-154 and the CTDOT Complete Streets Policy shall be applied to all planning, design, construction and major maintenance within State controlled ROWs.

http://www.ct.gov/dot/lib/dot/plng_plans/bikepedplan/cs-exo31-signed.pdf

<https://www.cga.ct.gov/2009/ACT/Pa/pdf/2009PA-00154-R00SB-00735-PA.pdf>

Utility companies operate within Town and State ROWs. Their planning, construction, and major maintenance can create both opportunities and barriers to Complete Streets improvements. The Town shall coordinate with the utilities to ensure that utility projects and Complete Streets improvements are coordinated wherever possible.

The Town shall also coordinate Complete Streets planning and construction with Shoreline Area Transit, the SCRCOG (South Central Regional Council of Governments), and adjacent municipalities to facilitate effective application of resources.

6. DESIGN

All Complete Streets improvements within public ROWs shall conform to the following standards. Of the following list, AASHTO and MUTCD are considered the definitive design guides for changes within the State ROW. Because Complete Streets design is an evolving field, the latest edition of these standards shall be referenced for design guidance:

- American Association of State Highway and Transportation Officials (AASHTO)
 - A Policy on Geometric Design of Highways and Streets
 - Guide for the Development of Bicycle Facilities
 - Guide for the Planning, Design and Operations of Pedestrian Facilities
- American Planning Association (APA)
 - Complete Streets: Best Policy and Implementation Practices
 - U.S. Traffic Calming Manual
- Federal Highway Administration (FHWA)
 - Manual of Uniform Traffic Control Devices (MUTCD)
 - PEDSAFE: Pedestrian Safety Guide and Countermeasures Selection System
 - Incorporating On-Road Bicycle Networks Into Resurfacing Projects
- Institute of Transportation Engineers (ITE)
 - Designing Walkable Urban Thoroughfares: A Context Sensitive Approach
- National Association of City Transportation Officials (NACTO)
 - Urban Bikeway Design Guide
 - Urban Street Design Guide
- U.S. Access Board
 - Accessible Public Rights-of-Way: Planning and Designing for Alterations

This section of the Complete Streets Policy shall be updated a minimum of every three years by the Engineering Department to ensure that the listing of design standards is current.

7. LAND USE AND CONTEXT SENSITIVITY

Land use context and flexibility shall be considered relative to potential Complete Streets improvements. The overall goal of this approach is to preserve and enhance scenic, aesthetic, historical, and environmental resources while improving or maintaining safety, mobility, and infrastructure conditions;

It is the intent of this Policy that the Town of Madison's Director of Planning & Economic Development and the Planning & Zoning Commission will endeavor to incorporate Complete Streets policies into the zoning regulations, subdivision regulations, the Plan of Conservation & Development, and all other relevant planning and regulatory documents to help support the community's Complete Streets vision. Additionally, the Planning & Zoning Commission is encouraged to review all private development proposals with the goal of helping facilitate the community's Complete Streets vision.

All Town projects that have land use considerations, such as school location decisions, property acquisition, park planning, etc., shall consider how such actions may impact the community's Complete Streets vision. Where feasible, biking and walking to municipally-owned facilities shall be encouraged and facilitated.

8. POLICY PERFORMANCE MEASUREMENT

Measurement of Complete Streets improvements represents a continuum that will require periodic and sustained evaluation to measure progress and effectiveness. To facilitate that regular evaluation, the Complete Streets Liaison shall work with staff from the Department of Public Works to complete a form containing the information below for the Bicycle & Pedestrian Advisory Committee (BPAC) each calendar year by the first of February. The BPAC will then report to the Board of Selectmen on the progress and effectiveness of the Complete Streets policy during the previous calendar year. The measurement of all Complete Streets improvements for the previous calendar year are to include the following:

- a) Funding: Total dollar amount spent on Complete Street Improvements
 - Town funds
 - Grant funds
 - Other funds
- b) Sidewalks/Pedestrian/Transit Improvements:
 - Linear feet of sidewalks and other pedestrian accommodations built or improved
 - Within ½ mile of schools
 - Outside ½
 - Number and description of crosswalks installed or improved
 - Number and description of Americans with Disabilities Act (ADA) accommodations installed or improved
 - Number and description of public or private transit accessibility improvements installed or improved by type and number
- c) Bicycling Improvements:
 - Linear feet of bicycle lanes, routes, or trails built by width and type
 - Number and description of bicycle parking facilities installed
- d) Traffic Calming:
 - Number and description of traffic calming measures implemented
 - Number of new traffic control signs / signals installed that assist with the Town's Complete Streets policies
 -
- e) Maintenance Activities:
 - Description of Maintenance Activities of existing Complete Streets facilities
- f) User and Crash Data:
 - Bicycle and pedestrian traffic counts
 - Motor vehicle, bicycle and pedestrian accident data
- g) Exceptions:
 - Number of Exceptions requested and approved, including dates and committee member(s) involved.

9. PROJECT SELECTION CRITERIA

When there is conflicting need among users and modes, the following prioritization will apply:

- a) Safety is the highest priority, followed by mobility;
- b) Among modes, pedestrian and bicycle needs shall receive priority, followed by the next most vulnerable user in each case;
- c) Strive for balance among all modes involved in each case. It is recognized that all modes cannot receive state of the art accommodation within every right-of-way (ROW – the publicly owned transportation corridor), but the overall goal is that all users of varying ability can safely and conveniently use the transportation network.

Priority or special consideration shall be given to locations and, or, improvements identified and recommended by the Bicycle & Pedestrian Advisory Committee (BPAC).

10. COMPLETE STREETS POLICY IMPLEMENTATION

The implementation of Complete Streets will require cooperation and collaboration between many stakeholders on a regular basis. As such, the Town will take the following steps to facilitate the process:

- a) The BPAC, or its successor, shall oversee implementation of the Complete Streets policy.
- b) The Director of Planning & Economic Development and the Planning & Zoning Commission shall review and propose revisions to all appropriate land use ordinances, policies and regulations to support the implementation of Complete Streets.
- c) The Public Works and Engineering Departments shall establish necessary procedures to ensure the application of Complete Streets principles at the earliest design stage.
- d) The Town shall encourage staff professional development in the area of Complete Streets through attendance at seminars, conferences and workshops.
- e) The Town shall actively promote public information and education and solicit feedback about Complete Streets to the community using outreach strategies that make use of natural gathering spaces, such as schools, parks, beaches, the Senior Center, community meetings, Town Green events, and the Town web site.

END

APPENDIX D: PROJECT SHEETS

**HORACE DOWNTOWN
NEIGHBORHOOD PLAN**



R.4 CR 17/Main Street.....	3
R.5 Drain 27.....	5
R.1 Park Drive.....	7
R.2 Wall Avenue.....	9
R.7 Front Street.....	11
R.6 Southwest Trail Alignment.....	13
R.3 CR 17 South.....	15
R.9 76 th Avenue S.....	17
R.8 64 th Avenue S.....	19
I.1 Main St. & Wall Ave.....	21
I.2 Main St. & Ironwood Dr.....	23
I.3 CR 17 & Lakeview Dr.....	25
I.4 Main St. & Center Ave.....	26
I.5 Main St. & Park Dr.....	28
I.7 CR 17 & 100 th Ave. S. (CR 14).....	30
I.9 Wall Ave. & Front St.....	32
I.10 Wall Ave. & Nelson Dr./Future Lakeview..	34
I.6 CR 17 & Liberty Ln.....	36
I.8 Wall Ave. & 63 rd St. S.....	38
I.11 CR 17 & Chestnut Dr./81 st Ave. S.....	40
I.12 63 rd St. & 79 th Ave. S.....	41

R.4 CR 17/Main St.



64th Ave S

..... Existing Gaps

8

Horace Lions Park

Maple Grove Park

76th Ave S

9

76th Ave S

CR17

SPEED LIMIT 45
SPEED LIMIT 35
SPEED LIMIT 25

CR 17 & 76th Ave S.
Existing crossing infrastructure may be sufficient

CR 17 & 68th Ave S.

East-West
uncontrolled

North-South
controlled



OR



CR 17 & 73rd Ave S.
Evaluate as development & street network buildout

CR 17 & Chestnut Dr/81st Ave S.
See intersection priorities (I.11)

Main Street & Ironwood Dr
See intersection priorities (I.2)

Main Street & Center Ave
See intersection priorities (I.1)

DETAILS

Route ID: R.4

Priority Rank: #1

Alignment: CR 17/Main Street

Functional Classification: Minor Arterial

Termini: From Wall Avenue to 52nd Avenue S.

Route Strategy: Construct 10' Shared Use Path, both sides of street.

Crossing Strategy: Varies by location, speed limit, traffic volume, intersecting roadway functional classification, controlled or uncontrolled (traffic) configuration, and adjacent land uses or destinations.

CR17

6

3

Meadowlark Park

Independence Park

Uncle John's Giant Champion Cottonwood

Arrowwood Park

Freed Park

Big Erv's

Terra Gardens Park

City Hall

100th Ave S

Park Dr

ROUTE	DESCRIPTION	LENGTH (MILES)	COST LOW	COST HIGH*
R.4	Shared use path, both sides of Main St./CR 17 from Wall Ave. to 52nd Ave. S.			
R.4.a	Upgrade multimodal infrastructure at downtown intersections.	See R.4.lines below.		
R.4.a.i	Main St. & Ironwood Dr. intersection (I.2) strategies.	intersection	\$365,000	\$1,085,000
R.4.a.ii	Main St. & Wall Ave. intersection (I.1) strategies.	intersection	\$930,000	\$930,000
R.4.a.iii	CR 17 & Chestnut Dr. or 81 st Ave. S. intersection (I.11) strategies.	intersection	\$355,000	\$575,000
R.4.a.iv	Install HAWK signal or median refuge island with RRFBs at CR 17 & 81 st Ave. S.	intersection	\$90,000	\$280,000
R.4.a.v	Construct grade separation or install HAWK signal at CR 17 & 68 th Ave. S. intersection.	intersection	\$280,000	\$515,000
R.4.b	Construct shared use path to fill gaps in existing network. Prioritize locations closest to downtown.	See R.5.b lines below.		
R.4.b.i	East side of Main St. from 81 st Ave S. to 76 th Ave. S.	0.34	\$680,000	\$680,000
R.4.b.ii	East side of Main St. from Wall Ave. to Ironwood Dr. (consider timing with I.1 Main St. & Wall Ave. intersection improvements).	0.16	\$320,000	\$320,000
R.4.b.iii	East side of CR 17 from 73 rd Ave. S. to 52 nd Ave. S.	1.74	\$3,480,000	\$3,480,000
R.4.b.iv	West Side of CR 17 from 76 th Ave. S. to 52 nd Ave S.	2	\$4,000,000	\$4,000,000
TOTAL ROUTE (MILES)		4.24	\$8,850,000	\$9,275,000
TOTAL INTERSECTIONS		5	\$2,020,000	\$3,385,000
SUM R.4			\$10,870,000	\$12,660,000
*if multiple options considered				

R.5 Drain 27

..... Existing Gaps



64th Ave S

8

Horace Lions Park

64th Ave S.

North-South
uncontrolled



76th Ave S.

Maple Grove Park

76th Ave S

9

76th Ave S

Evaluate 1/2-mile crossing opportunities of Drain 27 as development & street network buildout

DETAILS

Route ID: R.5

Priority Rank: #2

Alignment: Drain 27

Functional Classification: N/A

Termini: From 100th Avenue S. to 52nd Avenue S.

Route Strategy: Construct 10' Shared Use Path, at least one side of Drain 27. West side is closer to existing development & may be more accessible to Horace residents.

Crossing Strategy: North-south crossings of streets should consider grade separation of section-line, future arterial or high-speed roadways, mid-block crossings at collectors or local roads should prioritize bicycle & pedestrian mobility and safety. 1/2-mile east-west crossings of Drain 27 should be considered, whether along a street or bicycle & pedestrian specific infrastructure.

CR17

The Headquarters

High School

Taco Revolution

Casey's

Elementary School

4

Wall Avenue

North-South
uncontrolled



5

2

Terra Gardens Park

Park Drive

Evaluate as development & street network buildout

100th Ave S. (CR 14)

Evaluate connections to possible recreation or interpretive opportunities associated with the FM Diversion, as project progresses.

6

CR17

ROUTE	DESCRIPTION	LENGTH (MILES)	COST LOW	COST HIGH*
R.5	Off-street trail, at least one side of Drain 27 from south of 100th Ave. S. to City of Fargo trail north of 64th Ave. S.			
R.5.a	Construct off-street trail to fill gaps in existing network. Prioritize locations closest to downtown and existing development.	See R.5.a lines below		
R.5.a.i	East side of Drain 27 from south of 100 th Ave. S. to Wall Ave. S.	1.3	\$2,600,000	
R.5.a.ii	East side of Drain 27 from south of 76 th Ave. S. to City of Fargo trail north of 64 th Ave. S.	1.4	\$2,800,000	
R.5.b	Upgrade multimodal infrastructure at existing intersections and plan for future network crossing of Drain 27.	See R.5.b lines below		
R.5.b.i	Grade separation at Wall Ave. S.	intersection	\$500,000	
R.5.b.ii	Grade separation at 76 th Ave S.	intersection	\$500,000	
R.5.b.iii	Grade Separation at 64 th Ave. S.	intersection	\$500,000	
R.5.b.iv	Grade separation at 100 th Ave. S.	intersection	\$500,000	
TOTAL ROUTE (MILES)		2.7	\$5,400,000	
TOTAL INTERSECTIONS		4	\$2,000,000	
SUM R.5			\$7,400,000	
*if multiple options considered				

R.1 Park Drive



- Existing Gaps (yield street)
- Existing Gaps (SUP)

DETAILS

Route ID: R.1
Priority Rank: #3
Alignment: Park Drive
Functional Classification: Local
Termini: From Boxelder Circle to Drain 27
Route Strategy: Restripe as yield street on existing cross section, include signage. Opportunity to serve as city-wide bicycle & pedestrian connection. Shared use path should be considered through Independence Park and potential future extension of Park Drive to the east.
Crossing Strategy: East-west crossings of streets should consider conventional crosswalks or curb extensions to increase visibility. Sheyenne River, Main Street, and potential future Drain 27 crossings will require more significant crossing infrastructure.
Other: There is a half-block gap in Park Drive right-of-way (ROW) between Main Street and Nelson Drive. Full implementation of route #1 would require ROW acquisitions in two locations shown.

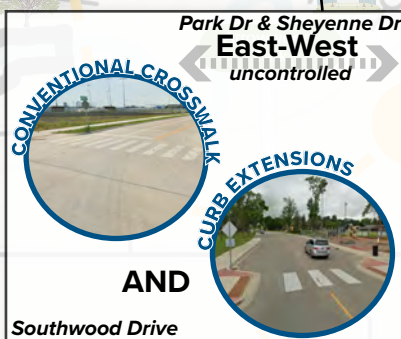


ROW acquisition west of Sheyenne River

ROW acquisition east of Main St.

Drain 27
Evaluate as development & street network buildout

Main Street & Park Drive
See intersection priorities (ID #5)



ROUTE	DESCRIPTION	LENGTH (MILES)	COST LOW	COST HIGH*
R.1	Off-street trail and yield street on Park Dr. alignment from Boxelder Cir. To Drain 27.			
R.1.a	Acquire right-of-way (ROW) for an off-street trail.	See R.1.a lines below		
R.1.a.i	Both sides of Sheyenne River, west of Independence Park.		\$250,000	\$500,000
R.1.a.ii	Park Dr. alignment from Main St. to Nelson Dr.		\$50,000	\$100,000
R.1.b	Install bike and ped bridge over Sheyenne River just west of Independence Park.		\$3,000,000	\$3,000,000
R.1.c	Construct off-street trail.	See R.1.c lines below		
R.1.c.i	From Boxelder Cir. to Sheyenne Dr.	0.4	\$800,000	\$800,000
R.1.c.ii	From Wild Goose Ln. to Drain 27	0.15	\$300,000	\$300,000
R.1.d	Install yield street striping and signage.			
R.1.d.i	From Sheyenne Dr. to Main St.	0.25	\$5,000	\$5,000
R.1.d.ii	From Nelson Dr. to Wild Goose Ln.	0.6	\$12,000	\$12,000
R.1.e	Upgrade multimodal infrastructure at existing intersections.	See R.1.e lines below		
R.1.e.i	Install conventional crosswalk with or without curb extensions at Park Dr. intersections with Southwood Dr. and Sheyenne Dr.	intersection	\$30,000	\$70,000
R.1.e.ii	Main St. & Park Dr. intersection (I.5) strategies.	intersection	\$115,000	\$115,000
TOTAL ROUTE MILES		1.4	\$4,417,000	\$4,717,000
TOTAL INTERSECTIONS		2	\$145,000	\$185,000
SUM R.1			\$4,562,000	\$4,902,000
*if multiple options considered				

R.2 Wall Avenue



DETAILS

..... Existing Gaps

Route ID: R.2
Priority Rank: #4
Alignment: Wall Avenue
Functional Classification: Collector/Local
Termini: From Main Street to 57th Street S.
Route Strategy: Shared use path (south side). May consider both sides however, as a collector roadway, one side may be considered with sidewalk on the other.
Crossing Strategy: North-south crossings of Wall Avenue should consider curb extensions, pedestrian refuge island (depending upon lane configuration), and raised crosswalk to keep vehicle speeds safe for pedestrian crossing activity along this route.

Wall Avenue & 62nd Street S.

East-West
uncontrolled

North-South
uncontrolled



OR

Wall Avenue & 63rd Street

See intersection priorities (ID #8)

Wall Avenue & 4th Street

Evaluate mid-block crossing as development & street network buildout

Main Street & Wall Avenue

See intersection priorities (ID #1)

Freed Park

North-South
uncontrolled



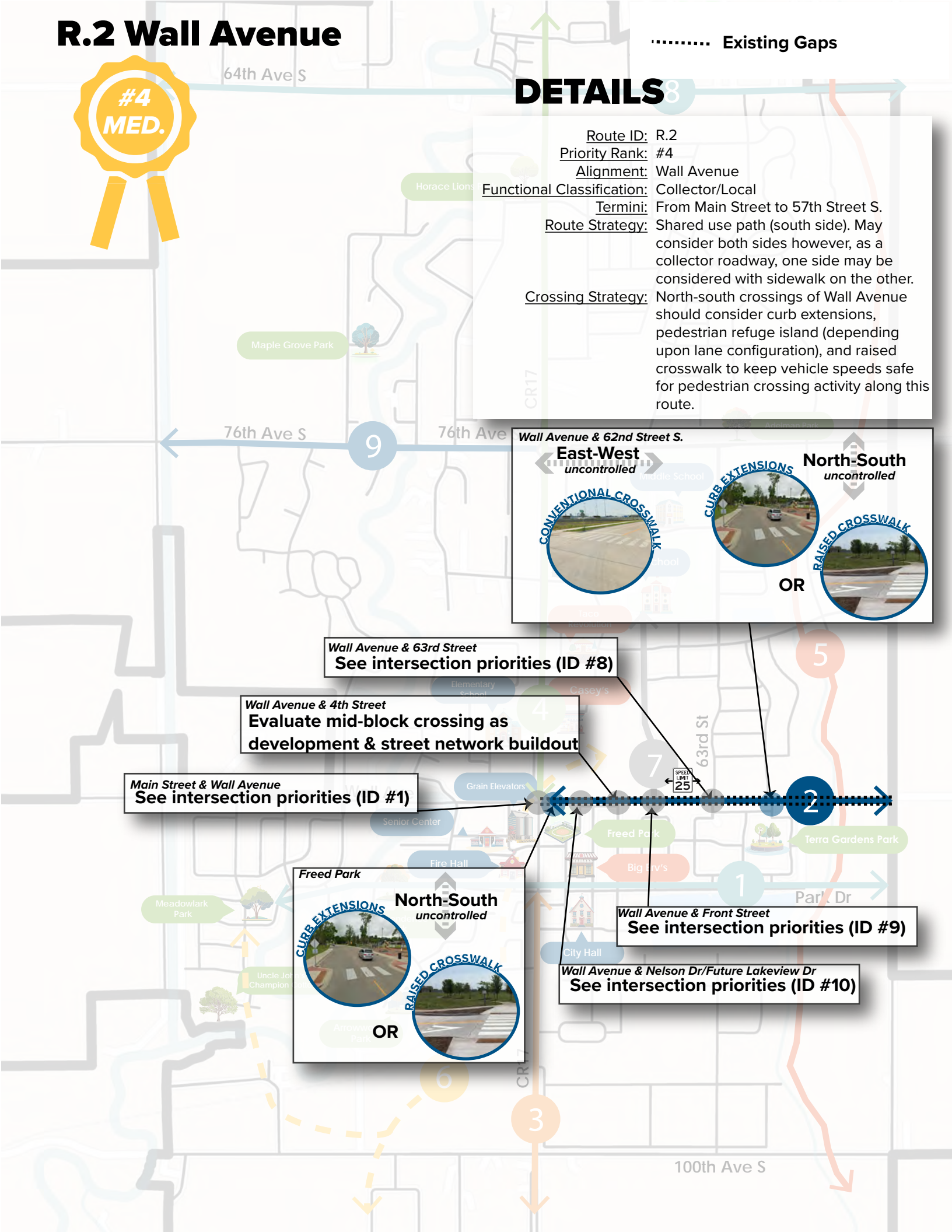
OR

Wall Avenue & Front Street

See intersection priorities (ID #9)

Wall Avenue & Nelson Dr/Future Lakeview Dr

See intersection priorities (ID #10)



ROUTE	DESCRIPTION	LENGTH (MILES)	COST LOW	COST HIGH*
R.2	Sidewalk (north side) and shared use path (south side) on Wall Ave. from Main St. to 57th St. S.			
R.2.a	Construct sidewalk/wide sidewalk (north side) and shared use path (south side). Prioritize areas nearest to downtown	1	\$3,000,000	\$3,000,000
R.2.b	Upgrade multimodal infrastructure at existing intersections. Prioritize locations closest to downtown.	See R.2.b lines below		
R.2.b.i	Main St. & Wall Ave. intersection (I.1) strategies.	intersection	\$930,000	\$930,000
R.2.b.ii	Install mid-block crossing to Freed Park with RRFBs and curb extensions or raised crosswalk.	intersection	\$60,000	\$90,000
R.2.b.iii	Wall Ave. & Front St. intersection (I.9) strategies.	intersection	\$145,000	\$195,000
R.2.b.iv	Wall Ave. & 63 rd St. intersection (I.8) strategies.	intersection	\$100,000	\$180,000
R.2.b.v	Wall Ave. & Nelson Dr./Future Lakeview Dr. intersection (I.10) strategies.	intersection	\$145,000	\$145,000
R.2.b.vi	Install curb extensions and/or raised crosswalk at Wall Ave. & 62 nd St. S. intersection.	intersection	\$20,000	\$50,000
TOTAL ROUTE (MILES)		1	\$3,000,000	\$3,000,000
TOTAL INTERSECTIONS		6	\$1,400,000	\$1,590,000
SUM R.2			\$4,400,000	\$4,590,000
*if multiple options considered				

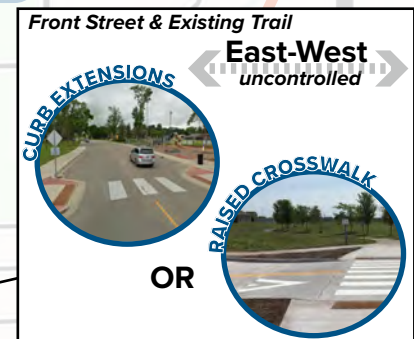
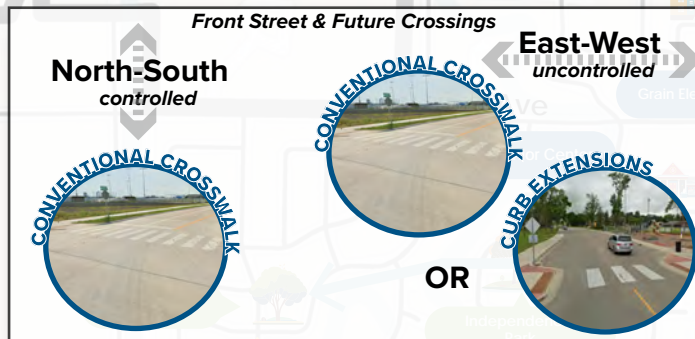
R.7 Front Street



- Existing Gaps (sidewalk)
- Existing Gaps (SUP)

DETAILS

Route ID: R.7
Priority Rank: #5
Alignment: Front Street
Functional Classification: Local
Termini: From Wall Avenue to 82nd Avenue S.
Route Strategy: Shared use path (west side), with sidewalk on the other (east side).
Crossing Strategy: North-south crossings of Wall Avenue should consider conventional crosswalks and east-west crossings conventional crosswalks and/or curb extensions. Curb extensions are encouraged at high-pedestrian crossing locations (such as shared use path, etc.).



Wall Avenue & Front Street
See intersection priorities (ID #9)

ROUTE	DESCRIPTION	LENGTH (MILES)	COST LOW	COST HIGH*
R.7	Sidewalk (east side) and shared use path (west side) on Front St. from Wall Ave. to 82nd Ave S.			
R.7.a	Construct shared use path to fill gaps in existing network.	See R.7.a lines below.		
R.7.a.i	West side of Front St. from Wall Ave. to 83 rd Ave. S.	0.37	\$740,000	\$740,000
R.7.b	Construct a sidewalk to fill gaps in existing network.	See R.7.b lines below.		
R.7.b.i	East side of Front St. from Wall Ave. to St. Anne Ave.	0.37	\$370,000	\$370,000
R.7.c	Upgrade multimodal infrastructure at existing intersections and plan for future crossing of the street.	See R.7.c lines below.		
R.7.c.i	Install mid-block crossing for existing off-street trail north of St. Anne Ave. with RRFBS and curb extensions or raised crosswalk.	intersection	\$60,000	\$90,000
R.7.c.ii	Wall Ave. & Front St. intersection (I.9) strategies.	intersection	\$145,000	\$195,000
TOTAL ROUTE (MILES)		0.74	\$1,110,000	\$1,110,000
TOTAL INTERSECTIONS		2	\$205,000	\$285,000
SUM R.7			\$1,315,000	\$1,395,000
*if multiple options considered				

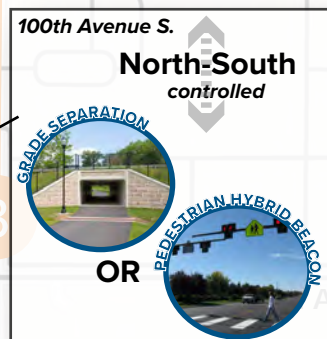
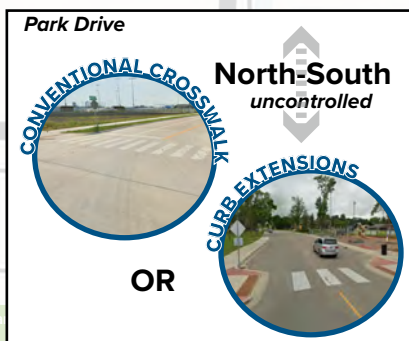
R.6 Southwest Trail Alignment

Conceptual Path Alignment



DETAILS

Route ID: R.6
Priority Rank: #6
Alignment: Off section-line roads
Functional Classification: Local
Termini: From south of 100th Avenue S. to north of Wall Avenue
Route Strategy: Construct 10' shared use path and/or off-street trail connecting developing areas south/southwest of Wall Avenue and Main Street intersection. Through discussions with the public, many people wanted to see routes to/from this area off of the section-line roads (e.g. CR 17 & 100th Avenue S.) There is a small section of the railroad spur which people thought would make a great shared use path between Park Drive and Wall Avenue. Portions of this route were identified through public engagement.
Crossing Strategy: Varies by type of road and natural features. Prioritization of pedestrian safety should be considered for 100th Avenue S. crossing. Potential future bike & pedestrian crossing of Sheyenne River may be challenged by Sheyenne Diversion inlet structure.



Main Street & Wall Avenue
See intersection priorities (ID #1)

ROUTE	DESCRIPTION	LENGTH (MILES)	COST LOW	COST HIGH*
R.6	Off-street trail/shared use path from south of 100th Ave. S. to downtown/Wall Ave.			
R.6.a	Plan to construct an Off-Street Trail and/or Shared Use Path from residential neighborhoods south and west of 100th Avenue S an CR 17.	1.5	\$3,000,000	\$3,000,000
R.6.b	Plan to align away from major section-line roads such as 100th Avenue S. and CR 17.	See R.6 lines below.		
R.6.c	Plan for two primary connections; (1) to Meadowlark Park and (2) to downtown/Wall Avenue.	See R.6.a above for total length.		
R.6.d	Acquire right-of-way (ROW) through existing land development procedures and acquisition for route away from 100 th Ave. S. and CR 17 alignment.		\$0	\$500,000
R.6.e	Plan for Bike and Pedestrian Bridge at Sheyenne River for connection to Meadowlark Park/Lost River Neighborhood.	intersection	\$3,000,000	\$3,000,000
R.6.f	Plan for and construct Grade Separations at Arterial streets.	intersection	\$500,000	\$500,000
R.6.g	Plan to construct Curb Extensions at Collector streets including RRFBs, as applicable.	intersection	\$45,000	\$45,000
R.6.h	Plan to construct Conventional Crosswalks at Local streets.	intersection	\$45,000	\$90,000
TOTAL ROUTE (MILES)		1.5	\$3,000,000	\$3,500,000
TOTAL INTERSECTIONS		4	\$3,590,000	\$3,635,000
SUM R.6			\$6,590,000	\$7,135,000
*if multiple options considered				

R.3 CR 17 South



64th Ave S

..... Existing Gaps

DETAILS

Route ID: R.3
Priority Rank: #7
Alignment: CR 17
Functional Classification: Minor Arterial & Collector (south of 100th Avenue S.)
Termini: From south of 100th Avenue S. to north of Wall Avenue
Route Strategy: Construct 10' Shared Use Path, both sides of street. Shared use path and/or off-street trail connecting developing areas south/southwest of Wall Avenue and Main Street intersection. Through discussions with the public, many people wanted to see routes to/from this area off of the section-line roads (e.g. CR 17 & 100th Avenue S.) There is a small section of the railroad spur which people thought would make a great shared use path between Park Drive and Wall Avenue.
Crossing Strategy: Varies by street classification, speed, etc. Prioritization of pedestrian safety should be considered for 100th Avenue S. crossing and for east-west crossings of CR 17.

Main Street & Wall Avenue

East-West
uncontrolled

North-South
controlled



OR



Main Street & Park Drive

See intersection priorities (ID #5)

CR 17 & Liberty Lane

See intersection priorities (ID #6)

CR 17 & 100th Avenue S. (CR 14)

See intersection priorities (ID #7)

100th Ave S

ROUTE	DESCRIPTION	LENGTH (MILES)	COST LOW	COST HIGH*
R.3	Shared use path, both sides of Main St./CR 17 from Park Dr. to south of 100th Ave. S.			
R.3.a	Upgrade multimodal infrastructure at existing intersections. Prioritize locations closest to downtown.	See R.3.a lines below.		
R.3.a.i	Main St. & Park Dr. intersection (l.5) strategies.	intersection	\$115,000	\$115,000
R.3.a.ii	CR 17 & Liberty Ln. intersection (l.6) strategies.	intersection	\$150,000	\$150,000
R.3.a.iii	Construct a grade separation or install a HAWK signal at CR 17 & Sparks Addition street, south of Liberty Ln.	intersection	\$265,000	\$500,000
R.3.a.iv	CR 17 & 100 th Ave. S. intersection (l.7) strategies.	intersection	\$3,125,000	\$3,125,000
R.3.b	Construct a shared use path to fill gaps in existing network.	See R.3.b lines below.		
R.3.b.i	Both sides of CR 17 from Park Dr. to south of 100 th Ave. S.	1	\$4,000,000	\$4,000,000
TOTAL ROUTE (MILES)		1	\$4,000,000	\$765,000
TOTAL INTERSECTIONS		4	\$3,655,000	\$3,890,000
SUM R.3			\$7,655,000	\$4,655,000
*if multiple options considered				

R.9 76th Avenue S.



CROSSING STRATEGY

76th Avenue S. & 78th St S + Brink/Sunnyside + 66th St S. + Cub Creek Way

North-South
uncontrolled

East-West
controlled



OR



OR



ROUTE STRATEGY



DETAILS

Route ID: R.9

Priority Rank: #9

Alignment: 76th Avenue S.

Functional Classification: Collector

Termini: Sheyenne Diversion Channel to City Line/57th Street S.

Route Strategy: Construct 10' Shared Use Path, on at least one side (south) of street however, as a potential future arterial road, may encourage both sides of street long-term. This route was identified through public engagement (west of CR 17).

Crossing Strategy: Varies by street classification, speed, traffic volume, etc. Specific crossing locations and strategy may not be understood until development and street network get laid out.

Existing Gaps

100th Ave S

ROUTE	DESCRIPTION	LENGTH (MILES)	COST LOW	COST HIGH*
R.9	Shared use path, at least one side (south side) of 76th Ave. S. from Sheyenne Diversion Channel to 57th St. S.			
R.9.a	Construct a shared use path to fill gaps in existing network.	See R.9.a lines below.		
R.9.a.i	South side of 76 th Ave. S. from Sheyenne Diversion Channel to CR 17	1	\$2,000,000	\$2,000,000
R.9.a.ii	North side of 76 th Ave. S. from CR 17 to 57 th St. S.	2.1	\$4,200,000	\$4,200,000
R.9.a.iii	South side of 76 th Ave. S. from Cub Creek Pkwy. To 57 th St. S.	0.4	\$800,000	\$800,000
R.9.b	Upgrade multimodal infrastructure at existing and future intersections. Prioritize locations strategic to downtown connections and connections to Horace Middle and High Schools.	See R.9.b lines below.		
R.9.b.i	Install a HAWK signal or construct median refuge island with RRFBs at Brink Dr./Sunnyside Ct. & 76th Ave. S. intersection	intersection	\$90,000	\$280,000
R.9.b.ii	Install a HAWK signal or construct median refuge island with RRFBs at Cub Creek Pkwy. & Future 66 th St S. intersection (future grade separation recommended at Drain 27, just to the east).	intersection	\$90,000	\$280,000
R.9.b.iii	Install a HAWK signal or median refuge island with RRFBs at 78 th St. S. & 76 th Ave. S. intersection (timed with development).	intersection	\$90,000	\$280,000
TOTAL ROUTE (MILES)		3.5	\$7,000,000	\$7,000,000
TOTAL INTERSECTIONS		3	\$270,000	\$840,000
SUM R.9			\$7,270,000	\$7,840,000
*if multiple options considered				

R.8 64th Avenue S.



64th Avenue S. & CR 17
Existing crossing infrastructure
may be sufficient

..... Existing Gaps

64th Avenue S. & Future 9th Street W.

North-South
uncontrolled


PEDESTRIAN HYBRID BEACON

OR


MEDIAN REFUGE ISLAND & BIKES

64th Avenue S. & Future 66th Street S.

East-West
controlled


CONVENTIONAL CROSSWALK

OR


CURB EXTENSIONS

DETAILS

Route ID: R.8
Priority Rank: #8
Alignment: 64th Avenue S.
Functional Classification: Local
Termini: Sheyenne Diversion Channel to City Line/57th Street S.
Route Strategy: Construct 10' Shared Use Path, on at least one side of street however, as a potential future arterial road, may encourage both sides of street long-term. This route was identified through public engagement.
Crossing Strategy: Varies by street classification, speed, traffic volume, etc. Specific crossing locations and strategy may not be understood until development and street network get laid out.

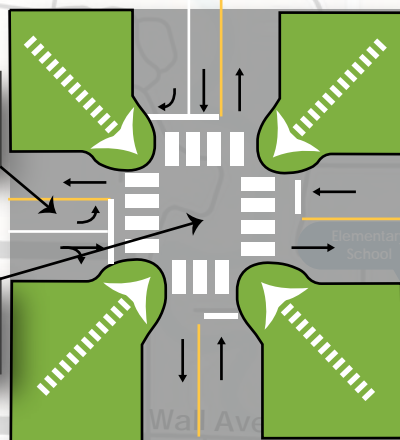
ROUTE	DESCRIPTION	LENGTH (MILES)	COST LOW	COST HIGH*
R.8	Shared use path, at least one side (south side) of 64 Ave. S. from Sheyenne Diversion Channel to 57th St. S.			
R.8.a	Construct a shared use path to fill gaps in existing network.	See R.8.a lines below.		
R.8.a.i	South side of 64th Ave. S. from Sheyenne Diversion Channel to 57th St. S.	2.1	\$4,200,000	\$4,200,000
R.9.b	Upgrade multimodal infrastructure at existing and future intersections. Prioritize locations strategic to downtown connections and connections to Horace Middle and High Schools.	See R.9.b lines below.		
R.9.b.i	Install HAWK signal or median refuge island with RRFBs at 64th Ave. S. & future 9th St. W. intersection.	intersection	\$90,000	\$280,000
R.9.b.ii	Construct grade separation or install HAWK signal at 64th Ave. S. & future 66th St. S. intersection.	intersection	\$280,000	\$500,000
TOTAL ROUTE (MILES)		2.1	\$4,200,000	\$4,200,000
TOTAL INTERSECTIONS		2	\$370,000	\$780,000
SUM R.8			\$4,570,000	\$4,980,000
*if multiple options considered				

I.1 Main Street & Wall Avenue

PRIORITY



INTERSECTION STRATEGY



Wall Ave. eastbound left turn lane warranted by 2031.

Traffic signal may be warranted by 2033 or 2034.

DETAILS

Intersection ID: I.1
Priority Rank: #1

Intersection Name: Main Street and Wall Avenue
Functional Classification: Minor Arterial and Collector/Local
Intersection Strategy: All-way stop control with traffic calming and crossing enhancements including curb extensions.
Other Considerations: Original concepts with mini roundabout at this location were poorly received by the public. Basic traffic operations analysis indicates signal warrant as soon as 2033. This intersection is key to multimodal traffic in downtown Horace, and vehicular mobility must be balanced with bicycle and pedestrian mobility and safety.
Associated Route ID(s): R.4 (High) & R.2 (Medium),



ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.1	Maintain existing control and add multimodal improvements at Main St. & Wall Ave. intersection.		
I.1.a	Upgrade multimodal infrastructure at all existing intersection legs with sidewalk/shared use path.	See I.1.a lines below	
I.1.a.i	Construct curb extensions (consider demonstration project).	\$2,500	
I.1.a.ii	Install crosswalk striping and stop bars.	\$7,500	
I.1.a.iii	Include ADA compliance.	\$5,000	
I.1.a.iv	Install pedestrian scale lighting.	\$25,000	
I.1.b	Construct full build out as multimodal connections are made or with Wall Ave. reconstruction project (all intersection legs).		
I.1.b.i	Construct curb extensions (consider demonstration project).	\$2,500	
I.1.b.ii	Install crosswalk striping and stop bars.	\$7,500	
I.1.b.iii	Include ADA compliance.	\$5,000	
I.1.b.iv	Install pedestrian scale lighting.	\$25,000	
I.1.c	At Main St. & Wall Ave. intersection, plan to construct a Wall Ave. eastbound left-turn lane by 2031.	\$100,000	
I.1.d	Evaluate traffic signal control with pedestrian-activated lead intervals, multimodal priority, and permissive detection at Main St. & Wall Ave. intersection.	\$750,000	
SUM I.1:		\$930,000	
*if multiple options considered			

I.2 Main Street & Ironwood Drive

PRIORITY



INTERSECTION STRATEGY



Ironwood Dr. eastbound right turn lane warranted by 2029.

DETAILS

Intersection ID: I.2

Priority Rank: #2

Intersection Name: Main Street and Ironwood Drive

Functional Classification: Minor Arterial and Local

Intersection Strategy: Reconfigure frontage road, eastbound Ironwood Drive right turn lane, enhanced pedestrian crossing, and consider traffic calming.

Other Considerations: Basic traffic operations analysis indicates signal warrant by 2040. This intersection was identified consistently as an area of concern for bicycle and pedestrian safety. Direct connection to Horace Elementary School with strategic importance for pedestrian priority.

Associated Route ID(s): R.4 (High).

Recommend frontage road reconfiguration (existing shown).

Traffic signal may be warranted by 2040.

ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.2	Maintain existing control and add multimodal improvements at Main St. & Ironwood Dr. intersection.		
I.2.a	Upgrade multimodal infrastructure at north leg of intersection.	See I.2.a lines below	
I.2.a.i	Install push-activated RRFBs and construct median refuge island.	\$90,000	\$90,000
I.2.a.ii	Upgrade/oversize existing signage and striping and install yield lines at crosswalk.	\$15,000	\$15,000
I.2.a.iii	Install pedestrian-scale lighting.	\$25,000	\$25,000
I.2.b	Demonstration Project: remove cut-thru traffic from Main St.'s west side frontage road. Add jersey barriers north of the Post Office to close from thru-traffic.	\$5,000	\$5,000
I.2.b.i	Construct a permanent closure of the frontage road north of the Post Office, which should include a turnaround adequate for emergency services and U.S. Postal Service vehicles.	\$100,000	\$100,000
I.2.c	Main St. & Ironwood Dr. turn lane improvements should only be considered with closure of thru-traffic on Main St.'s west side frontage road: Plan to construct an Ironwood Dr. eastbound turn-lane by 2029.	\$100,000	\$100,000
I.2.d	Evaluate all-way stop at Main St. & Ironwood Dr. intersection. Future traffic signal may be warranted.	\$30,000	\$750,000
SUM I.2:		\$365,000	\$1,085,000
*if multiple options considered			

I.3 CR 17 & Lakeview Drive

PRIORITY



INTERSECTION STRATEGY

Maintain existing with prioritized improvements north (Chestnut/81st Ave.) and south (Ironwood Dr.) of this intersection.

DETAILS

Intersection ID: I.3

Priority Rank: #3

Intersection Name: CR 17 and Lakeview Drive

Functional Classification: Minor Arterial and Local

Intersection Strategy: No change, maintain existing.

Intersection configuration with multiple lanes and turn-lanes makes multimodal crossing improvements more challenging, may be dangerous to encourage bicycle and pedestrian crossing of CR 17 at this location.

Other Considerations: Priority should be given to intersection just north and just south of this location where crossing may be safer. There was a lot of concern for bicycle and pedestrian safety near the Horace Elementary School therefore focused, robust bicycle and pedestrian improvement should be considered at Ironwood Drive over this location with more CR 17 travel/turn lanes to cross.

Cost Estimate: No project recommended at this time.

Associated Route ID(s): R.4 (High)

Horace Lions Park

CR17

76th Ave S

76th Ave S

Middle School

The Headquarters

High School

Taco Revolution

Casey's

Elementary School

3

Wall Ave

Grain Elevators

63rd St

Freed Park

Big Erv's

Terra Gardens Park

Park Dr

City Hall

CR17

100th Ave S

I.4 Main Street & Center Avenue

PRIORITY



INTERSECTION STRATEGY



DETAILS

Intersection ID: I.4

Priority Rank: #4

Intersection Name: Main Street and Center Avenue

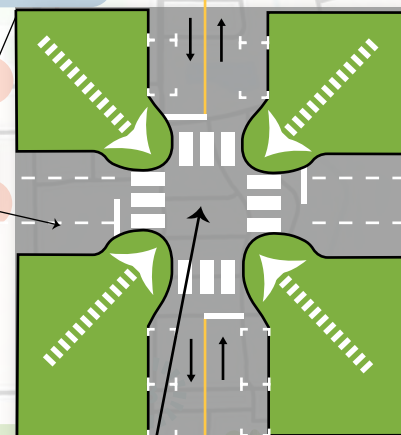
Functional Classification: Minor Arterial and Local

Intersection Strategy: All-way stop control with traffic calming and crossing enhancements including curb extensions.

Other Considerations: All-way stop control should be considered alongside future development downtown. Integrate existing multimodal infrastructure such as the yield street markings on Center Avenue/Thue Court. This intersection is key to multimodal traffic in downtown Horace, and vehicular mobility must be balanced with bicycle and pedestrian mobility and safety.

Associated Route ID(s): N/A (Downtown Focus Area)

Integrate existing yield-street on Center Ave.



All-way stop timed with future development

ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.4	Convert Main St. & Center Ave. intersection to a 4-way stop with multimodal improvements.	\$30,000	
I.4.a	Upgrade multimodal infrastructure at all intersection legs.	See I.4.a lines below	
I.4.a.i	Construct curb extensions	\$40,000	
I.4.a.ii	Install crosswalk striping and stop bars.	\$15,000	
I.4.a.iii	Update ADA compliance	\$10,000	
I.4.a.iv	Add pedestrian scale lighting.	\$50,000	
	SUM I.4	\$145,000	
*if multiple options considered			

I.5 Main Street & Park Drive

PRIORITY



INTERSECTION STRATEGY



DETAILS

Intersection ID: I.5

Priority Rank: #5

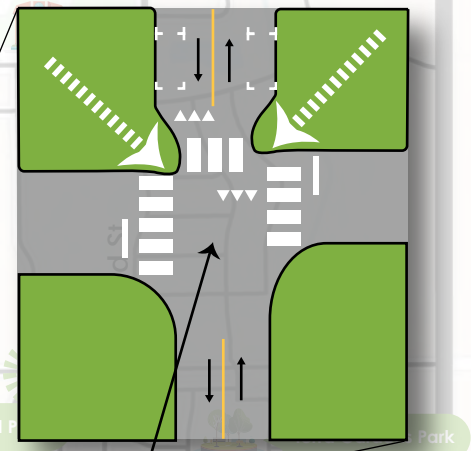
Intersection Name: Main Street and Park Drive

Functional Classification: Minor Arterial and Local

Intersection Strategy: Traffic calming and crossing enhancements including curb extensions.

Other Considerations: Park Drive provides a critical gateway to downtown Horace from the south. This intersection can contribute to calming traffic by integrating curb extensions. Consider timing improvements at this intersection with future route IDs #1 and #3.

Associated Route ID(s): R.1 (High) & R.3 (Low)



North-south crosswalks should be installed with potential bicycle and pedestrian facilities.

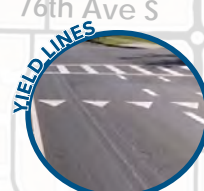
ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.5	Convert Main St. & Park Dr. intersection to a 4-way stop with multimodal improvements.	\$30,000	
I.5.a	Upgrade multimodal infrastructure all intersection legs.	See I.5.a lines below	
I.5.a.i	Construct curb extensions.	\$40,000	
I.5.a.ii	Install crosswalk striping and stop bars.	\$15,000	
I.5.a.iii	Include ADA compliance.	\$10,000	
I.5.a.iv	Pedestrian scale lighting.	\$50,000	
	SUM I.5	\$115,000	
*if multiple options considered			

I.7 CR 17 & 100th Avenue S. (CR 14)

PRIORITY



INTERSECTION STRATEGY



DETAILS

Intersection ID: I.7

Priority Rank: #6

Intersection Name: CR 17 and 100th Avenue S. (CR 14)

Functional Classification: Minor Arterial and Minor Arterial/Collector

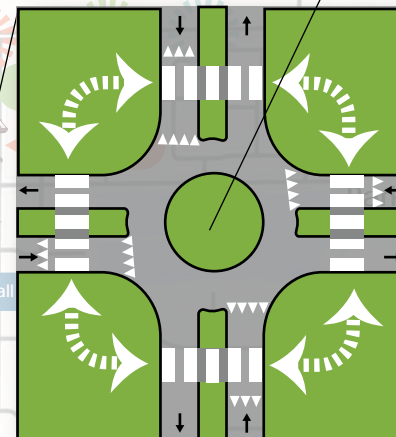
Intersection Strategy: Traffic calming and crossing enhancements with roundabout.

Other Considerations: There were several comments from the public regarding safety concerns at this intersection (all modes). Other regional intersections such as 76th Avenue S. and 64th Avenue S. have successfully used roundabouts for traffic control however, pedestrian crossing enhancements should be implemented like the aforementioned examples.

Cost Estimate: Planning-level cost TBD with phasing.

Associated Route ID(s): R.3 (Low)

Roundabout pedestrian crossing is critical at this location, at the convergence of two minor arterial roads.



ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.7	Convert CR 17 & 100 th Ave. S. intersection to a roundabout with multimodal improvements.	\$3,000,000	
I.7.a	Include multimodal improvements on all intersection legs.	See I.7.a lines below	
I.7.a.i	Include median refuge islands.	\$50,000	
I.7.a.ii	Include crosswalk striping and yield lines (time with path connections).	\$15,000	
I.7.a.iii	Include ADA compliance.	\$10,000	
I.7.a.iv	Include streetlights.	\$50,000	
	SUM I.7	\$3,125,000	
*if multiple options considered			

I.9 Wall Avenue & Front Street

PRIORITY



INTERSECTION STRATEGY



AND/OR



+



DETAILS

Intersection ID: I.9

Priority Rank: #7

Intersection Name: Wall Avenue and Front Street

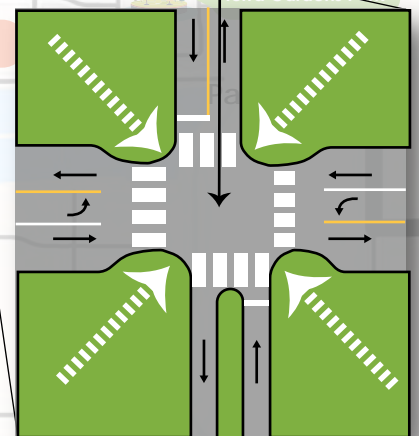
Functional Classification: Collector and Local

Intersection Strategy: Traffic calming and crossing enhancements with curb extensions and/or median refuge island on west side of intersection.

Other Considerations: Wall Avenue currently has no bicycle and pedestrian facilities. There is a high-level of interest to urbanize the corridor as a 2-3 lane cross section. Between Main Street and Nelson Drive a 2-lane cross section is recommended. A 3-lane cross section may be recommended east of Nelson Drive as adjacent growth and development fills in. Front Street provides a direct route to the High School and Middle School and poses a significant gap and barrier for multimodal mobility.

Associated Route ID(s): R.2 (Medium) & R.7 (Medium)

Strategy may change depending upon Wall Avenue design.



ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.9	Add multimodal improvements at Wall Ave. & Front St. intersection.		
I.9.a	Upgrade multimodal infrastructure at all intersection legs.	See I.9.a & b lines below	
I.9.a.i	Construct curb extensions.	\$40,000	\$40,000
I.9.a.ii	Install crosswalk striping and stop bars.	\$15,000	\$15,000
I.9.a.iii	Include ADA compliance.	\$10,000	\$10,000
I.9.a.iv	Install streetlights.	\$50,000	\$50,000
I.9.a.v	Install RRFBs and/or median refuge island at shared use path connection across Wall Ave.	\$25,000	\$75,000
I.9.b	Install a temporary/demonstration pedestrian crossing improvement.	\$5,000	\$5,000
	SUM I.9:	\$145,000	\$195,000
*if multiple options considered			

I.10 Wall Avenue & Nelson Drive/Future Lakeview Drive

PRIORITY



INTERSECTION STRATEGY



DETAILS

Intersection ID: I.10

Priority Rank: #8

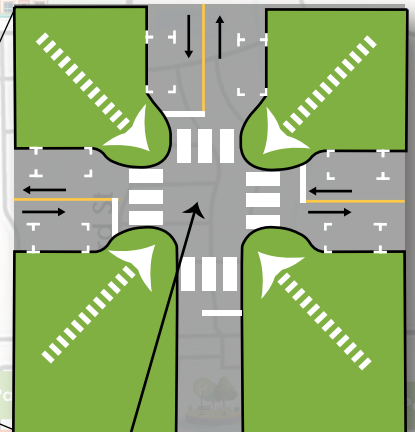
Intersection Name: Wall Avenue and Nelson Drive/Future Lakeview Drive

Functional Classification: Collector and Local

Intersection Strategy: All-way stop control with traffic calming and crossing enhancements including curb extensions.

Other Considerations: All-way stop control should be considered alongside future development downtown and in Lakeview, north of intersection. This intersection is key to multimodal traffic in downtown Horace and to make a cohesive connection to Lakeview. Vehicular mobility must be balanced with bicycle and pedestrian mobility and safety.

Associated Route ID(s): R.2 (Medium)



All-way stop timed with future development downtown and in Lakeview.

ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.10	Convert Wall Ave. & Nelson Dr./Future Lakeview Dr. to a 4-way stop with multimodal improvements.	\$30,000	
I.10.a	Include multimodal improvements on all intersection legs.	See I.10.a lines below	
I.10.a.i	Include curb extensions.	\$40,000	
I.10.a.ii	Include crosswalk striping and stop bars.	\$15,000	
I.10.a.iii	Include ADA compliance.	\$10,000	
I.10.a.iv	Pedestrian-scale lighting.	\$50,000	
	SUM I.10	\$145,000	
*if multiple options considered			

I.6 CR 17 & Liberty Lane

PRIORITY



INTERSECTION STRATEGY



DETAILS

Intersection ID: I.6

Priority Rank: #9

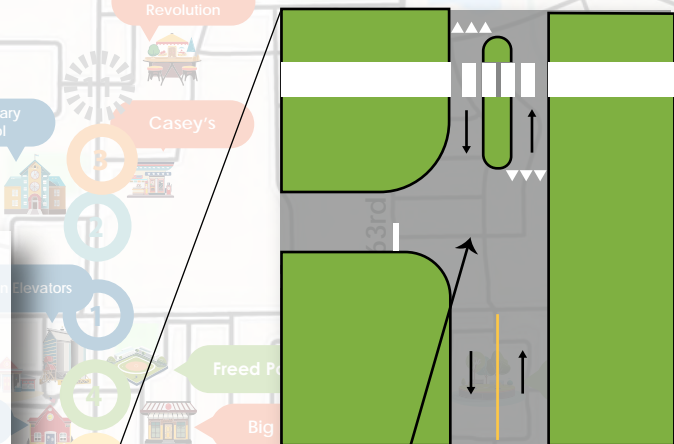
Intersection Name: CR 17 and Liberty Lane

Functional Classification: Minor Arterial and Local

Intersection Strategy: Traffic calming and crossing enhancements including pedestrian all refuge island with RRFBs.

Other Considerations: Crossing may not be necessary until implementation of Route ID #3 and future development on the east side of CR 17.

Associated Route ID(s): R.3 (Low)



Crossing may be contingent upon development east of CR 17 and Route ID #3.

ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.6	Add multimodal improvements at CR 17 & Liberty Ln. intersection.		
I.6.a	Upgrade multimodal infrastructure at north leg of intersection (time with path connections).	See I.6.a lines below	
I.6.a.i	Install median refuge island with RRFBs.	\$75,000	
I.6.a.ii	Install crosswalk striping and yield lines.	\$15,000	
I.6.a.iii	Include ADA compliance.	\$10,000	
I.6.a.iv	Install streetlights.	\$50,000	
	SUM I.6	\$150,000	
*if multiple options considered			

I.8 Wall Avenue & 63rd Street S.

PRIORITY



INTERSECTION STRATEGY



DETAILS

Intersection ID: I.8

Priority Rank: #10

Intersection Name: Wall Avenue & 63rd St.

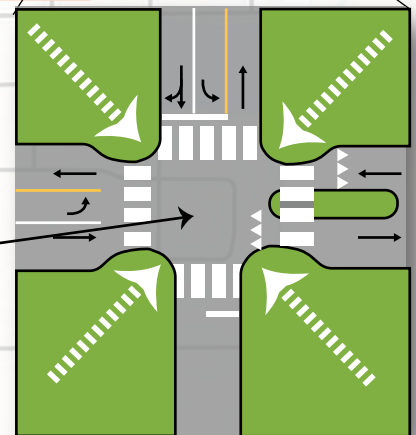
Functional Classification: Collector and Collector

Intersection Strategy: Traffic calming and crossing enhancements including pedestrian all refuge island with RRFBs.

Other Considerations: Pedestrian refuge located on east side to align with existing shared use path.

Associated Route ID(s): R.2 (Medium)

Strategy may change depending on Wall Avenue Design.



ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.8	Add multimodal improvements at Wall Ave. & 63rd St. S. intersection.		
I.8.a	Upgrade multimodal infrastructure at east and south legs of intersection.	See I.8.a lines below	
I.8.a.i	Install crosswalk striping and stop bars.	\$15,000	
I.8.a.ii	Install push-activated RRFBs at shared use path connection across Wall Ave (east leg).	\$75,000	
I.8.a.iii	Include ADA compliance.	\$10,000	
I.8.b	Full build out of multimodal improvements (time with future Wall Ave. project).	See I.8.b lines below	
I.8.b.i	Include curb extensions.		\$40,000
I.8.b.ii	Include median refuge island with RRFBs (east leg) and yield lines.		\$75,000
I.8.b.iii	Include crosswalk striping and stop bars.		\$15,000
I.8.b.iv	Include streetlights.		\$50,000
	SUM I.8:	\$100,000	\$180,000
*if multiple options considered			

I.11 CR 17 & Chestnut Drive/81st Avenue S.

PRIORITY



INTERSECTION STRATEGY



OR



DETAILS

Intersection ID: I.11

Priority Rank: #11

Intersection Name: CR 17 and Chestnut Drive/81st Avenue S.

Functional Classification: Minor Arterial and Local

Intersection Strategy: Given the speed and configuration of CR 17 through this intersection, a pedestrian hybrid beacon or grade separation would be recommended.

Other Considerations: Any other crossing strategy should not be considered unless however, a traffic signal is warranted in the future. Pedestrian refuge located on east side to align with existing shared use path.

Associated Route ID(s): R.4 (High)

Crossing infrastructure intensity is dictated by CR 17 speed and current configuration

ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.11	Add multimodal improvements at CR 17 & Chestnut Dr. intersection or CR 17 & 81st St. S. intersection.		
I.11.a	Upgrade multimodal infrastructure at north or south leg of intersection.	See I.11.a lines below	
I.11.a.i	Install HAWK signal or grade separation.	\$280,000	\$500,000
I.11.a.ii	Install crosswalk striping and stop bars or yield lines as applicable.	\$15,000	\$15,000
I.11.a.iii	Include ADA compliance.	\$10,000	\$10,000
I.11.a.iv	Install streetlights.	\$50,000	\$50,000
	SUM I.11	\$355,000	\$575,000
*if multiple options considered			

I.12 63rd Street S. & 79th Avenue S.

PRIORITY



Infrastructure could be retrofit into existing intersection.

INTERSECTION STRATEGY



DETAILS

Intersection ID: I.12

Priority Rank: #12

Intersection Name: 63rd Street S. and 79th Avenue S.

Functional Classification: Collector and Local

Intersection Strategy: Traffic calming and crossing enhancements including pedestrian refuge island with RRFBs.

Other Considerations: This intersection was mentioned in public feedback several times and provides critical multimodal mobility to the High School and Middle School.

Associated Route ID(s): N/A (From public feedback)

CR17

100th Ave S

ROUTE	DESCRIPTION	COST LOW	COST HIGH*
I.12	Add multimodal improvements at 63rd St. S. & 79th Ave. S. intersection.		
I.12.a	Upgrade multimodal infrastructure at east and north legs of intersection.	See I.12.a lines below	
I.12.a.i	Install median refuge island with RRFBs (north leg).	\$280,000	
I.12.a.ii	Install crosswalk striping, stop bars, and yield lines as applicable.	\$15,000	
I.12.a.iii	Include ADA compliance	\$10,000	
I.12.a.iv	Install streetlights	\$50,000	
	SUM I.12	\$355,000	
*if multiple options considered			